

City of Alexandria, Virginia

MEMORANDUM

DATE: JANUARY 20, 2004
TO: THE HONORABLE MAYOR AND MEMBERS OF CITY COUNCIL
THRU: PHILIP SUNDERLAND, CITY MANAGER
FROM: EILEEN FOGARTY, DIRECTOR 
PLANNING AND ZONING
SUBJECT: UNDERGROUND PARKING AT T.C. WILLIAMS HIGH SCHOOL

ISSUE: Underground parking at the proposed new T.C. Williams High School.

RECOMMENDATION: That Council approve the proposed T. C. Williams application as it relates to parking, with two levels of structured above-ground parking.

DISCUSSION

At its hearing on the proposed new T. C. Williams High School, the Planning Commission inquired as to the feasibility of building underground parking at the new school site. Attached is the response of the Alexandria City Public Schools, prepared for City Council, which explains ACPS' reluctance to include underground parking because of both security and expense concerns.

As to security, ACPS is concerned that many people will not use the underground parking because of fear of danger, that its ability to monitor student behavior in the garage will be limited, and that an underground garage could create a target for terror-based acts. In addition, it cites its heightened responsibility for students and their inexperience with negotiating difficult driving maneuvers often associated with underground parking garages.

As to expense, ACPS projects that the garage which is part of the current proposal will cost approximately \$2,850,000. It includes 416 parking spaces for a per space cost of \$6,850. The cost of an underground parking space is typically cited as \$20,000. For 416 spaces, the total cost of the garage would be \$8,320,000, an approximate cost increase of \$5.47 million to provide underground parking.

City staff discussed the potential for underground parking with ACPS at the outset of the planning process and at several meetings. City staff's interest was in keeping as much of the constrained school site open as possible. ACPS was adamant at that time that it did not support underground parking as part of the T.C. Williams campus because of its concerns for the security of students, teachers and staff. City staff has, therefore, supported the schools' position and the proposed above-ground structured parking.

The proposed structured parking is a great improvement over a typical school layout, included at the existing T.C. Williams site, which includes acres of surface parking. The land required for the proposed garage (approximately 1.6 acres) is about half as large as the land area that would be required if the same number of spaces were provided in surface lots. Therefore, given ACPS' strong reluctance to include underground parking, staff finds that the proposed parking garage to be a good solution for parking at the new school.

STAFF: Barbara Ross, Deputy Director, Planning and Zoning
Thomas Luebke, City Architect

CONSIDERATION OF UNDERGROUND PARKING AT T. C. WILLIAMS HIGH SCHOOL

At the January 6, 2004 Planning Commission meeting, the T. C. Williams High School project was approved. However, the commission requested that the school division submit a written justification for why they did not want an underground parking structure.

The following is an analysis of the issues regarding an underground parking garage for T. C. Williams High School:

- The T. C. Williams High School Project includes a parking structure designed with a partially in-ground first floor parking garage and a second floor open parking deck.
- It is designed in such a way that adult staff members park on the lower level and the students park on the upper open deck. Observation and supervision of student activity is achieved by oversight from the existing two-story school, security cameras, and staff monitors.
- This structure will accommodate 198 automobiles on the lower level and 218 automobiles on the upper deck for a total of 416. The structure is estimated to cost approximately \$2,850,000 or \$6,850 per parking space.

As part of the planning process, ACPS investigated building an underground parking structure in order to create additional open space at the school site. An underground parking structure was not considered feasible due to the following reasons:

- People generally have a strong perception that underground garages are unsafe or dangerous. This is not necessarily true. Safeguards can be put in place to ensure safety by the use of electronic surveillance or security monitors. Regardless, the safety concern is real, especially with regard to young people using an underground garage. The school division is legally "in loco parentis" to the students at T. C. Williams High School. Its responsibility is greater than perhaps other institutions or companies that offer its occupants underground parking. Young people are somewhat more vulnerable to a host of actions and behaviors different from the activities or circumstances that may exist in underground garages open to the general public or office tenants. Additionally, underground garages typically include steep ramps and sharp turns in order to efficiently provide access to and from the garage. The types of maneuvers required to operate a motor vehicle in an underground garage are somewhat riskier than on surface style parking lots. High school students for the most part are relatively inexperienced and less proficient drivers.
- School divisions must always anticipate potential safety and security issues. This is particularly true in light of the recent events facing the nation. Unfortunately, terrorism is a real and potential danger. An underground garage poses a risk as a possible target for a terrorist. This structure, depending on the timing, could house several hundred automobiles all containing fuel in a tight area with limited means of egress. As hard as it might be to imagine, these conditions could contribute to an enormous tragedy should someone take a deliberate action to cause harm.

The Commonwealth of Virginia Homeland Security guidelines for local government actions during heightened threat levels, as in Alert Level Orange, states “erect outside barricades and security checks to deny access to underground parking areas.” Based on this guideline, underground parking areas are identified as an area of risk. This condition would render the facility unavailable for parking and would significantly affect school operations.

- An underground parking garage requires a much more complex infrastructure which includes:
 - an additional structure to create a roof as well as provide a waterproofing system for that roof
 - additional excavation costs
 - an additional sprinkler system
 - a complete ventilation system for automotive exhaust
 - an elevator (not required by current design)
 - an additional stairtower for egress
 - external groundwater control and sump pump system
 - costs to provide landscaping and establishment of lawn area
 - a larger structure to accommodate internal vehicle circulation

- The current parking garage facility is projected to cost \$2,850,000 or approximately \$6,850 each for the planned 416 parking spaces. The relative simplicity of the current design with only a single elevated deck and a slab-on-grade lower level, coupled with the most efficient internal vehicle circulation pattern, provide the most cost-effective parking solution for the available land area. Generally accepted industry standards for underground parking garages indicate costs of approximately \$20,000 per parking space. This would result in a potential cost increase of approximately \$5.47 million to create underground parking in lieu of the proposed parking structure.

In summary, for reasons of student safety, supervision, costs and construction schedules, the school division recommends the elevated garage as delineated in the design documents.

January 16, 2004

EXECUTIVE SUMMARY

The proposal by Alexandria City Public Schools (ACPS) to build a new T.C. Williams High School provides the City with a singular opportunity. This state-of-the art facility will allow ACPS to deliver a school that, over the next decades, will represent the best in educational programming and design, all while addressing the particular makeup and needs of Alexandria's high school population.

Prospects for Alexandria students are especially exciting in light of the recently proposed and completed improvements to other city schools, including George Washington Middle School, Hammond Middle School, and Minnie Howard Ninth Grade Center. The educational edge provided to the students of these schools, through modern additions and technology upgrades, will be fully realized as they continue learning at a new, equally modern T. C. Williams High School.

Staff endorses the T.C. Williams High School project, and recommends approval of the land use requests discussed in these reports, subject to staff's recommended conditions.

Land Use Issues

The new high school, proposed to be built on a relatively small site, presents several issues from a land use perspective. Some of the land involved must be rezoned to accommodate the school; the increased size and height of the building require special use permit approval; and the distance between the building and its neighbors on two sides is narrower than required for a building of this size, which requires zoning modifications.

City staff, ACPS staff, and ACPS consultants have worked extensively over the last several months on this complex project, to collaboratively produce the best possible facility and site improvements. In response to concerns expressed by City staff and property owners neighboring the school site, the following issues have been addressed:

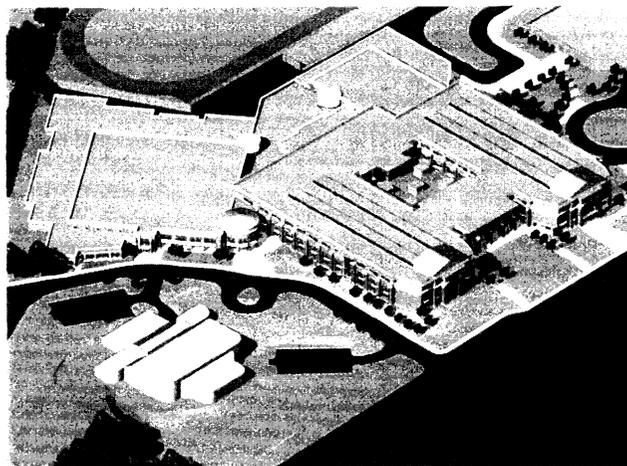
- maximizing open space and recreational facilities
- reducing the perceived mass of the building through architectural techniques
- landscaping to soften the appearance of the building and enhance outdoor spaces
- addressing parking needs
- ensuring the smooth flow of traffic on school property and King Street
- creating usable streetscape and pedestrian environments
- reducing noise and other impacts on adjoining properties
- securing the building and grounds for safe educational and community use
- phasing construction so as to minimize disruption for students, neighbors, and the greater community

Each of the above issues is discussed in detail in the reports that follow. The most significant issues and solutions are summarized below.

The Proposed School Site

The planning process for the school has been guided by two decisions: to build the new school only on school land, west of Chinquapin Drive; and to allow the existing school to operate while the new school is built, thus not requiring a temporary, off-site school facility during the construction period. Also guiding the process are the desire to preserve open space and retain the school's existing track and stadium in their current locations. All of these factors effectively limit the available land to the area east of the existing school building and west of Chinquapin Drive.

With such a large building (460,000 square feet) in such a small allowable area, the result is a school with a massive appearance.



New T.C. Building (Chinquapin Center bottom left)

In order to address the impact of the proposed building, staff has worked with ACPS to create a welcoming yet highly functional facility for students and the community.

Open Space and Recreational Uses

While the proposed new building is large, and will be located close to King Street and Chinquapin Drive, it will be framed by significant open spaces. Chinquapin Park is adjacent to the building on the east side, and city staff has collaborated with ACPS to create a broad open space, with a new athletic field, along King Street on the school's west side.

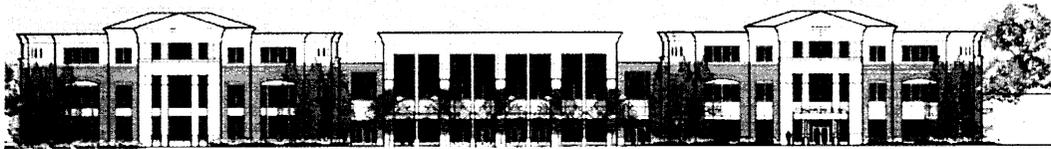
The new open space serves a dual purpose: to provide relief from the prominence of the school building, and to provide shared athletic and recreational facilities, which has been another driving factor in designing the new school building. In addition, the school's interior has been rearranged to place the gymnasium and athletic facilities close to the Chinquapin Recreation Center, to maximize joint student/community use of both facilities.

Green Building

The new school complex is being built as a “green” building, in keeping with the Leadership in Energy and Environmental Design (LEED) program. The LEED rating system for environmentally-friendly construction sets a standard for determining the environmental components of a project. ACPS voted to support a sustainable design, and its consultants have designed a variety of features for the school that meet the LEED requirements. The school’s green facilities include a large underground cistern for capturing and reusing rainwater, landscaping with native species, reduced light pollution and use of natural light, a recycling program for building materials, reduced energy and water use, and use of chemical-free and recycled interior materials.

Architecture

City staff has worked with the school architects to preserve their building design concept, while minimizing the perceived mass of the building through architectural treatments. Those treatments include adding recesses and projections along the long building walls, designing special entry elements, adding architectural details to provide a smaller visual scale, and varying the roof line. The alterations provide as much visual relief as possible given the expanse of the structure, and help achieve a well-proportioned, dignified appearance appropriate for a prominent civic building.



Front of Building - King Street



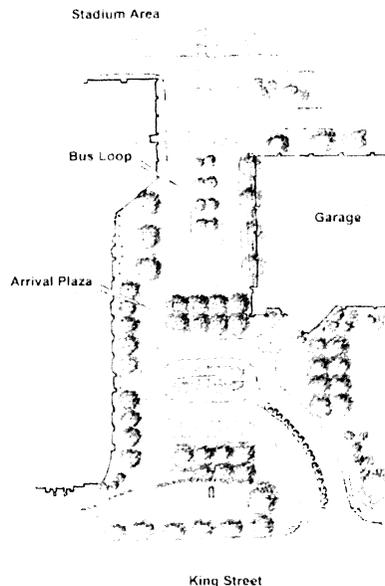
Front West Side of Building

Pedestrian Environment

One of the most significant areas of design focus has been on the pedestrian who uses the school building and grounds. The goal has been to achieve functional, convenient, and attractive places for people to walk between functions within the school site (for example, from the parking garage to the building or an event venue), as well as between the school facilities and Chinquapin Park and Recreation Center. Additional design improvements to the pedestrian plan are recommended by staff and additional work will be completed as part of the final site plan.

The outdoor spaces on the overall site will be enhanced with landscaping, and the site plan includes several important pedestrian areas for access and gathering. Some examples are:

- A large arrival plaza located at the center of the site, between the entrance to the school and the parking lot. The plaza is adjacent to the student drop-off area and the bus loading area, and provides a direct path to the track and stadium field. Designed to include trees and decorative pavers, the plaza pulls together several functions and creates an attractive ceremonial entrance for vehicles and pedestrians.



Main Entry Plaza

- A rear pedestrian walkway along the east/southeast of the site. Students and citizens will be able to follow a decoratively paved and landscaped path from Chinquapin Park to the stadium field, and from the field to the parking garage.
- A formal streetscape along King Street, with a 10-foot sidewalk and street trees along the long, 1000-foot frontage.
- A student commons area, on the King Street frontage and in the middle of the building, which provides architectural interest in addition to a screened outdoor dining/meeting area adjacent to the cafeteria.

Parking

The question of parking for the high school has been central for ACPS and the city. ACPS is committed to providing adequate parking for all the school's uses, which include a comprehensive after-hours adult education program, athletic events, and community functions.

The proposed parking plan calls for a two-floor parking garage to occupy the land west of the new school facility (where the current building stands). The garage will provide 416 parking spaces for staff and for students with parking permits. Additional on-site parking will be allotted for visitors and staff, for a total of 431 – nearly 25 percent more spaces than are available at the school today.

While there is some concern regarding the need for overflow parking, during such times as when two events occur simultaneously, the school will continue its current utilization of off-site parking to augment its own.

Community Concerns

Seventeen single-family homes are located on the rear (Bishop Lane) and west (Woods Avenue) boundaries of the school site. The owners of these and other nearby properties continue to be impacted by living so close to a large, active high school. To provide a venue for concerns, City and school staff have held meetings with many community groups, and several issues raised during these meetings have become staff conditions for development approval. Specific recommendations include a prohibition on permanent lighting at the stadium, limits on vehicular access at Woods Avenue, and provision of landscaped buffers.

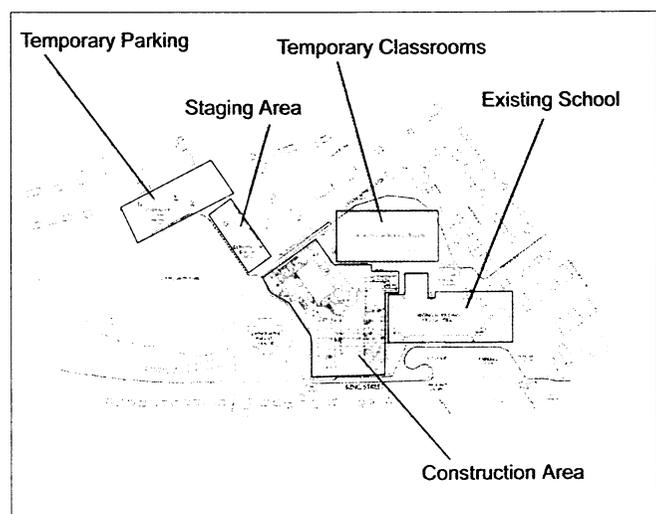
ACPS is also collaborating with neighboring property owners on a series of operational issues for the future, which address the use of the stadium and other athletic fields; the use of the sound system for the fields and the school; parking for the Oakland Baptist Church; and hours for deliveries.

Perhaps the most important development to come from the neighborhood meetings is the establishment of two advisory committees. These committees, composed of representatives from ACPS, the City recreation department, and neighborhood groups, provide a forum for discussion and resolution of issues that may arise during construction or after the new school has opened. ACPS has agreed to coordinate the committees, and City staff has included a condition ensuring the coordination.

Construction Phasing

Staff is aware that construction of a new high school at this centrally located site on King Street will cause disruption for students, teachers, neighbors, and the greater community. Under ACPS' current schedule, construction will begin in 2004, with the new main school building opening in September 2007 but with construction continuing through much of 2008.

During that time, there will be new routes for traffic to enter and leave the school site, temporary parking facilities, and temporary classroom facilities for some students.



Construction Phasing Plan

City staff is working closely with ACPS to address concerns regarding each of these disruptions. Detailed phasing plans, completed early in the final site plan review process, are required by the staff conditions.

Generalized phasing plans are included with the current ACPS application and show the use of the stadium field for school trailers or temporary buildings. T. C. football games will take place at an alternative location, likely at a Fairfax County school facility. In addition, the practice fields behind the school, along the Chinquapin Drive loop, will be used for temporary parking and construction staging.

The existing school building and some of its parking lots will remain operational for buses and staff during construction.

Conclusion

Staff recommends approval of the development special use permit (including the associated rezoning, master plan amendment, and zoning modifications and approvals required) for the T. C. Williams High School project. Although the building is large, particularly given the limited available land area, ACPS and the City have worked collaboratively to enhance the facility's appearance while preserving its intended functionality.

Docket Item #
MASTER PLAN AMENDMENT #2003-0010
REZONING #2003-0006
T.C. WILLIAMS HIGH SCHOOL

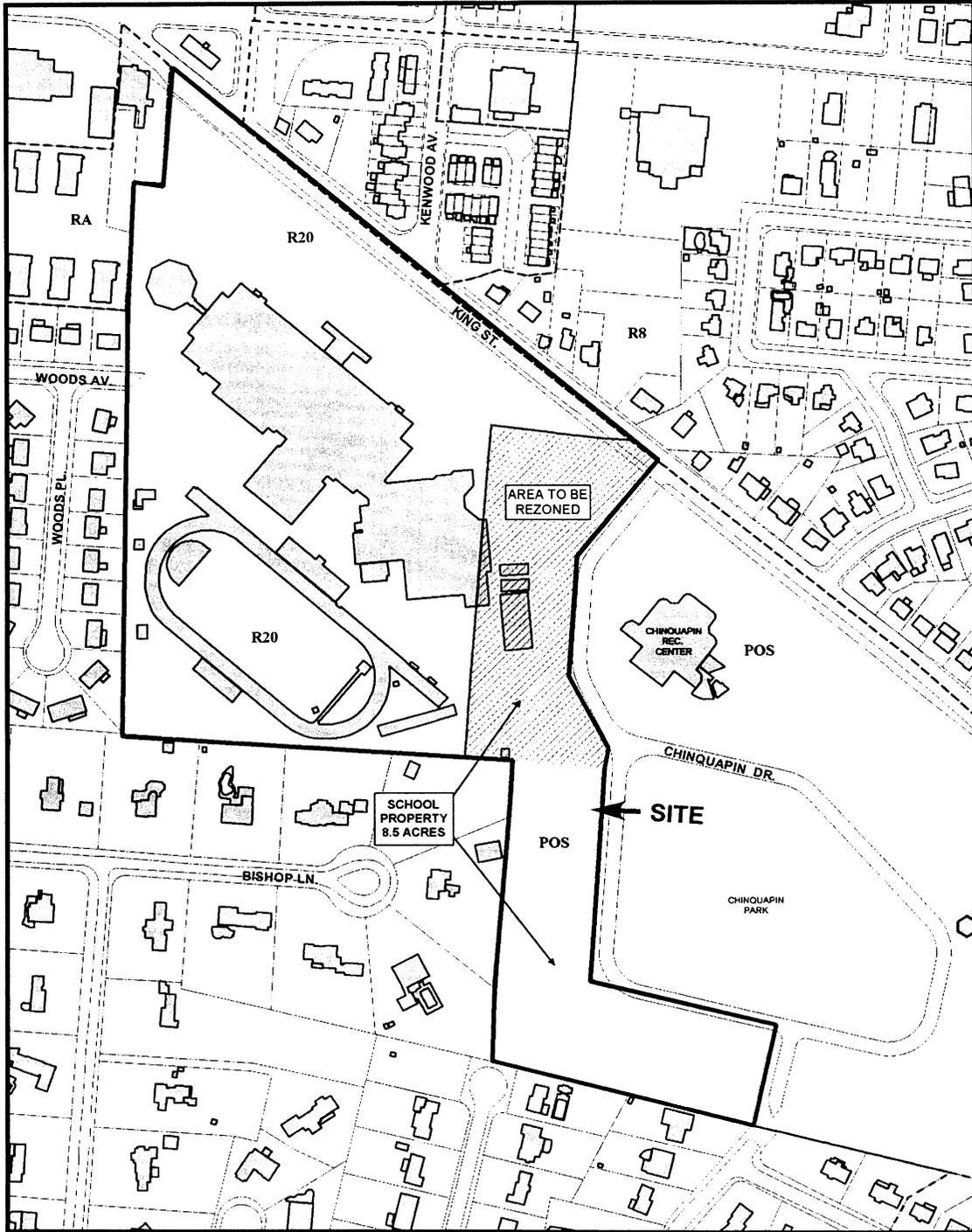
Planning Commission Meeting
January 6, 2004

ISSUE: Consideration of a request for an amendment to the Taylor Run/Duke Street Small Area Plan chapter of the City's Master Plan to change the land use designation of the subject property from P/Parks to I/Institutional.

Consideration of a request for a zoning amendment to change the zoning designation of the subject property from POS/Public Open Space to R-20/Single family residential.

APPLICANT: Alexandria City Public Schools, by J. Howard Middleton, attorney

LOCATION: 3330 King Street



MPA #2003-0010
REZ #2003-0006

01/06/04



MPA #2003-10
REZ #2003-06
T.C.WILLIAMS HIGH SCHOOL

STAFF RECOMMENDATION

Staff recommends **approval** of the master plan amendment, pursuant to the attached resolution.

Staff recommends **approval** of the rezoning request.

I. BACKGROUND

The applicant, Alexandria City Public Schools (ACPS), requests approval to change the zoning of a 3.8-acre parcel of land within the T.C. Williams High School site from POS/Public Open Space to R-20/Residential. The applicant also requests approval to amend the Taylor Run/Duke Street Small Area Plan to redesignate an 8.5-acre parcel from P/Parks to I/Institutional, which is the designation that applies to the remainder of the T.C. Williams school site. The amendments are proposed in order to allow the proposed new T.C. Williams complex to be built.

A. LAND PARCELS

The T.C. Williams/Chinquapin Park land area occupies some 51 acres and is comprised of three parcels of land.

Chinquapin Park site. To the east of the existing school is the Chinquapin Park land area. It is 28 acres in size, zoned POS/Public Open Space, and controlled by the City. The land is developed with a variety of athletic and community facilities, including Chinquapin Recreation Center, playing fields, tennis courts, and community gardens.

Existing T.C. Williams High School site. To the west of Chinquapin Park is the 19.8-acre ACPS site where the existing T.C. Williams building, stadium, and parking lots are located. In the master plan, this parcel is shown as I/Institutional. It is zoned R-20/Residential. Public schools are allowed within all of the residential zones.

8.5-Acre ACPS site. Between the two larger parcels is the Chinquapin Drive entrance from King Street and the remaining, separate 8.5-acre parcel, which is "L" shaped and runs along Chinquapin Drive. This parcel is controlled by ACPS. The north end of the parcel, closest to King Street, includes parking lots for T.C. Williams, a part of the Career Wing, and temporary classroom trailers. In the remaining part of the "L" are the T.C. Williams' practice field and additional playing fields that border homes on Bishop Lane.



School Owned Parcels

B. REZONING

Because the plan is to retain the existing high school building while the new school is being constructed, ACPS is limited to using a POS-zoned 3.8-acre portion of the site for the building. The zoning ordinance does not permit school buildings in the POS zone. Therefore, rezoning is necessary.

The 3.8 acres in question are closest to Chinquapin Drive, the current location of some of the school parking lots and the temporary trailers. By retaining the large school parcel (19.8 acres) in the R-20 zone and rezoning a 3.8 acre portion of the POS land to R-20, the proposed amendment will create a new 23.68-acre parcel in the R-20 zone for the new T.C. Williams building.

In addition, the existing high school is a noncomplying structure; since 1992, the building has exceeded the maximum floor area ratio requirements of its R-20 zoning. The proposed building will continue to exceed the R-20 FAR, and adding more land to the FAR equation affects the calculation by lowering the FAR slightly.

A text amendment (#2003-0005) recently approved by City Council allows an increase in floor area ratio for public schools in the R-20 zone and, under that provision, ACPS is requesting an increase in density. The following chart shows the difference in land areas and density before and after the proposed rezoning of the 3.8-acre site.

Zoning Specifications of T.C. Williams High School Site

	Current	Proposed *
Total land area in school site	28.3 acres	28.3 acres
R20	19.8 acres/	23.7 acres
POS	8.5 acres	4.62 acres
Building floor area	355,000 sf	547,000
FAR	.41(.25 allowed in R-20 zone)	.51

* Proposed with rezoning and DSUP approval.

III. STAFF ANALYSIS

Staff supports the rezoning of a part of the school's POS parcel to the R-20 zone. Although staff is rarely supportive of rezoning POS land, in this case the land will retain its public benefit for the community. The amendment will allow a new T.C. Williams High School to be built on the current school site, while maintaining the existing school during construction.

Furthermore, this land, although a relatively open area historically, has never been a park. Given that the parcel is currently the location for parking and temporary classroom trailers, there will be very little loss of open space in conjunction with this rezoning.

Finally, the plan for the new high school includes a new practice field at the northwest corner of the site, which will provide additional open space and public benefit.

IV. CONCLUSION

Staff recommends approval of the applicant's request to rezone a portion of the property from POS to R-20 and to amend the City's Master Plan to change the land use designation of a portion of the property from P/Parks, Public Schools and Open Space to I/Institutional.

STAFF: Eileen Fogarty, Director, Department of Planning and Zoning
Barbara Ross, Deputy Director, Department of Planning and Zoning
Laura Durham, Urban Planner

RESOLUTION NO. MPA 2003-0010

WHEREAS, under the Provisions of Section 9.05 of the City Charter, the Planning Commission may adopt amendments to the Master Plan of the City of Alexandria and submit to the City Council such revisions in said plans as changing conditions may make necessary; and

WHEREAS, an application for amendment to the Taylor Run/Duke Street Small Area Plan chapter of the 1992 Master Plan was filed with the Department of Planning and Zoning on November 11, 2003 for changes in the land use designations to the parcels at 3330 King Street.

WHEREAS, the Department of Planning and Zoning has analyzed the proposed revision and presented its recommendations to the Planning Commission; and

WHEREAS, a duly advertised public hearing on the proposed amendment was held on January 6, 2004 with all public testimony and written comment considered; and

WHEREAS, the Planning Commission finds that:

1. The proposed amendment is necessary and desirable to guide and accomplish the coordinated, adjusted and harmonious development of the Taylor Run/Duke Street Small Area Plan section of the City; and
2. The proposed amendment is generally consistent with the overall goals and objectives of the 1992 Master Plan and with the specific goals and objectives set forth in the Taylor Run/Duke Street Small Area Plan chapter of the 1992 Master Plan; and
3. The proposed amendment shows the Planning Commission's long-range recommendations for the general development of the Taylor Run/Duke Street Small Area Plan; and
4. Based on the foregoing findings and all other facts and circumstances of which the Planning Commission may properly take notice in making and adopting a master plan for the City of Alexandria, adoption of the amendment to the Taylor Run/Duke Street Small Area Plan chapter of the 1992 Master Plan will, in accordance with present and probable future needs and resources, best promote the health, safety, morals, order, convenience, prosperity and general welfare of the residents of the City;

RESOLUTION NO. MPA 2003-0010
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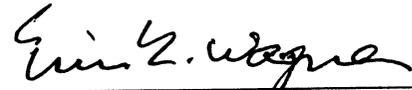
NOW, THEREFORE, BE IT RESOLVED by the Planning Commission of the City of Alexandria that:

1. The following amendment is hereby adopted in its entirety as an amendment to the Taylor Run/Duke Street Small Area Plan chapter of the 1992 Master Plan of the City of Alexandria, Virginia in accordance with Section 9.05 of the Charter of the City of Alexandria, Virginia:

Change the designation of parcels at 3330 King Street
from Parks and Open Space to Institutional.

2. This resolution shall be signed by the Chairman of the Planning Commission and attested by its secretary, and a true copy of this resolution forwarded and certified to the City Council.

ADOPTED the 6th day of January, 2003.



Eric Wagner, Chairman
Alexandria Planning Commission

ATTEST:


Eileen P. Fogarty, Secretary

Docket Item # 16
DEVELOPMENT SUP # 2003-0044
T.C. WILLIAMS HIGH SCHOOL

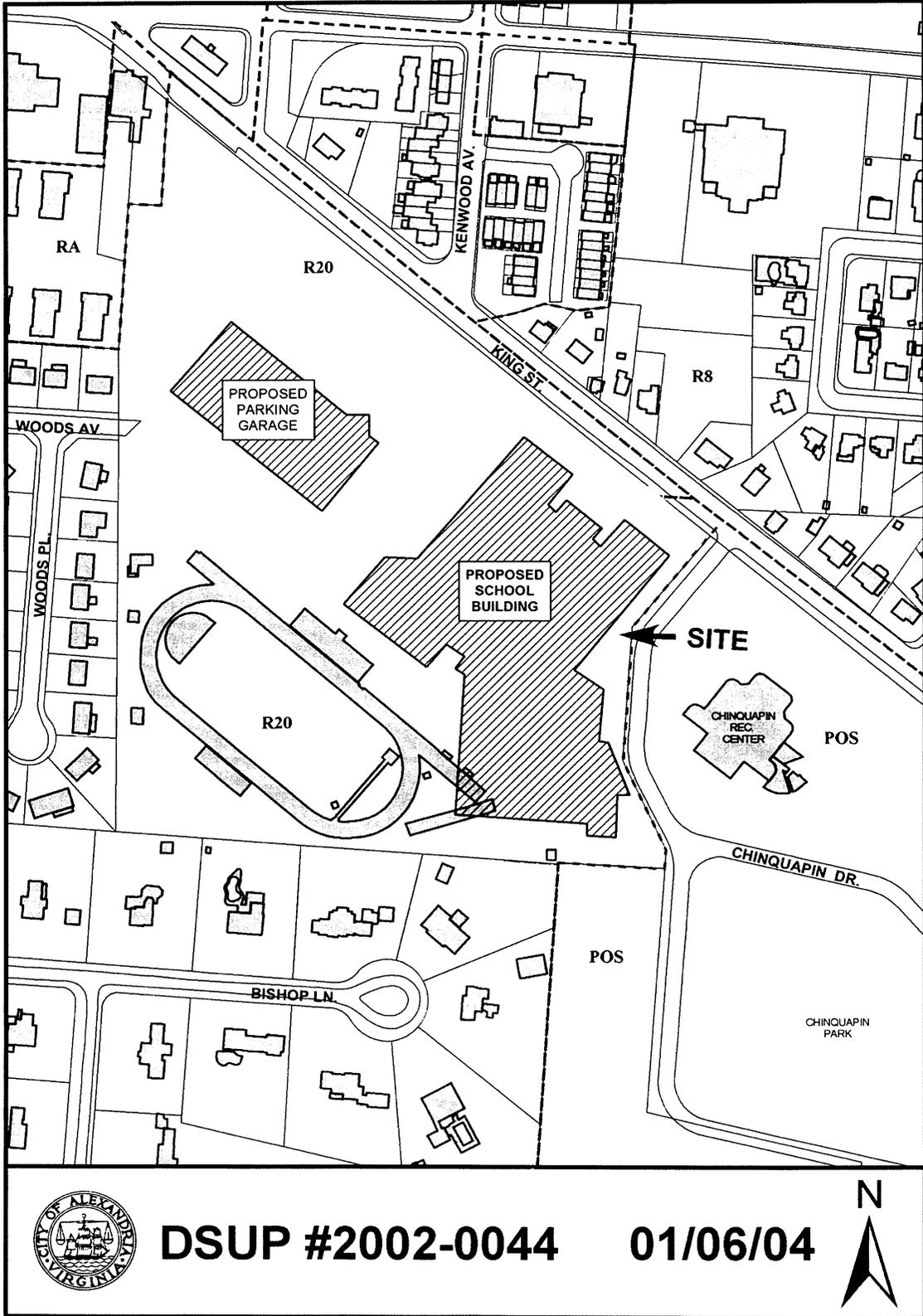
Planning Commission Meeting
January 6, 2004

ISSUE: Consideration of a request for a development special use permit, with site plan, subdivision, and site plan modifications, for construction of a new high school which exceeds the FAR and height in the R-20 zone.

APPLICANT: Alexandria City Public Schools, by J. Howard Middleton, attorney

LOCATION: 3330 King Street

ZONE: R-20/Residential zone
POS/Public Open Space zone



DSUP #2002-0044

01/06/04



I. BACKGROUND

A. T.C. WILLIAMS HIGH SCHOOL SITE

1. Land Parcels

At the geographical center of the City is a 51-acre site that includes T. C. Williams High School, Chinquapin Park, and Chinquapin Recreation Center. This complex of open space and buildings includes schools, parks, and recreational and community facilities, and comprises the largest single area in the City devoted to civic use. It is fitting that the City's one high school be located there.

The school's 28-acre portion of the site consists of two separate parcels: a 19.8-acre tract, where the existing T. C. Williams building, the stadium, and most of the Career Tech Wing and parking lots are located; and an adjacent 8.5-acre tract of open space, which forms an "L"-shaped land area along Chinquapin Drive and includes the remainder of the Career Tech Wing, temporary classroom trailers, and additional playing fields. The school property is separated from the City's park lands by Chinquapin Drive.



School Owned Parcels

2. History of the Site

Since 1965, T.C. Williams High School, named for the ACPS Superintendent who retired in 1963, has been the defining landmark at King Street and Quaker Lane. But before the school existed, this triangle of land was defined by the Chinquapin Village government housing development and the African-American neighborhood known as Mudtown.

Chinquapin Village. The Chinquapin Village development was built in 1941 by the federal government as housing for Alexandria's torpedo factory workers, occupying part of what is now Chinquapin Park. After World War II, Chinquapin Village was purchased by the Alexandria Redevelopment and Housing Authority, providing housing for war veterans and later becoming the City's first subsidized housing development.

Mudtown. Neighboring Chinquapin Village to the west was the neighborhood of Mudtown, or Macedonia as its residents knew it. Mudtown formed circa 1900 on the City's western edge as an outgrowth of the limited availability of housing options in Alexandria for African-Americans. Nicknamed for its muddy, undedicated access lanes, Mudtown had formed before most building codes were in place and had no sewers, paved streets, or fire hydrants. In order to accommodate a new high school there, residents of Mudtown were relocated, many to new houses built adjacent to the school site as part of one of the country's first urban renewal projects.

Chinquapin Park. In 1965, the City's Department of Planning began to design Chinquapin Park as a reuse of the Chinquapin Village land. The final tract of land for the park included Chinquapin Village plus adjacent parcels acquired by the City between 1968 and 1971. Efforts to expand T.C. Williams High School into the park were prevented by the City Council in 1976, in order to preserve open space.

3. Land Use Context

The large, triangular-shaped T. C. Williams/Chinquapin site is surrounded by a variety of uses, primarily residential, with single-family houses, townhomes, and apartments adjacent to the school site. A number of institutional uses are also in the vicinity. The Oakland Baptist Church and First Baptist Church sites are located on King Street, adjacent to the site; and nearby, on Braddock Road, are the Everly Wheatley Funeral Home, Scottish Rite Temple, and Blessed Sacrament church.

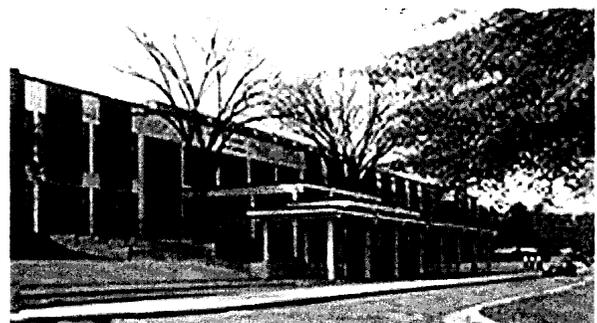
Perhaps the most defining characteristic of the site's land use context is its ideally centralized location on King Street, close to Braddock Road and Quaker Lane. These major arteries link the school to all areas of the City and provide students, staff, and citizens with easy access to the school and the adjacent complex of recreational and community facilities. The site is also close to several light commercial centers, which include a variety of uses – doctors' offices, gas stations, dry cleaners, garden nurseries, grocery stores, restaurants, and other small retail outlets.

B. EXISTING T.C. WILLIAMS FACILITY

The T.C. Williams complex was constructed in 1965 (Site Plan #63-086); the Career Tech Wing was added in 1976 and the Sports Barn in 1982.

1. General School Program

T.C. Williams High School serves grades 10 through 12 and had an enrollment of 2,037 students and 192 teachers as of September 2003. T.C. Williams currently offers a curriculum of 188 academic courses, as well as offering Career and Technology education programs such as business, marketing, health occupations, trade, technology, and work and family studies. The



Existing School Entrance

school is the site for the Secondary Training and Education Program (STEP), an alternative education center designed to meet the academic and social needs of 125 secondary school students who may not be achieving success in the regular school programs. The City's adult education program is also based at T.C. Williams, with approximately 585 students utilizing the school's facilities after hours.

Students at T.C. Williams participate in 17 interscholastic sports, with approximately 25 percent of students involved and making use of the athletic facilities and fields on the site. There are also more than 55 clubs and organizations in which students participate.

2. Land Use and Building

The existing school complex consists of a number of connected structures. The main building is a two-story, 355,000 square-foot concrete-frame building with brick infill panels and a prominent, canopied main entrance in the center of the King Street frontage. The main building is situated about 250 feet back from King Street and measures about 525 feet in length. The school has a number of other attached structures, including the music pavilion on the west side of the building, the vocational training wing on the east, and the athletic field house extending southward toward the stadium.

The building complex is situated close to Woods Avenue on the west, and set back approximately 200 feet from Chinquapin Drive. Many mature trees grow in front of the building, within the curving, symmetrical vehicular drop-off drive, which serves as the principal bus loading zone and a parking area for 340 cars. Service to the school is through an access point on Woods Avenue, on the west side of the building. The stadium, with its bleachers, main playing field, and 440-yard track, are to the southwest of the main building, as are tennis courts and other playing areas.

ACPS has determined that a new school is warranted because the T.C. Williams facility has grown antiquated. Its classroom facilities are proving inadequate, its building design no longer affords the institution of innovative educational techniques, and its infrastructure does not permit the implementation of the technological advancements required for the education of today's high school students. The new high school building will address these deficiencies, allowing for increases in student enrollment and enabling ACPS to stay in step with progressive education. (See Attachment 3 for ACPS' assessment of the need for a new school.)

3. Recreational Facilities

The existing school complex features athletic facilities for its students, including several playing fields and the City's only football stadium. The school complex is also an important recreational facility for the community, used regularly by the City's Department of Recreation, Parks and Cultural Activities (RPCA) for summer and vacation camps, spring/fall youth soccer games and practice, and summer football camp. An annual citywide cheerleading event and the Hershey Track and Field event are also held at the school.

The school's sports barn is used by youth soccer, youth baseball, youth basketball, and other organizations, and the neighboring residential community uses the stadium track for exercise. A large community park and public facility also occupy the T.C. Williams/Chinquapin site.

4. Additional Community Uses

ACPS identifies the following groups as regular users of the school building:

- *Alexandria Harmonizers*
- *Chris Collins Youth Talent Conference*
- *Alexandria Singers*
- *Rep. Jim Moran Womens Conference*
- *Campagna Center*
- *Delta Sorority*
- *Port City Playhouse*
- *Yohnex Sports Body Building Competition*
- *City of Alexandria Housing Fair*
- *Alexandria Business and Professional Women's Association*
- *Crowe Dream Builders*
- *Pro-Bolivia Committee*
- *Various church services*

The school building is also used widely by the community for City-sponsored events such as Student Career Day and the Home Ownership Fair, and instructional programs such as Continuing Education. A number of plays, musical performances, and other cultural events are held in the school auditorium.

C. PROCESS FOR PLANNING A NEW SCHOOL

When ACPS determined that the physical facility of the current T.C. Williams High School would limit the ability of ACPS to continue to provide a superior educational experience, it began a lengthy, collaborative process by which it arrived at the current proposal for a new high school. Beginning in 2001, ACPS met with a variety of interested educational, community, and City groups to outline issues and options, and in late 2001, architectural consultants were hired to add expert guidance to the planning process.

1. Location of new school

An early issue in the planning process was whether to use the existing T.C. Williams site for the new school or, in conjunction with the City, use the Chinquapin Park site in for a new school and create a new City park on the existing school site. After a September 2002 joint work session of the School Board, the City Council, the Planning Commission, and the Parks and Recreation Commission, and after a Town Meeting on September 19, 2002, the School Board unanimously approved the location of the new building on the its existing site.

For a more detailed chronology of the planning for the new school, along with several early site options that were considered, see Attachment 4.

2. Joint Recreational Use

Another issue central to the planning effort was designing the new school to maximize its joint function with the adjacent Chinquapin Park and Recreation Center. Starting in late 2002, ACPS, the City, and their respective architects began collaborating with the Parks and Recreation Commission, the Commission on Aging, and City Council representatives towards this goal. The resultant T. C. Williams building design places all athletic and recreation functions, including the gymnasium and locker room facilities, in the southeastern corner of the school plant, adjacent to Chinquapin Park and Recreation Center.

The group continues to work together on the potential future uses for a renovated Chinquapin Recreation Center.

II. PROJECT DESCRIPTION



King Street/Western Approach

ACPS describes the new T.C. Williams as a state-of-the-art high school housed in an architecturally attractive building that will meet the educational needs of the community. The new school is designed to accommodate 2500 students and 300 staff members. Building plans indicate a facility with approximately 460,000 square feet of floor space, consisting of a core academic area with approximately 100 classrooms; a media center and communications cluster; a visual arts area; performing arts classrooms and a 1200-seat auditorium; physical education and athletics areas, including three gymnasiums; a career technical education center; a student center; a building and ground services area; and an administration area.

The existing track and football field will remain in their current locations.

The school includes a large new kitchen, which will serve the other ACPS schools, and a cafeteria of sufficient size to allow a closed campus for students. Other facilities at the school include the STEP program, automobile shop, and a day care center for students' children.

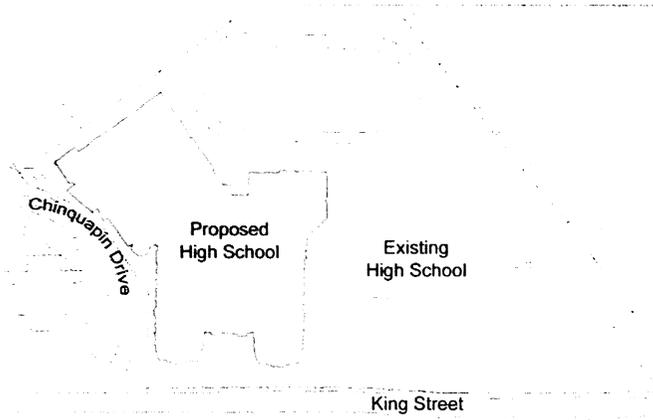
The school will be served by a 416 space parking garage on the west part of the site.

A landscape plan is included in the proposal with the goal of providing an attractive frontage along King Street and a buffer for the adjacent homes.

Designing a building with these and other features on the land available, and making it attractive, convenient, and compatible with neighboring uses, has been a challenging but exciting endeavor.

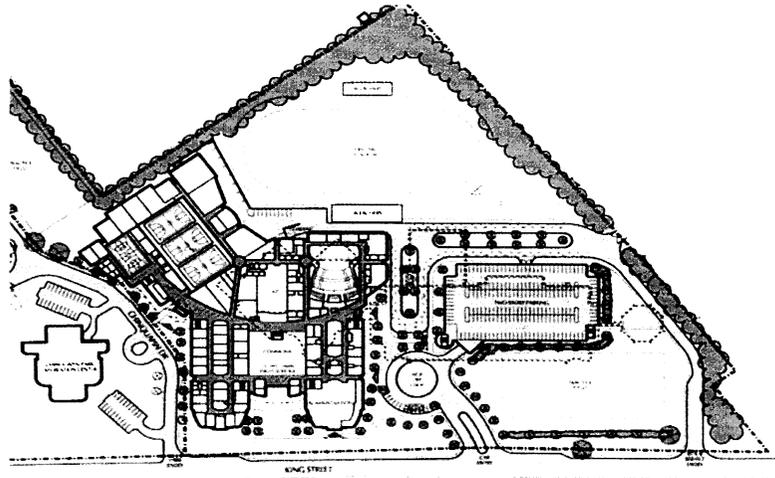
A. SITE DESIGN

The decision to locate the new T.C. Williams school building at the site of the existing school, combined with the need to maintain the current high school building for its students during the four years of construction, results in a constrained land area on which to construct a large facility.



Proposed (left) and Existing (right) Buildings

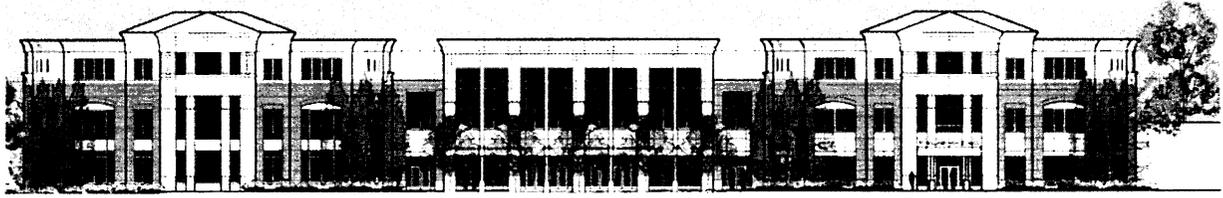
After the new high school building is completed, the existing school will be demolished. The west part of the site will then be developed with a two-story parking garage and a new playing field.



Proposed T.C. Williams Site Plan

B. SCHOOL BUILDING ORGANIZATION

The proposed high school building presents a 370 feet King Street facade, and features a pair of three-story pavilions which make the building's main impression and provide five academic "houses," or subdivisions, within the school. One of the pavilions acts as the ceremonial entrance for pedestrians from King Street.



Proposed T.C. Williams Front Elevation

The front part of the building also includes the administration wing and the media center on the second floor. A large central cafeteria, including a “commons” area outside the building, connects the two academic wings, and a 60-foot recess between the wings provides a visual break in the building facade.

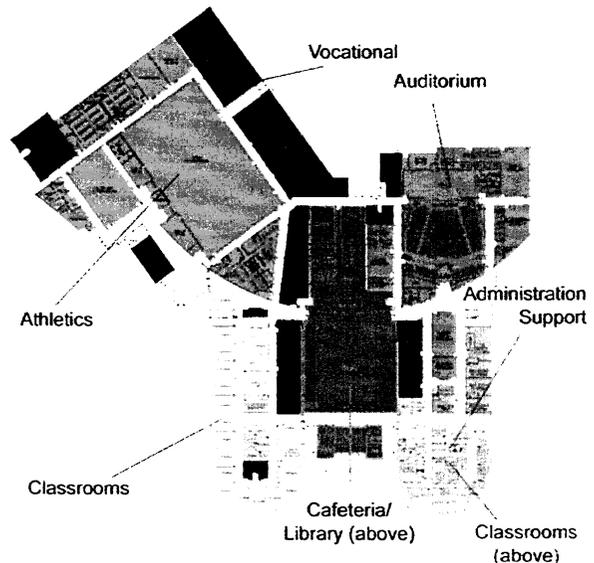
The height of the building varies dramatically at different locations, depending on the function. Although the academic pavilions are three stories tall, many of the building operations and assembly functions (including the auditorium, athletic area, cafeteria, and service areas) are on the first floor and are connected by a circulation spine—or “main street”—that runs through the building from west to east.

The auditorium is located behind the administrative offices in the southwest corner of the building. Opposite it in the southeast corner of the building are the large athletic spaces, including three gymnasiums and locker rooms. The gyms and the auditorium are necessarily higher internal spaces and occupy the equivalent of two stories.

The remainder of the first floor includes the vocational training areas and performing arts classrooms, as well as service areas for the entire complex, including the delivery dock, kitchen, and mechanical areas.

The athletic wing has a fully developed entrance on Chinquapin Drive which also serves as one end of the circulation corridor; the other end of the internal street is the main student entry at the arrival plaza and is directly adjacent to the auditorium.

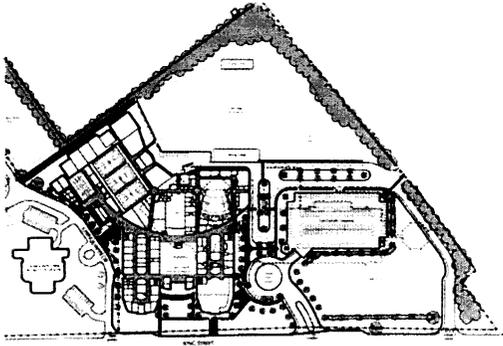
Detailed floor plans are included at Attachment 5.



Proposed General Floor Plan

C. PEDESTRIAN CIRCULATION

Pedestrian access to the building site is proposed as a system of sidewalks connected to the King Street sidewalk, the central arrival plaza, and throughout the campus. The exterior entrances connect to the internal circulation corridor, providing a connection between the garage and the auditorium or gymnasium.



Pedestrian Circulation (in red)

D. PARKING

One early decision in the design work for the new school was to provide a parking garage rather than surface parking, in order to accommodate the large number of requisite parking spaces without using more land area than necessary. The open space preserved by the garage is significant – the school’s engineer calculates that to achieve the same number of garage spaces on a surface lot would require 178 percent of the surface land area needed for the garage.

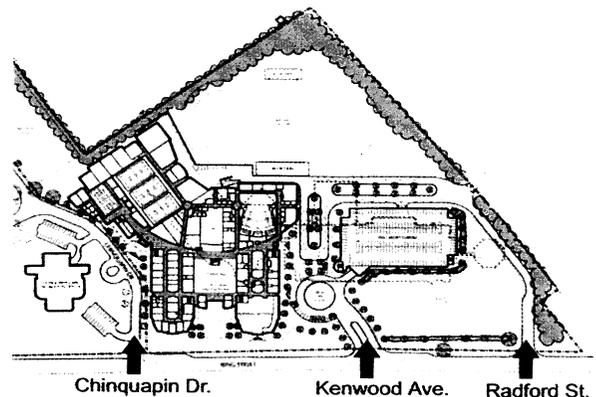
The school site includes a total of 431 parking spaces. In addition to the parking garage, which includes 416 parking spaces for students and staff, the site has two small surface parking areas with specific designations: eight visitor spaces are located near the front entrance; seven spaces behind the cafeteria and loading area are available for service staff.

E. VEHICULAR CIRCULATION

There are three main vehicular entries proposed for the new school.

1. Kenwood Avenue

The principal vehicular entry for parents, students, and staff will be from the existing intersection of King Street and Kenwood Avenue. Kenwood Avenue will extend south into the site in a split driveway, and provide access to the parking garage, the surface visitor parking spaces, and the main visitor entrance to the building. The extension ends in an entry court, which will serve as a major drop-off and pick-up location for students and as the main student entrance.



Vehicular Entries

2. Chinquapin Drive

A second means of vehicular entry to the site is on the east side of the high school property, from the existing Chinquapin Drive, which currently provides access to the Chinquapin Drive loop and the Chinquapin Recreation Center. There are several points of entry along this side of the building, most notably the proposed drop-off and plaza area at the connection between the academic and athletic wings.

Further south on Chinquapin Drive there is a proposed turn-around, to provide an opportunity for cars to discharge or collect passengers and to leave the site again without continuing around the Chinquapin Loop or entering the recreation center property.

3. Radford Street

An additional entrance will be located at Radford Street, and will be restricted to school buses and service vehicles driving to the bus loop behind the parking garage or the service area behind the school. The separation of buses and trucks from private vehicles on the school site is intentional, and an important safety element of the proposed site plan.

III. ZONING AND LAND USE APPROVALS

ACPS is requesting approval of a development special use permit, with a site plan, for the new T.C. Williams High School complex. In a separate case, the applicant seeks a rezoning and master plan amendment for some of the land on which the new school will be built. See MPA #2003-10 and REZ#2003-06.

A. ZONING

The main school site is zoned R-20/Residential, similar to the single-family land areas to the south and west of the school. Public schools are a permitted use within the R-20 zone.

However, locating the new school between the existing building and Chinquapin Drive has required that the building site include a 3.8-acre parcel of land now zoned POS/Parks and Open Space and used currently for parking and classroom trailers. ACPS is requesting a rezoning of the parcel to R-20, to allow construction and to consolidate the parcel with the main school site.

ACPS also proposes to subdivide the rezoned parcel from the remaining ACPS land, which will retain its POS zoning and continue its use as parks and open space.

B. REQUIRED SPECIAL USE PERMIT AND ZONING MODIFICATION APPROVALS

Special use permit approval is required for the new school building because the floor area ratio (FAR) and height of the building exceed the amount allowed in the R-20 zone. In addition, the building requires approval of modifications of the required zoning setbacks at the front of the new building along King Street and on the east side of the building next to Chinquapin Drive. SUP approval is also required for temporary classroom trailers, which will be used during construction, and for the main T. C. Williams High School freestanding sign, to be located on King Street.

1. FAR

If the 3.8-acre parcel of POS land along Chinquapin Drive is changed to R-20, the FAR of proposed school building and covered floor of the parking garage will be .51. The R-20 zoning permits a .25 FAR, but public schools may achieve up to a .6 FAR with special use permit approval.

2. Height

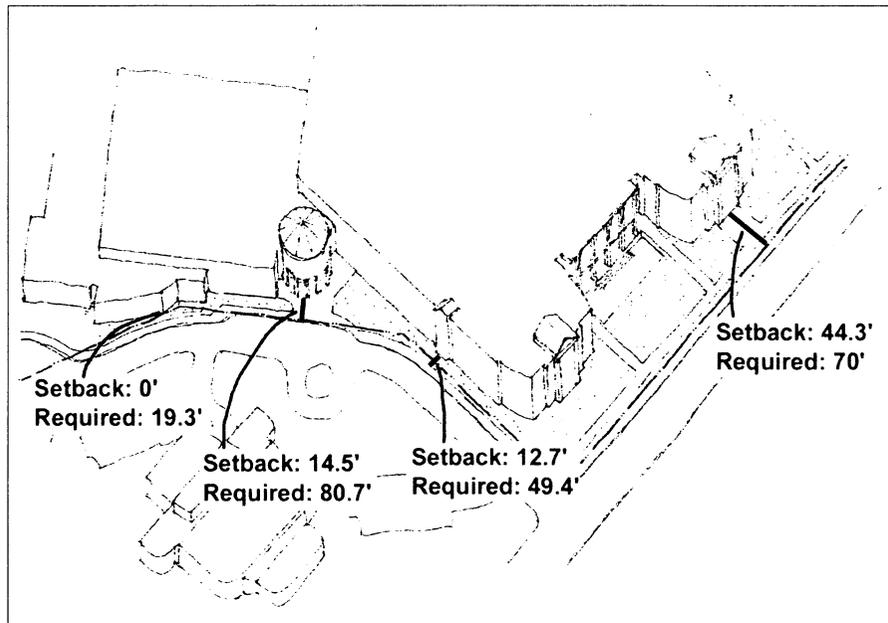
Under R-20 zoning, the allowable maximum height for a school building is 40 feet; with a special use permit, a height of up to 60 feet is allowed. To accommodate the school's programming needs, the building is three stories high, with a maximum height of 52.8 feet at the planetarium near the center of the school. The applicant is therefore requesting a height increase of 12.8 feet under this SUP.

3. Front Setback

The school building projects toward King Street to varying degrees, with a portion of the main building as close as 44.3 feet, instead of the 70 feet required by the R-20 zone. The zoning ordinance permits a modification of the setback rule where warranted. The applicant is requesting approval of a 26-foot modification of the front yard requirement.

4. Side Setback

On the eastern edge of the site, the building is very close to Chinquapin Drive and the Chinquapin Park property line. The distance here also varies, but at one point the setback is zero. A modification is necessary of the required ratio of one foot of setback for each foot of height at each point.



Front and Side Setbacks

5. Temporary Classroom Trailers

The new high school will require significant relocation of classrooms during construction. Because the zoning ordinance requires an SUP for approval of temporary classroom trailers, ACPS has included a request in this application for the 25 to 40 trailers it proposes to place on the stadium field during construction.

6. School Sign

The zoning ordinance requires SUP approval for any freestanding sign at a school. The new T.C. Williams High School plan includes a proposal for a freestanding sign near the school's entrance along King Street.

**T.C. Williams High School
SUMMARY OF PROPOSED DEVELOPMENT***

Property Address:	3330 King Street
Total Site Area:	28.3 acres
Zone:	R-20 (23.7 acres) and POS (4.62 acres)
Current Use:	Public High School
Proposed Use:	Public High School

	<u>Permitted/Required</u>	<u>Proposed</u>
Floor Area	258,093 sf/619,423 sf w/SUP	547,000 square feet
FAR	.25/.6 with SUP	.51**
Yards		
Side	25 ft. (1:1)	0 ft.
Rear	25 ft. (1:1)	42.2 ft.
Front	70 ft.	44.3 ft.
Height	40 ft/60 ft. with SUP	53 ft.**
Parking	250 (1 space/10 students)	444

* Zoning shown is based on rezoning approval

**SUP required for increased FAR and height

IV. ANALYSIS

Staff supports this proposal for a new high school, including the requisite zoning changes and approvals. Over the last several months, City staff, ACPS staff and their consultants have worked together extensively to address the significant issues involved in this complex undertaking and, in addition, have worked with the community to address their concerns. The goal has been to find ways to design a school facility that is functionally sound and is appropriate for its context on the site, as a prominent feature in the neighborhood, and as an important civic building for Alexandria.

The following major issues have been City staff's focus.

- **Site and building design**
 - Maximizing Open Space
 - Creating a Strong Streetscape Along King Street
 - Making Pedestrian Spaces
 - Enhancing the Architectural Design
 - Landscaping as a Site Feature
- **Parking and transportation management**
- **Neighbors' concerns**
- **Special features of the new school**
 - Green Building Technology
 - Security
 - ADA Access
 - Public Art
- **Construction Phasing**

These and the other issues that have attended the development of this site plan and building project are each discussed below.

A. SITE AND BUILDING DESIGN

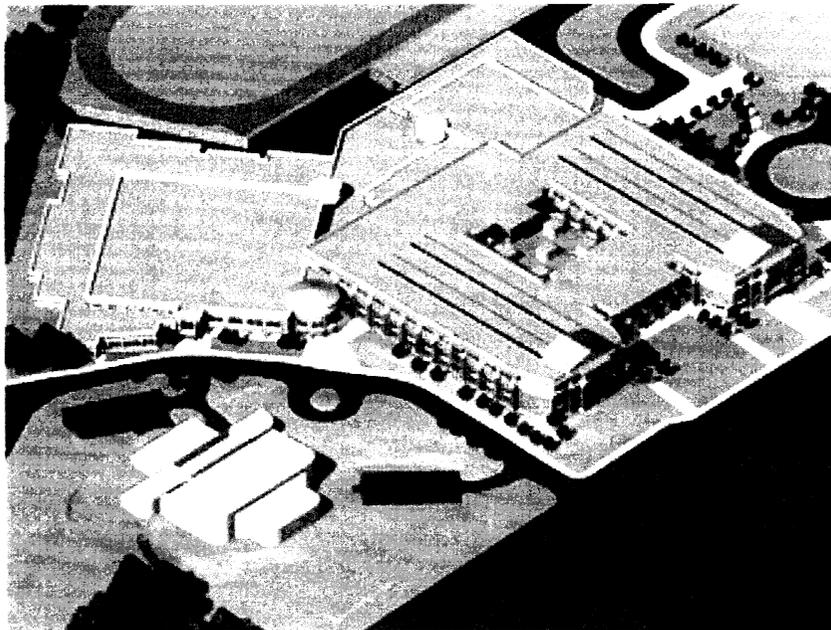
The building's location on the school site results in a very large building that is located very close to King Street and Chinquapin Drive. Therefore, significant time and effort has been spent attempting to ensure the most appropriate building and grounds possible on a site that functions well for both the users of the school and the community at large.

The proposed building is large – almost 500, 000 square feet – with a 370-foot long facade along King Street. The western face of the building extends about 490 feet toward the stadium area. The east side of the building along Chinquapin Drive is about 625 feet in length; it extends roughly 275 feet to where it angles directly south another 340 feet. The rear building elevation is 320 feet long, and the portions of the building facing the stadium are 280 feet (facing due west) and 265 feet long (facing southwest).

The height of the building varies dramatically at different locations, depending on the function. The academic pavilions along King Street are three stories tall, but there are two story spaces, including the gym and auditorium behind the pavilions and one story portions of the building in the rear. The height of the building along its southern perimeter is 19.3 feet.

The typical height of the three-story volume of the building is about 45 feet from the average grade around the building. However, due to the slope of the site, the building appears taller from the northeast and shorter from the southwest. Along King Street at the Chinquapin Drive entrance where the building roof is higher, the building height is 58.6 feet above the grade, but that does not include the 12 feet of additional berming to be installed above sidewalk level.

In addition, the building is located extremely close to King Street and Chinquapin Drive. Although the setbacks on each frontage vary, the King Street setback area is reduced to 44 feet at one location and, on Chinquapin, there is a point with no setback at all. By comparison, the existing high school building is setback approximately 250 feet from King Street and the area in front of the school contains a number of mature trees that now screen the building.

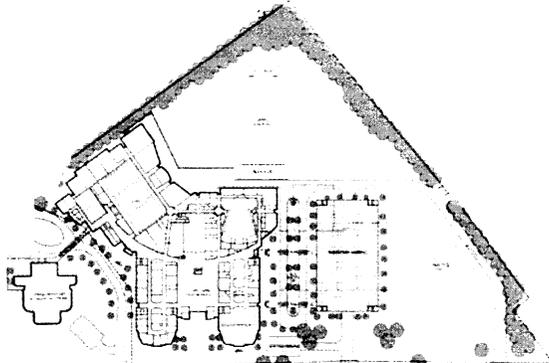


Aerial View from the East

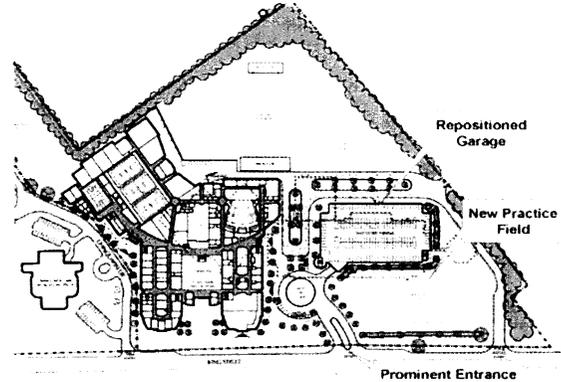
The challenge in this case has been to create a building and a setting that reduce the visual intrusion of such a large school building, and make it and the school grounds an attractive location, compatible with its surroundings. Specifically, staff has worked to balance the large size of the building by maximizing open spaces, creating a strong King Street frontage, and enhancing pedestrian areas and landscaped spaces; it has attempted to minimize the perceived mass of the building by working with the schools' architects to refine the building's design.

1. Maximizing Open Space

Given the mass and scale of the proposed building, its location close to King Street, and the loss of so many mature shade trees from the front of the school, staff reviewed the site itself to find ways to maximize open spaces, green areas and other outdoor spaces. When an initial concept plan was submitted to the City's land use process for consideration, planning staff worked with the architects for ACPS to redesign the site significantly in order to achieve additional open space and a more prominent entryway, consistent with the importance of the building.



Original Site Plan



Site Plan with Staff Changes

In addition to creating a large, central and visible entry for vehicles as well as pedestrians, the change in orientation of the parking garage has created open space opportunities. With the parking garage located 250 feet back from King Street on the western side of the site, space is available for a new playing field. The addition of a field fills a critical need for the school and City, and its location on the site creates an open space buffer between the street and the parking garage.

Matched by the large open space of Chinquapin Park on the east side of the proposed school, the two buffer areas begin to balance the mass of the large school building.

2. Creating a Strong Streetscape along King Street

The building's location on King Street gives it a prominent address and central location in the community. Consistent with its important and visible location, significant effort has been placed on the physical development of the King Street frontage to create a formal presence and to soften the appearance of the school and parking garage. An additional concern is the loss of the many existing mature trees located now between King Street and the existing school building. Several elements of the proposed site plan coupled with staff's conditions address these issues.

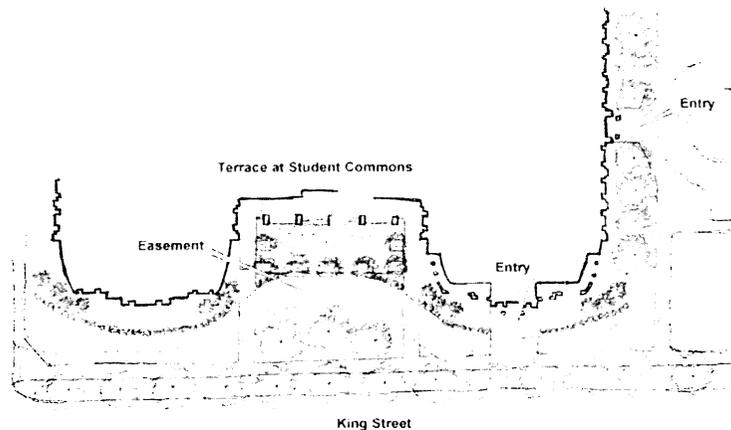
The proposed landscape treatment for the King Street frontage is intended to be a consistent but relaxed design as a foreground to the entire high school site. In order to create a green edge to the site's 1300-foot frontage, and to buffer the tall school building, the proposed site plan includes a typical City standard of street trees – willow oaks in this case – along the King Street

right-of-way, as well as a ten-foot sidewalk for pedestrians. Inside the property, the main landscape design idea is to create a buffer of curving evergreen hedgerows intermittently in front of the school as well as in front of the practice field on the west. These curving hedge elements are proposed to be complimented by other less formal vegetation, particularly on the west side of the entry drive.

Together, this landscaping provides buffer to the property from King Street traffic, and screens views of school activity from the street.

In addition, in order to maximize green area, staff has included a condition requiring that the driveway entrances be reduced to the minimum width required for safety.

Staff has also worked extensively with schools staff regarding the setback area immediately in front of the school building along King Street.



King Street Setback

The setback area in front of the school has been a challenge in the site plan design work, because it functions as both a green lawn area and a location required to be structurally strong enough to withstand the weight of emergency vehicles. ACPS proposes to treat the surface area here with a structured grass material, but there is concern about whether, given the slopes from west to east in the front of the school, the product will withstand such a function over time. ACPS staff has indicated its intent and commitment to a green alternative, including the addition of a retaining wall if necessary – a less desirable feature, but one that would allow reducing grades and ensure that a structured turf product would endure. The alternative, that the area required for emergency vehicles be paved, is an unacceptable solution for the school’s front lawn.

The school’s commitment that the area be green is critical, and will be supplemented with additional landscaping outside the emergency access area, so that the area in front of the building

will be as pleasant and graceful as possible. The green area in front of the building will then relate to the large open area west of Kenwood Avenue, with extensive landscaping to be planted there, and the planting of formal street trees along the entire frontage, creating a connected series of green spaces along King Street.

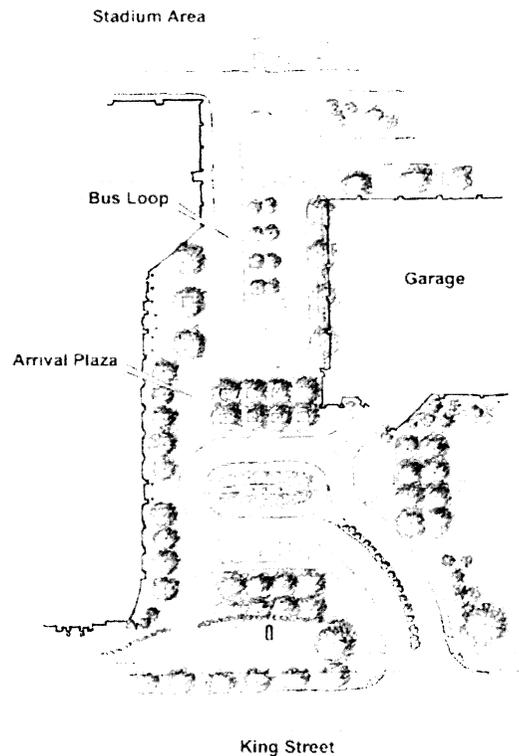
3. Making Pedestrian Spaces

Working with City staff, the consultants for ACPS have agreed to incorporate several features in the proposed design to enhance the pedestrian environment at the school. In addition to the broad sidewalk with street trees along King Street and sidewalk areas throughout the campus, the plan includes the following special pedestrian areas:

Entry plaza. Although the school has a formal pedestrian entrance on King Street, there is also a large plaza entrance area between the western facade of the building and the parking garage.

This plaza is an important element of the school, from both a functional and design standpoint, serving as the main entrance and arrival point on the western side of the building. It acts as a pedestrian connection and a natural gathering place for students, by nature of its location at the south end of the main driveway entrance and at the north end of the school bus drop-off area and pedestrian route to the stadium.

Landscaped with plantings and trees, and paved with decorative hardscape, the entry plaza should become an important feature of the outdoor space at the school complex.



Main Entry Plaza

Rear pedestrian way. In the original site plan, the school building extended to the rear of the property, with no opportunity to walk between Chinquapin and the school stadium, except along King Street. Given the extensive use of the adjacent area for physical education, athletic events, and community leisure activities, City staff recommended, and ACPS agreed, to change the design of the rear of the building to include an attractive walkway.

The proposed walkway will extend behind the athletic wing of the school. At 40 feet in width, it will be wide enough to provide an attractive pedestrian addition to the site and will be closed to vehicles except for emergency purposes.

Emphasis on pedestrian sidewalk areas. The proposed design includes enhancements to the proposed site plan, including wider sidewalks on the north south axis between the central entry plaza and the stadium area, an additional exit point from the garage for easy access to the stadium, and increased green area with reduced pavement for vehicles.

In addition, staff is recommending that there be stamped crosswalks at each vehicular entrance on King Street, and stamped crosswalks and pedestrian countdown signs at the King Street and Kenwood Street intersection. Staff is also recommending that the schools work with City staff in the final site plan phase in order to achieve even greater sensitivity to pedestrians walking through the site, and from any one point, such as the parking garage, to another.

Decreased drive areas. While there have been serious efforts to increase the size and functions of open space and pedestrian areas on the site, the proposed site plan includes some paved areas for vehicles that are not essential. Staff has included a condition requiring that roadways and other unnecessary paved areas be reduced to their minimum required sizes.

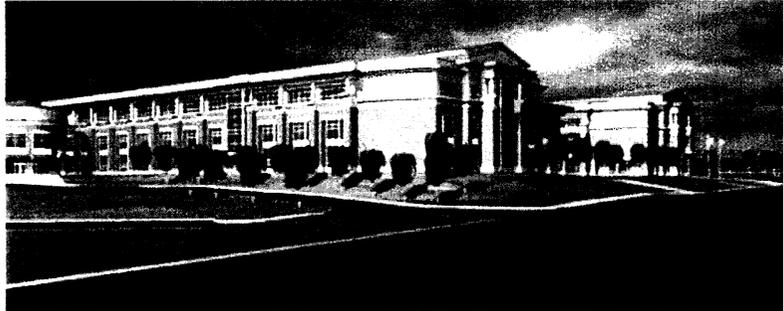
3. Enhancing Architectural Design

A primary issue raised by staff from the outset was that the building is too massive for the site given the adjacent uses, the scale of the required setbacks, and the space requirements for circulation, parking, and athletics. In addition, there was a concern that the City's one public high school appear dignified, well-proportioned, and built of high-quality materials in order to create a civic building appropriate to the context, traditions, and aspirations of Alexandria.

Staff met with the applicants numerous times to advance and refine the building's architectural design. The approach recommended by staff to treat the scale of the building was twofold: *to break up the perceived mass with recesses and projections; and to include architectural details that connote a smaller visual scale.*

Two projecting pavilions. The two academic pavilions on King Street are treated as pedimented fronts with hipped roofs; their width on the end faces of the academic wings helps to reduce the apparent breadth of the wings. The effect of the design is dignified and is derivative of traditional architecture without being literally neo-classical. The student commons/media center element is more transparent, with larger areas of glass and metal canopies – making a counterpoint to the more traditional treatment of the twin pavilions. The commons area is severely recessed, helping to break the King Street frontage of the building.

Articulation. The basic skin of the building is made up of bays corresponding to the rhythm of the classrooms behind them, and is treated as a traditional brick masonry building. The first and second stories of the building are treated together with large two-story arched openings between buttress-like piers, with precast concrete sill courses and accents. The third level of the building is clad in a lighter accent color of brick, to help the building appear less heavy at the top. There is a light-colored metal cornice which wraps around the top level of the building and is supported by a pair of columns on top of the two-story piers.



Proposed T.C. Williams (from the east)

The least articulated skin of the project occurs on the garage, where the brick piers and arched openings of the main building are recalled in a simple way.

Special entry elements. In addition to the pavilions facing King Street, there are numerous special entry elements around the perimeter of the building which are used to interrupt the mass of each facade. Several minor projecting bays on both the west and east face of the building project slightly from its face and, with their angled roofs, break the roof line to create a varied profile in the mass of the building. In addition, the major drum-shaped element on the Chinquapin Drive facade provides a transition on the east side of the building between the three-story academic block and the lower scale of the athletic wing. Located at one end of the “main street” internal circulation spine, this major feature provides relief along the longest building wall.

4. Landscaping as a Site Feature

The applicant was encouraged from the beginning of the site design process to develop a strategy for the treatment of the landscape that would help the project harmonize with its context. In addition to the King Street frontage, discussed above, the major landscape elements now proposed by the applicant focus on entry conditions and gathering areas adjacent to the building; foundation and other plantings, intended to soften the appearance of the building; screening for the parking garage; and various areas of landscape buffer around the perimeter of the site.

As part of the school’s commitment to environmental responsibility, the proposed landscaping employs a palette of native plant materials that are more likely to survive local climate conditions including periods of drought without irrigation.

The following landscaped areas are of particular note:

Bus Loop/Rain Garden. The western portion of the bus loop is lined with ash trees, and as part of the environmental program, is proposed to contain a “rain garden,” or naturalistic wetland-type detention area for stormwater runoff.

Garage screening. The parking garage presents a 375 foot long facade to King Street. Although setback 250 feet from the street, and a portion of it located beneath the grade, without landscaping to screen the structure, it would present a long, unrelenting wall to the street. Therefore, the northern and western faces of the garage are planted with a combination buffer of evergreen shrubs and trees of numerous varieties in an informal pattern. Staff has recommended that those plantings be augmented to ensure sufficient screening.



Parking Garage (viewed from King Street)

Landscape buffers. Finally, landscape screening is proposed along the western and southern perimeter of the site to mitigate the visual impact of the school from the adjacent residential areas at the rear of the site. Along Woods Avenue to the west, some landscape buffer currently exists but will be increased with a variety of evergreen shrubs and trees. This treatment is continued at the perimeter around the entire stadium/track area to the south adjacent to the residences located on Bishop Lane and along the western edge of the practice field at the southeastern portion of the property.

With the extensive landscaping included in the proposed plan, and the staff’s recommended conditions, along with the open spaces, architectural treatment, and enhanced outdoor pedestrian areas, staff believes ACPS has achieved the best possible complex of building and site design in the space provided.

B. PARKING

An issue that concerns the entire community is the ability of the new school complex to handle the parking demands it creates. To ensure adequate parking on site for its school use, and to minimize overflow parking which impacts the neighborhood, ACPS has committed to managing traffic to match the site's parking capacity. In addition, ACPS and City staff have reviewed extensively the existing and proposed parking needs for the new school complex, as well as methods for reducing the total number of drivers to the school.

1. Number of parking spaces

The proposed site plan for the new high school includes a total of 431 parking spaces, allocated as follows.

Garage. The first floor of the garage will have 198 spaces for staff. The second floor will have 218 spaces for students, adult student nurses, visitors, and staff).

Surface. Eight spaces will be available at the visitors' parking area. Seven spaces for staff will be available behind the kitchen and service areas.

An additional nine spaces are provided at the rear of the building for the auto mechanics program. There are 11 handicapped accessible spaces on site. 290 of the garage spaces are sized for compact cars.

The proposed number of parking spaces is more than the 250 required by the zoning ordinance. The number is also an increase over the parking at the high school today, which consists of a total of 340 parking spaces – 85 segregated for student parking and 255 spaces for staff, nurses and visitors. In addition, students are now permitted to park on the Chinguapin Drive loop road.

ACPS calculates that the number of spaces being built with the new school will result in 61 more spaces than needed to handle today's high school use, thus allowing some room for growth. ACPS also indicates that should changes occur that result in a greater need for parking than anticipated, it will act so as to limit student parking further.

Under the school's current parking policy, only students who are issued permits to park at the school or at Chinguapin may use the spaces. Permits now cost \$75.00 for a year. Although there are only 85 parking spaces on the school site for student parkers, the school issued some 118 permits for the 2003-04 school year. ACPS plans to continue its current student permit parking system when the new school is built. Only students with permits will be allowed to park on site.

2. Adult Education and Special Events

In addition to the daytime school population, the school is home to a sizeable evening adult education program, and to a series of special events that require parking. A few events each year attract significant numbers of people, such as graduation, Titan Expo, College Night, and home basketball and football games. Of special concern is the parking need that occurs on an evening

when both adult education and a popular event, such as a basketball game, occur. Parking consultants estimate that if the same ratio of growth occurs in the adult education student population as is proposed for the high school population, then 360 spaces are needed for the evening classes. There is also an estimated need for 320 parking spaces for home basketball games. While both the current and proposed high school parking plans can accommodate either one of those events, neither is sized to accommodate both occurring at the same time.

3. Parking at Chinquapin Park

There is the potential for the redesign of Chinquapin Park in the future, in which event some or all of the parking on the loop road could be lost. Currently, the loop road can accommodate approximately 225 parked cars; experience shows that about 90 of them are used by students on any given day. In addition, the loop road is used when special events occur at the school, especially in conjunction with regular adult education classes. ACPS is aware of the potential loss of parking at Chinquapin, and is fully committed to having the parking on its own site function adequately for its own uses. At the same time, ACPS and City officials have acknowledged that with the redesign and possible expansion of Chinquapin Recreation Center in the future, there may be both a need and an opportunity to create additional parking opportunities on the east part of the site for use by both the park and the T.C. Williams facility.

4. Parking study

A parking study was performed for ACPS in conjunction with its original proposal for a new high school, which determined that, if the school grew to a population of 2500 students, then many additional spaces would be required to handle the growth. The parking study's broad assumptions about the growth of faculty, staff, adult education and the student population were then refined with ACPS to include its plan to limit student permits, reduce the expansion of adult education and to control scheduling of events. The parking consultants then updated their findings (Attachment 6) to conclude that there is a need for 565 parking spaces to meet the needs of the school in the future, including 385 spaces for teachers, staff and visitors, and 180 spaces for students.

The proposed site plan only includes 431 spaces, for a deficit of 134 spaces. However, ACPS only issues 115 parking permits for students now, and parking is now available at Chinquapin Park. If in the future the parking availability at Chinquapin changes, or the student parking population increases, ACPS will have to change its parking permit system to reduce the number of student parkers. ACPS has indicated its willingness to do that.

5. Parking management plan

ACPS has devised a parking management plan to reduce the number of drivers to the campus, and to handle the drivers who do attend school and events there. The following are key components of the plan.

Limits on student permits. ACPS will issue a maximum of 180 student permits for parking in the garage. Additional permits will not be issued.

Relocation of Adult Education classes. ACPS will relocate several Adult Education programs to other facilities, decreasing the number of spaces needed in the evening. ACPS will also stagger the schedule for adult education programs. It will control the scheduling of major events in order to minimize conflicting peak crowds.

Overflow parking. ACPS will continue to use neighboring large parking lots for such special events as graduation, Titan Expo, and other events requiring overflow capacity. T. C. Williams has in the past worked successfully with both the First Baptist Church on King Street and with the Scottish Rite Temple on Braddock Road to accommodate overflow parking on an event by event basis. Both organizations and ACPS have indicated a willingness to continue that arrangement.

Alternative modes of transportation. Finally, ACPS is committed to encouraging transit use, carpooling, and other means of avoiding single vehicle trips to the school, in order to reduce the need for parking.

6. Community Parking

The current parking lots at T. C. Williams High School are used by the community generally for events, but also by neighboring uses on a regular basis. For example, members of the Oakland Baptist Church regularly park in the lot closest to the church at the far west end of the site. In addition, the school allows residents at the Quaker Lane Apartments to park overnight at the school. Because there will be ample parking on the school site at the key times needed by the church and the apartment residents, ACPS has committed to continuing to make its parking available to those users.

ACPS plans to leave the garage open at all hours, in deference to the needs of the community, so that a variety of general users, from citizens attending events at the school or Chinquapin Park to early morning joggers, can continue to use the parking facilities at the school.

Staff has reviewed the extensive material submitted by ACPS regarding parking, has discussed it with school representatives at length, and believes that the 431 parking spaces provided – which is almost 100 more than provided today, and almost 200 more than required by zoning – should be adequate and should improve parking conditions over what exists at the school today. Staff is also mindful that, given the limitations of the land area, it would be unfortunate to see additional land or building devoted to parking in this increasingly urban environment. While staff acknowledges that there will be times when the demand for parking exceeds the number of spaces on site, ACPS is committed to minimizing any conflicts for the community, by arranging for overflow parking at nearby institutional uses, and by scheduling events to minimize the need for overflow parking.

With those arrangements, and the steps required by the parking management plan (which are included as a condition), staff finds, on balance, that the parking is sufficient.

C. TRAFFIC MANAGEMENT

With any school complex, there are concerns regarding traffic to and from the facility, and the high numbers of cars and buses arriving as school opens and closes, and the use of the facility for special events. With the proposed school's three access points on King Street, it is imperative that each – and all three in combination – successfully handle the traffic that uses them.

ACPS commissioned a traffic study when it prepared its original site plan, and has since updated that study to reflect changes to the site plan during the concept review planning process with the City. While the traffic consultants have found overall that the arrangement of entry points, driveways, and parking areas will work from a traffic management standpoint, they do suggest that certain adjustments be made.

1. Kenwood Avenue

T&ES staff have insisted that the Kenwood entrance of the school be aligned with the portion of Kenwood Avenue across the intersection so that unwary drivers can maneuver the intersection crossing. In addition, there is concern that vehicles entering at Kenwood Avenue and queuing while waiting to drop off or pick up students, will conflict with cars trying to enter (morning) or exit (afternoon) the parking garage. City and school staffs have discussed this issue and there are recommendations with this approval requiring that the entrance lane be one way, that signage be posted, and that traffic personnel be hired to manage the entry loop as necessary to alleviate any traffic conflicts.

2. Chinquapin Drive

A concern about the Chinquapin Drive entrance is the fact that there is now and will continue to be a back up and delay in exiting the site for drivers heading east on King Street. Although the school's main entrance for vehicles will be at Kenwood Street, Chinquapin Drive will continue to be used for drop off and pick up, and also for event traffic to athletic events. The schools' traffic consultants suggest creating an additional lane for traffic exiting the site, but neither the City nor the schools are interested in creating additional pavement, or taking land and trees from the entrance to Chinquapin Park. If, in the future, there is a need for additional road width, the issue will have to be addressed.

3. Radford Street

There is a potential traffic problem with the access point at Radford because buses entering the site from the east, or leaving the site and turning to the west, on King Street, will have to cross King Street traffic without a traffic light. While these turning conflicts occur only at a few times during the day, they could interfere significantly with King Street traffic management. Alternatively, buses would have a difficult time entering or leaving the site.

Again, staff has discussed the issue with ACPS staff and determined that the intersection should not be signalized, and that ACPS will hire or assign traffic management personnel to manage that intersection for arriving and departing buses at peak times.

With staff's recommended conditions and enhancements at each of the three site entrances, the site appears to be capable of managing the traffic to and from the site without undue impact on King Street or the community.

D. NEIGHBORS' CONCERNS

Bordered by single family homes on two sides, and with additional residential development directly across King Street, T.C. Williams High School must coexist in harmony with its neighbors in addition to being functional and attractive. Significant time during the planning of the high school has been spent meeting with community groups, listening to their concerns, and addressing them as part of this proposal. Issues include:

- communication
- noise from the athletic fields
- activity at the athletic fields
- traffic on Woods Avenue
- lighting
- church parking

The Seminary Hill Civic Association created a task force of citizens to represent the homeowners who live adjacent to the school on Woods Avenue and Bishop Lane, and ACPS and City staff have met with the task force group many times. In addition, there have been presentations at North Ridge Civic Association, and meetings with Chapel Hill homeowners, who live in a townhouse development across King Street from the school.

1. Communication

The residents who border the school express some frustration with the history of communication between the T.C. Williams administration and ACPS regarding problems such as noise at the stadium, cut through traffic and loitering on Woods Avenue, and increasing activity levels at the athletic fields that border Bishop Lane homes. The current dialogue helps align differing perspectives and should be the beginning of a new relationship between the schools and the T. C. Williams neighbors.

The most significant result of the recent meetings with neighbors is the formation of two committees to provide a forum for discussion of ongoing issues. Specifically, ACPS will coordinate a Construction Advisory Committee to discuss issues during the three years of construction with neighbors. It will then create and coordinate a Community Advisory Committee. Each committee will include representatives of ACPS, the T.C. Williams administration, the City's Department of RP&CA, and neighbors of the school. These committees are required as conditions to this approval, but ACPS has already begun discussing and recommending solutions for the following issues with the neighbors.

2. Noise

The greatest complaint from neighbors on Bishop Lane is the noise generated from the school, including especially from the sound system or other amplified devices at the stadium and athletic fields. While neighbors understand that football and other sports and activities necessarily generate noise, they complain about unmonitored use of, for example, the stadium sound system.

ACPS has agreed to work with the neighbors on this issue, including by hiring an acoustic consultant regarding upgrades to the existing stadium sound system. ACPS has also agreed to restrict the use of the stadium sound system to sanctioned school and City recreation events, and to restrict its hours of use.

The school public address system will be designed so that there will be no exterior sound from it except for emergencies.

Another noise issue relates to the truck traffic making deliveries to the school, which must of necessity begin in the early morning. Working together, the neighbors and ACPS staff have agreed to limited hours for deliveries. Specifically, all deliveries will occur after 7:00 a.m. during the week and after 9:00 a.m. on the weekends, except for food deliveries which may occur between 6:00 and 7:00 a.m. if necessary.

Finally, ACPS has agreed to limit noise from buses and other commercial vehicles on site by restricting the time they are allowed to have their engines idling to no more than five minutes.

3. Activity Levels

There is also concern by neighbors about what they perceive to be an increased use of the stadium and practice fields, and especially for activities not associated with the schools or the City. After a series of discussions with ACPS and City staff there is agreement about the general use and hours of the fields.

4. Woods Avenue Traffic

The Woods Avenue entrance to the school is now open to vehicles and must be in order to access the service and loading areas behind the existing school building, which are located very close to that entrance. With the redesign of the school site, there is no longer a need to use the entrance, and it will be blocked with a gate prohibiting vehicular traffic in the future, except for emergency vehicles and police personnel. Pedestrians will be able to continue to use the street for access to and from Quaker Lane.

5. Lighting

There is concern among neighbors that the new school will be built with increased lighting levels, and specifically about the potential for night athletic events at the stadium. The schools have agreed that there will be no permanent lighting installed at the stadium behind the schools, or on any of the athletic fields on the site, and a condition is included in staff's recommendation reflecting that agreement. General lighting at the school for security and convenience is also the subject of several of staff's recommended conditions. ACPS will be required to submit a detailed

lighting plan as to all site lighting, and it will be available for review by neighbors. The conditions specifically address the need for a balance among security concerns, aesthetics and impacts on the neighbors. It is important for example that light fixtures be directed so as not to shine onto adjacent properties, and also so as not to create a beacon effect for the community.

6. Church Parking

The Woods Avenue neighbors, many of whom attend Oakland Baptist Church, have been concerned about their ability to continue to park on the school site. ACPS has agreed that the new parking garage will be available after regular school hours for church parking. In addition, parking will be allowed on the Radford Street service drive on Sundays for church members.

Each of the above issues, and others, are addressed either in the conditions which staff is recommending be attached to the approval of the new school, or in a separate document signed by Rebecca Perry, ACPS Superintendent, attached.

D. SPECIAL FEATURES OF THE NEW SCHOOL

The proposed new school includes a series of innovative and technologically advanced features, including:

1. Green Building Technology

The new T.C. Williams building is proposed to be “green”—meeting the certification criteria for The Leadership in Energy and Environmental Design (LEED). LEED’s Green Building Rating System represents the U.S. Green Building Council’s effort to provide a national standard for what constitutes a “green building.” Through its use as a design guideline and third-party certification tool, it aims to improve occupant well-being, environmental performance, and economic returns of buildings using established and innovative practices, standards and technologies.

Projects are evaluated using a point system, earning one or more points toward certification by meeting or exceeding each credit’s technical requirements. Points add up to a final score that relates to one of four possible levels of certification: Certified (26-32 points), Silver (33-38 points), Gold (39-51 points), and Platinum (52-69 points).

The system allots points for environmentally beneficial building materials and design in the following specific categories: Sustainable Sites; Water Efficiency; Energy and Atmosphere; Materials and Resources; Indoor Environmental Quality; and Innovation and Design Process.

Although there is no one technique for designing and building a green building to these specifications, certified buildings often preserve natural vegetation; contain no-toxic or recycled-content building materials; maintain good indoor air quality; use water and energy efficiently; conserve natural resources; feature natural lighting; include recycling facilities throughout; include access to public transportation; feature flexible interiors; and recycle construction and demolition waste.

The Alexandria City School Board voted unanimously on Thursday, April 3, 2003 to approve \$1.5 million in additional funds to support such sustainable design features in the construction of T.C. Williams High School.

“Green” features at the new high school will include a cistern that will capture and reuse rainwater that will be sanitized and then reused for toilet flushing, cooling and for irrigating athletic fields and landscaping. In addition, Moseley Architects, the designers of the new T.C. Williams High School, is incorporating more than 30 other sustainable design elements, including but not limited to:

- Erosion and sediment control
- Reduced light pollution and increased use of natural daylight
- Recycled building materials
- Water efficient landscaping
- Light colored roofs to reflect solar energy
- Using 30% less water overall through waterless urinals and other measures
- 20-40% reduction in energy usage
- Use of materials with recycled content
- Use of materials produced in a 500 mile radius to lower transportation/energy costs
- Increased fresh air ventilation
- Use of carpets with no formaldehyde outgassing
- Toxin-free pest management
- Use of arsenic-free pressure treated wood
- Green/garden roofs over central part of building

The combination of these elements could earn the building a silver LEED certification rating, which would be the highest rating attained by any high school in the country.

2. Security

Security is a key concern with any school facility and the planning for T.C. Williams includes several significant components designed to ensure a safe and secure environment for students, staff and visitors. Especially as to the parking garage, careful thought has attended the design of the facility and grounds.

Motion sensors and sound systems. As at existing schools in Alexandria, the new high school will be equipped with motion sensors which are designed to identify unexpected activity in any facility after hours. When the motion sensors are tripped, security officers who man a central security office located at Minnie Howard are able to use the reverse sound system in the schools in order to listen to sounds at the location of the motion.

Security cameras. The new high school will also include a comprehensive and Web based system of cameras to monitor the interior of the school building and parking garage, as well as the perimeter of the building. The cameras operate 24 hours a day, and are also connected to viewing machinery at Minnie Howard, where personnel can quickly

view activity occurring at any location. Although every camera tape is not monitored in live time, if there is an incident, or if the motion sensors are activated or a noise is heard over the reverse sound system, the film allows security personnel to quickly view the activity and take appropriate action.

Personnel. The schools employ roving personnel who circulate among the various school properties after hours creating a visible deterrent to uninvited activity.

Lighting. An additional technique under consideration for the new T. C. Williams parking garage is a system of enhanced lighting connected to motion sensors, so that when activity occurs, increased brightness will be generated, indicating after-hours usage and providing a significant deterrent to unauthorized use.

3. ADA Access

The new T.C. Williams will be equipped with sufficient handicapped accessible features to be convenient to the disabled. The school design goes beyond the minimum requirements of the American for Disabilities Act to accommodate as many handicapped features as feasible. Representatives of the Commission for Persons with Disabilities have met with ACPS staff and architects and are actively involved in reviewing plans for the school.

4. Public Art

T.C. Williams High School will be an important new public building, and it is appropriate that it provide a setting for public art. The proposed central entry plaza can be the setting for an example of public art and, provided there is budget approval, staff has included a condition to this effect.

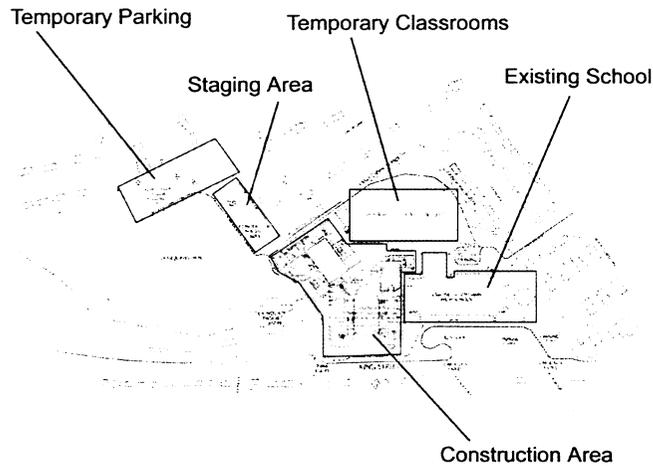
E. CONSTRUCTION PHASING

The construction of the new high school building will be an extremely complex project, made all the more so by the existing high school's uninterrupted operation during construction. There are a myriad of functions which must operate on site, such as school bus loading and unloading; parking for teachers, students, visitors, and construction workers; emergency access for police, fire and emergency medical services; access for recreation functions; construction staging and offices; and temporary classroom space.

City and ACPS share concerns regarding the handling of the construction process, and want to ensure that all aspects of the school operation and the construction project coexist without undue impact on students and the school operation, the adjoining recreation activities, the adjacent neighborhood, and King Street and the community in general.

In order to address the inevitable issues that will arise during this complicated, multi-year project, the School Board and City Council have established a joint task force, consisting of two Council members, two School Board members, and the City Manager and Superintendent of Schools.

The group will meet to address project related issues, such as design, engineering, procurement or neighborhood issues.



Construction Phasing Plan

The proposed site plan includes generalized phasing plans which set out the four stages of construction as follows.

1. Phase 1

Construction is scheduled to begin in late summer of 2004, with the creation of a construction staging area, temporary parking facilities on the athletic fields at Chinquapin, and the demolition of the Career Tech Wing. Temporary classrooms will be located on the stadium field in the form of trailers or modular buildings.

Athletic activity that regularly occurs on the stadium field or the practice fields will have to be curtailed. ACPS is now working on a plan for temporary locations for its programs. For example, football games are likely to occur at a Fairfax County facility.

2. Phase 2

After the temporary facilities are constructed and the improvements on the new school site are removed, active construction of the new building will occur. According to ACPS, that work will occur between August 2004 and August 2006.

3. Phase 3

After the new building is finished, students will move from the old high school building into the new space, scheduled to open in September 2007. Temporary classrooms will be removed and the stadium track and field restored. The construction staging area will, at that point, become a temporary bus pick up and drop off location. And the existing T. C. Williams building will be demolished.

4. Phase 4

With the existing high school building removed, the new parking garage, playing field and new entrance to the school and entry plaza will be constructed. The practice fields at Chinquapin will then be restored. Construction is scheduled to be completed in 2007.

Staff has required that the phasing and construction plans be made more detailed, and that work will occur as part of the final site plan process with the City.

G. STAFF RECOMMENDATION

Staff recommends approval of the development proposal for the new T.C. Williams High School, including the special use permit approvals, the subdivision and the zoning modifications requested by ACPS.

Staff's recommendation is subject to the attached list of conditions for refining the plans and expects to continue to work with ACPS staff, architects and engineers in a collaborative way on the final stages of this exciting development project.

STAFF: Eileen Fogarty, Director, Planning and Zoning
Barbara Ross, Deputy Director
Thomas Luebke, City Architect
Laura Durham, Urban Planner

STAFF RECOMMENDATION AND CONDITIONS

Staff recommends **approval** of the development special use permit, subject to compliance with all applicable codes and ordinances and the following conditions:

I. Site Planning:

1. The preliminary site plan shall be redesigned to include at a minimum the following revisions prior to submission of the first final site plan, to the satisfaction of the Director of P&Z:
 - a. The emergency vehicle easement area on the southeast portion of the site between the gymnasium and the property line shall be designed to provide decorative paving, landscaping (shrubs and trees), and pedestrian scale lighting to provide a pedestrian connection from the fields and Chinquapin Park as generally depicted in *Figure No. 1*. The retaining wall for this area shall be masonry (brick, stone or precast concrete) and shall be the minimum height necessary.
 - b. The area between King Street and the new practice field shall include a continuous natural feature, such as a berm with increased vegetation and/or a depressed field with proposed landscaping adjusted as necessary, to prevent balls and athletic equipment from leaving the field area.
 - c. The sidewalk adjacent to the right turn lane on King Street shall be relocated to provide a 10 ft. landscape strip between the curb and the sidewalk to provide additional area for the proposed street trees. Refinements to the sidewalk dimension may be permitted in order to save existing trees by the entrance.
 - d. The southern portion of the bus loop area shall be revised to enhance the proposed pedestrian-student crosswalk to the sports field and parking structure by reducing the length of the crossing, widening the sidewalk and increasing the amount of open space and landscaping as generally depicted in *Figure 2*.
 - e. The eastern building frontage and sidewalk along Chinquapin Drive shall be revised to provide enhanced pedestrian circulation and access points that should at a minimum include the following:
 - i. At-grade access directly to the curb from the fire stair exit leading to stairs across landscaped area.
 - ii. Revised design of the entry to the daycare area including paved areas, areas of landscape, stairs, retaining walls, etc.

- f. Refine the drive aisle widths and sidewalks to reduce the paved areas to the extent possible including at a minimum the following:
 - i. Reduce 34' width of bus/service entry road , drive aisle and bus loop to 24 ft. and 30 ft. for the western drive aisle where on-street parking may be provided.
 - ii. The setback area in front of the school building shall be planted with natural materials, except to the extent that the area is required for emergency vehicle easement, in which case it shall be planted with a structured turf product suitable to the area to the satisfaction of the Director of P&Z and Code. The sidewalk on King Street shall be 10 ft. wide and where shared with an emergency vehicle easement, the remaining 12 feet shall be reinforced turf. The proposed north-south emergency vehicle easement adjacent to the administration wing shall be an 8 ft. wide sidewalk north of the drop-off area, and the remaining 14 ft. of the EVE in that area shall be reinforced turf. All remaining emergency vehicle areas in this location shall be reinforced turf.
 - iii. Revise handicap ramps to provide City standard access ramps to reduce the amount of paving where possible to the satisfaction of T&ES and P&Z.
 - iv. Reduce the sidewalk width along the south side of the parking structure garage to 8 ft. wide if feasible, and if consistent with other sidewalk widths.
 - v. Increase the depth of the landscaped area adjacent to the west face of the school between the visitor and student entries to 30 feet.
 - vi. Reduce the width of the lay-by on Chinquapin Drive from 15 ft. to 10-12 ft. wide.
 - vii. All curb returns shall be a minimum of 25 ft. radius unless otherwise necessary for vehicles, emergency vehicles or bus turning movements to the satisfaction of the Directors of T&ES, Code and P&Z. (P&Z)

II. Landscaping/Open Space:

2. A revised landscape plan shall be provided with the final site plan to the satisfaction of the Directors RP&CA and P&Z. The final landscape plan shall be prepared and signed by a licensed landscape architect. At a minimum the plan shall provide the level and quality of landscaping depicted on the preliminary landscape plan and the plan shall also provide:
 - a. enlarging the size of the tree wells to 8 ft. x 8 ft., or the closest smaller standard size, between the school and the parking garage within the internal courtyard. The tree wells shall include decorative tree grates.
 - b. Provide three (3) Willow Oak street trees on the western portion of the site adjacent to the church.
 - c. The triangular green area at the western portion of the site along King Street and

adjacent to the Oakland Baptist Church should include landscaping, to include 10-15 additional deciduous, evergreen and flowering trees, evergreen hedges to screen the parking lot, and additional shrubbery to provide a useable open space area with amenities such as benches, trash receptacles, etc. to encourage use of this area.

- d. additional evergreen and deciduous trees at the north west portion of the parking structure to provide additional screening of the parking structure.
- e. additional landscaping at the west edge and southwest corner of the parking garage with a reduction of the sidewalk at that location.
- f. Continuous row of evergreen shrubs within the median at Kenwood Avenue, which shall be a minimum 6' width for planting.
- g. Additional 10-15 evergreen trees on the eastern portion of the north face of the parking garage near the vehicular entrance.
- h. A continuous row of dense evergreen and flowering foundation shrubs between the proposed building and Chinquapin Drive.
- i. A landscaping bed at the new proposed sign location.
- j. Provide a sculpture, focal element or public art within the central entry plaza, with budget approval.
- k. Revise the plantings in the area of the southeastern portion of the building to be a landscaped buffer of mixed evergreens and flowering trees on the sloped area between the rear EVE area and the residential property to the south. The species for this area shall be of a size and type well suited to the soil conditions, and light level of this area. Additional landscaping, in the areas shown on Figure 1 shall be added.
- l. a continued pattern of curved hedges shown in front of the school building to extend to the area between the Kenwood Avenue entrance and the west property line to create a consistent treatment along the entire King Street frontage.
- m. Relocate storm sewer on northern portion of the parking structure to minimize impact on the proposed landscape screening for the parking structure.
- n. Landscaping shall not conflict with existing or proposed utilities. Depict all utility structures, including transformers, on the final development plan. All utility structures (except fire hydrants) shall be clustered where possible and located so as not to be visible from a public right-of-way or property. When such a location is not feasible, such structures shall be located behind the front building line and screened with landscaping. (P&Z)

3. All plant specifications shall be in accordance with the current and most up to date edition of the American Standard For Nursery Stock (ANSIZ60.1) as produced by the American Association for Nurserymen; Washington D.C. All work shall be performed in accordance with Landscape Specifications Guidelines 4th Edition as produced by the Landscape Contractors Association (LCA) of Maryland, District of Columbia and Virginia; Gaithersberg, Maryland. (P&Z, RP&CA)

4. All landscaping shall be maintained in good condition and replaced as needed. (P&Z, RP&CA)
5. The applicant shall design and install irrigation for all areas between King Street and the proposed parking structure and proposed school building and for areas with reinforced turf areas, all with non-potable water to the extent possible. (P&Z) (RP&CA)
6. Tree protection shall be installed, and approved by the City Arborist prior to beginning any demolition, clearing, or construction. As many of the existing mature trees on the site as possible shall be saved and must be protected, including but not limited to the following to the satisfaction of the Director of P&Z and RP&CA.
 - i. Show the Sycamore at the end of Woods Avenue to be saved and relocate the landscaping accordingly.
 - ii. Relocate the landscaping from the existing trees to remain at the edge of the neighboring houses and track, including next to Lots 1, 2, 3, 15A and the Oakland Baptist Church.
 - iii. All proposed tree protection details shall be depicted on the final site plan and be provided throughout the construction process to the satisfaction of the City Arborist and Director of P&Z.
 - iv. Show tree protection for existing trees to be saved at the main entrance. (RP&CA) (P&Z)
7. Proposed trees should be moved: relocate trees farther from the proposed 72" pipe on Chinquapin Drive; shift tree locations at the corner of the playing field where tree is located over yard inlet (sheet C-25). (RP&CA)
8. Landscaping shall be provided of sufficient depth and character, to include evergreen plantings, to buffer neighboring residential property from the visual impact of the school buildings, athletic fields and activities without infringing on the existing athletic fields. (P&Z)
9. Artificial turf shall be provided on the stadium field, as requested by RP&CA, with budget approval. (RP&CA)

III. Building Design

10. The massing, articulation and general design of the office building shall be generally consistent with the drawings and renderings submitted with this application dated November 7, 2003. The final design of the building shall be revised to incorporate the following to the satisfaction of the Director of P&Z.
 - a. entirely masonry (brick, precast, or stone) materials for the facade with painted metal accents limited to those elements as depicted, except for screening of the mechanical equipment;

- b. The angled elevation of the performing arts wing facing the entry court shall be treated with recesses, piers, and/or horizontal accents to add relief to the 35-foot expanse of blank wall.
 - c. Rooftop mechanical equipment shall be screened from view. If rooftop equipment will be visible from King Street within a reasonable distance, as indicated by sightlines calculated by ACPS, screening may require providing a masonry parapet at the perimeter of the building. Details on the screening methods shall be indicated on the final site plan.
 - d. Enclosures providing rooftop access from the stairs flanking the library shall be designed to harmonize with the adjacent roof forms and minimize their visual impact.
 - e. Precast masonry trim shall be used as depicted, including tops of masonry piers and bases under the windows shall be provided as shown.
 - f. Spandrel glass shall be provided under the 2nd floor windows on the north side of the west pavilion facing King Street to match the east pavilion. The vertical divisions of these windows shall match those in the same position on the east pavilion
 - g. Detail at the tops and bases of the metal columns at the library exterior shall be similar to the typical metal columns on the perimeter of the building and at the athletic wing entrance.
 - h. The wall adjacent to the student commons courtyard shall be limited to a maximum height of 2' . The walls shall be a combination of brick and precast masonry. (P&Z)
11. Architectural elevations (front, side and rear) and floor plans of ground levels shall be submitted with the submission of the final site plan. Elevations shall indicate material treatment and color which will be accompanied by materials sample board for review. Each elevation shall indicate the average finished grade line. All refinements to the design and materials shall be revised prior to the release of a building permit for any above grade construction. A material board shall be constructed on-site prior for the review and approval of all exterior materials in consultation with the Department of P&Z, to occur timely, prior to the final selection of materials for the building. (P&Z)
12. Provide detailed drawings showing all proposed fences and walls, including structural details, identifying all materials and dimensions to the satisfaction of the Director of Planning and Zoning and Transportation and Environmental Services. All faces of retaining walls shall be entirely brick, stone or other similarly high-quality material to the satisfaction of the Director of P&Z. (P&Z) (T&ES)
13. Provide decorative metal picket fencing on the top of retaining walls adjacent to the retaining walls along Chinquapin Drive overlooking the daycare play area and pedestrian egress areas.

14. For firefighting reasons all stairs should extend through the roof. Two stairs noted on sheet A2.17, which shall contain roof doorways; the remaining rated stair enclosures shall include mechanisms for easy access to the roof, with such mechanisms to be to the satisfaction of the Directors Code Enforcement and Planning and Zoning. (Code)
15. A maximum of one freestanding monument sign is permitted. The proposed freestanding sign for the King Street frontage shall be designed to be compatible with the building design, but should be reduced in height to 6 ft. and shall not be internally illuminated. (P&Z)
16. The applicant shall prepare design plans for all signage, including pedestrian and traffic signage, on the high school site for approval by P&Z and T&ES. (P&Z, T&ES)
17. The building shall be designed to minimize sound from mechanical equipment (i.e., cooling towers, condensers, refrigeration equipment, exhaust fans, and generators) being projected into the adjoining neighborhoods.(P&Z)
18. The athletic wing and gymnasium, exits, exterior walkways, windows, HVAC equipment, exhaust fans, etc. shall be designed and constructed (including sufficient screening and sound attenuation) in a manner to minimize the sound levels that will be generated by such a large facility, its mechanical systems, and its patrons. (P&Z)
19. The school building shall be designed to allow a possible future pedestrian bridge connection between the athletic wing of the high school and Chinquapin Center in the future to promote the joint use of both facilities. The design shall take into consideration that at least 14 feet of clearance is required from the lowest point of the future structure for fire apparatus to pass under the pedestrian bridge on Chinquapin Drive. (RP&CA) (Code)
20. The applicant shall revise the materials and design of the freestanding parking structure to the satisfaction of the Director of P&Z. The revisions shall at a minimum include the following:
 - a. The use of freestanding light poles on the top level shall be minimized and the height shall be the minimum necessary. The use of bollard lighting or similar light sources other than freestanding poles shall be encouraged.
 - b. The entire exterior of the parking structure shall be brick and precast concrete as generally depicted on the preliminary elevations.
 - c. The window openings for the stair towers of the parking structure shall be as generally represented on the preliminary architectural elevations.
 - d. The height of the parking structure shall not be increased above what is generally represented on the preliminary plans. (P&Z)

IV. Pedestrian Circulation and Access

21. The concrete sidewalks shall continue over all three curb cuts on King Street to provide an uninterrupted sidewalk. Provide stamped and colored bomanite paver crosswalks at these locations. All sidewalks within the site shall be connected to the public sidewalks to the satisfaction of the Directors of P&Z and T&ES. (P&Z)(T&ES)
22. Provide a hardscape and courtyard plan to the satisfaction of the Director of P&Z that at a minimum includes the following:
 - a. Extension of the center median at the Kenwood Avenue to create a pedestrian refuge area.
 - b. Improved pedestrian access from the main student entrance and from the garage to the stadium/fields with functional pedestrian access and wheelchair access between the parking garage and the main school building, track, playing fields and stadium, generally consistent with *Figure 2*.
 - c. Provide an additional pedestrian entrance/exit at the southeast corner of the garage.
 - d. Revised design for the stairs from the athletic wing to the stadium to maximize the open area around the track.
 - e. Special pavement materials and accents shall be used to distinguish and emphasize pedestrian areas.
 - f. Stamped pavement or contrasting pavers on all pedestrian crossings of internal vehicular ways.
 - g. A 10'-wide stair from the rear EVE area to the stadium field located so as to be centered on the EVE for pedestrian convenience.
 - h. The material for the student drop-offs, main entrance area, student commons and the western end of the new playing field shall include decorative pavers, seating areas, landscaping and other amenities to encourage its use to the satisfaction of the Director of P&Z. (P&Z)

V. Traffic and Transportation Management

23. Provide a detailed operating plan for the main entrance at Kenwood Avenue to ensure safe access for vehicles and pedestrians, to avoid traffic conflicts, and to prevent queuing back ups onto King Street. Provide enhancements as necessary for the drop-off/pick up loop and parking garage entrance, including signage details, lane marking, a traffic channelization island, and on site personnel to direct traffic at peak times. (P&Z) (T&ES)
24. Extend the median at the main entrance at Kenwood Avenue rearward (south) to prohibit cars leaving the garage from making a left turn and crossing traffic to exit the area. The extended median is to include a mountable curb for emergency vehicles and landscaping

to the satisfaction of P&Z and RP&CA. (P&Z)

25. Maintain the alignment of the school's main entrance/exit lining up with Kenwood Avenue at King Street to the satisfaction of the Director of T&ES. (T&ES)
26. Provide a detailed operating plan for the pick up/drop off entrance on Chinguapin Drive to ensure safe access for vehicles and pedestrians, to maintain access to Chinguapin Center and Chinguapin Park, to minimize traffic conflicts, and to prevent queuing back ups onto King Street. Provide enhancements as necessary, including signage, parking restrictions, location of the drop off layby, and on site personnel to direct traffic at peak times. (P&Z) (T&ES)
27. The bus entrance at Radford Street will not be signalized. Provide a detailed operating plan for the bus egress during morning and afternoon exits. If operational plan includes use of personnel to control or assist traffic movement, provide assurance that personnel can safely perform this function. (T&ES)
28. The bus shelter to be provided at the existing bus stop on King Street at the western service drive shall provide seating and shelter integrated into a structure with a design that complements the design of the new school building to the satisfaction of the Directors of P&Z, TES. (P&Z) (T&ES)
29. Prior to the start of construction of any traffic equipment or structures, the applicant shall submit shop drawings to T&ES for approval for the following equipment: traffic signal poles/masts, LED traffic signals, pedestrian signals, backlit street signs, and traffic and pedestrian signage/poles. (T&ES)
30. All Traffic Control Device design plans, Work Zone Traffic Control plans, and Traffic Studies shall be sealed by a professional engineer, registered in the Commonwealth of Virginia. (T&ES)
31. All traffic signal poles and mast arms shall be black matte finish. (P&Z)
32. The applicant shall provide a traffic signal plan with final site plan detailing the proposed signal modifications at the Kenwood Avenue/ King Street intersection. Provide pedestrian countdown heads at the intersection of King Street and Kenwood Ave. (T&ES)
33. All driveway entrances and sidewalks in public ROW or abutting public ROW shall meet City standards. (T&ES)
34. A decorative metal gate with locked entrance shall be installed at the Woods Avenue entrance to the school site to preclude vehicular traffic, except for emergency and police vehicles. (P&Z)

35. Replace existing curb and gutter, sidewalks, and handicap ramps that are in disrepair or broken along King Street frontage. (T&ES)
36. Street trees and landscaping to be trimmed and maintained as necessary to avoid blocking visibility of traffic control devices in public right of way. (T&ES)

VI. Parking:

37. The applicant shall follow the Parking Management Plan submitted which includes methods to minimize the number of vehicles parking on the high school/Chinquapin site, including:
 - a. creating a student parking permit system to limit the number of students who park on site;
 - b. relocating and scheduling adult education classes so that parking can be accommodated on site;
 - c. scheduling special events to the extent possible so as not to conflict with other on site parking demands;
 - d. providing overflow parking at neighboring lots;
 - e. continuing to collaborate with the RP&CA to create opportunities for programs on both sites to share parking facilities as needed; and
 - f. providing incentives to encourage employees to use public transportation such as DASH passes and Metro Check which allows employees to purchase Metro and DASH fare cards. (P&Z)
38. The applicant shall ensure that the parking spaces in the garage and on the site are built to standard dimensions. The conflicts among the various site plan and architectural drawings with varying numbers of parking spaces shall be revised and made consistent. Compact, handicapped and visitor parking spaces shall be labeled on the drawings and signed. (P&Z).
39. The applicant shall provide 20 bicycle parking spaces to the satisfaction of the Director of T&ES. (T&ES)
40. Provide wheel stops at the visitor parking lot. (P&Z)

VII. Environmental:

41. The applicant is advised that all stormwater designs that require analysis of pressure hydraulic systems and/or inclusion and design of flow control structures must be sealed by a professional engineer, registered in the Commonwealth of Virginia. If applicable, the Director of T&ES may require resubmission of all plans that do not meet this standard. (T&ES)

42. Plan must demonstrate to the satisfaction of the Director of T&ES that adequate stormwater outfall is available to the site or else developer is to design and build any on or off site improvements to discharge to an adequate outfall. (T&ES)
43. Provide proposed elevations (contours and spot elevations) in sufficient details on grading plan to clearly show the drainage patterns. (T&ES)
44. Indicate the pipe sizes and add flow arrows for storm and sanitary sewer lines. (T&ES)
45. Provide pre and post storm water runoff computations for the two- and ten-year storms. (T&ES)
46. The stormwater collection system is part of the Taylor Run/Cameron Run watershed. All on-site stormwater curb inlets and public curb inlets within 50 feet of the property line shall be duly marked to the satisfaction of the Director of T&ES. (T&ES)
47. The stormwater Best Management Practices (BMPs) required for this project and the cistern shall be constructed and installed under the direct supervision of the design professional or his designated representative. Prior to issuance of the certificate of occupancy, the design professional shall submit a written certification to the Director of T&ES that the BMPs and cistern are:
 - a. Constructed and installed as designed and in accordance with the approved Final Site Plan.
 - b. Clean and free of debris, soil and litter by either having been installed or brought into service after site was stabilized. (T&ES)
48. The surface appurtenances associated with the on-site structural stormwater Best Management Practices (BMPs) shall be marked to the satisfaction of the Director of T&ES to identify them as part of a structural BMP system. (T&ES)
49. For any surface-installed stormwater Best Management Practice (BMP), i.e. Bio-Retention Filters, Vegetated Swales, etc. that are employed for this site, descriptive signage for the BMPs is required to be installed to the satisfaction of the Director of T&ES. (T&ES)
50. Prior to the release of the final site plan, the applicant shall execute and submit a maintenance agreement with the City for the stormwater quality Best Management Practices (BMPs). The applicant shall be responsible for maintenance of all BMP's associated with this project, including those located within the public right-of-way. For BMPs that are located off of the school property and within City right-of-way, the maintenance agreements shall address the additional levels of maintenance to adjacent appurtenances that are affected by the BMPs (ie. sidewalks, landscaping, streets, storm

sewers, storm sewer inlets). (T&ES)

51. The applicant shall execute a maintenance service contract with a private contractor for a minimum of three years. A copy of the contract shall be placed in the BMP Operation and Maintenance Manual. Prior to issuance of the certificate of occupancy, a copy of the contract shall be submitted to the City. (T&ES)
52. The applicant shall prepare an Owner's Operation and Maintenance Manual for all the Best Management Practices (BMPs) used on site. The manual shall include at a minimum: an explanation of the functions and operations of the BMP(s); drawings and diagrams of the BMP(s) and any supporting utilities; catalog cuts on maintenance requirements; manufacturer contact names and phone numbers; a copy of the executed maintenance service contract; and a copy of the maintenance agreement with the City. Prior to issuance of the certificate of occupancy, a copy of the Operation and Maintenance Manual shall be submitted to the City on a digital media. (T&ES)
53. Stormwater quality and quantity management must be addressed for temporary impervious areas that are proposed to be in place and functioning (i.e. temporary parking lots) for a significant period of time. Proposed temporary BMPs if required by T&ES must be included on the final site plans. (T&ES)
54. Proposed synthetic surfaces (i.e. playing field) require stormwater management treatment in an appropriate fashion, including, if necessary, with a BMP. (T&ES)
55. Solid waste services are provided by the City. The development must meet all the minimum street standards, including all standards for turnaround movements. (T&ES)

VIII. Construction and Utilities:

56. The City Attorney has determined that the City lacks the authority to approve the gravity fed sanitary sewer systems which serve over 400 persons. Accordingly, the overall sanitary sewer system for the proposed development must be submitted for approval by the Virginia Department of Health (VDH). Both City and VDH approval are required, though City approval may be given conditioned upon the subsequent issuance of VDH approval. Should state agencies require changes in the sewer design, these must be accomplished by the developer prior to the release of a certificate of occupancy for the units served by this system. Prior to the acceptance of dedications of the sewers by the city or release of any construction bonds, the developer must demonstrate that all necessary state agency permits have been obtained and as-built drawings submitted to the City that reflect all changes required by the state. (T&ES)
57. The site is located on marine clay areas as delineated on City map of marine clay areas. Provide geotechnical report including recommendations from a geotechnical professional

for proposed cut slopes and embankments. (T&ES)

58. Provide a demolition plan that indicates which existing utilities are to be removed and which are to remain. (T&ES)
59. During the construction phase of this development, the site developer, its contractor, certified land disturber, or owner's other agents shall implement a waste and refuse control program. This program shall control wastes such as discarded building materials, concrete truck washout, chemicals, litter or trash, trash generated by construction workers or mobile food vendor businesses serving them and sanitary waste at the construction site and prevent its off site migration that may cause adverse impacts to the neighboring properties or the environment to the satisfaction of Directors of T&ES and Code Enforcement. All wastes shall be disposed off site properly in accordance with all applicable federal, state and local laws. (T&ES)

IX. General Site Plan Requirements:

60. Any inconsistencies among the various drawings submitted by the applicant shall be reconciled to the satisfaction of the Directors of P&Z and T&ES. (P&Z)
61. The applicant shall be allowed to make minor adjustments to the building if the changes do not result in a loss of parking or open space, impact on the Emergency Vehicle Easement, or an increase in building height or floor area ratio. (P&Z)
62. The limits of disturbance on the proposed site plan shall be revised to include all land area within the phasing plan, to include for example the temporary parking area, the construction staging area and the area for classroom trailers. The limits of disturbance shall be adjusted so as not to include tree save areas on the western perimeter near Woods Avenue. (P&Z)
63. Clarify the location of the cistern. The cover sheet states that it is under the parking structure, sheet 7 indicates that it is under the practice field, and sheet 22 indicates that it is under the parking structure. (T&ES)
64. Applicant shall provide three (3) City standard trash receptacles, Iron Site Bethesda Series Model #SD-42, with spun steel dome and side door access, manufactured by Victor Stanley, Inc. of Dunkirk, MD, or provide \$850 each to the Director of T&ES for the purchase and installation of three (3) City standard street cans, two along King Street at both the existing and proposed bus stops and one on Chinquapin Drive at the layby area (T&ES).
65. Show all existing and proposed easements, both public and private. (T&ES)

66. Provide City standard pavement details or alternative pavement details on the plan for emergency vehicle easements, sealed by a professional engineer registered in the Commonwealth of Virginia, designed to the satisfaction of the Director of T&ES. (T&ES)
67. Prior to the issuance of a demolition permit or land disturbance permit, a rodent abatement plan shall be submitted to Code Enforcement that will outline the steps that will be taken to prevent the spread of rodents from the construction site to the surrounding community and sewers. (Code)

X. Neighborhood Committees:

68. To provide a forum for the discussion and resolution of neighborhood issues that arise during the construction of the new school, ACPS shall create and coordinate a Community Advisory Committee, to include ACPS officials; T.C. Williams officials; Department of Recreation, Parks and Cultural Activities officials; representatives of the adjoining neighborhoods; and representatives of the entities involved in the construction project, such as the general contractor. The Advisory Committee shall meet periodically as conditions dictate throughout the period of construction. (P&Z)
69. To provide a permanent forum for the discussion and resolution of neighborhood issues that arise as a result of the ongoing operation of the new high school, ACPS shall create and coordinate a Community Advisory Committee to include representatives of ACPS, TC Williams High School, the Department of Recreation, Parks and Cultural Activities, and the adjoining neighborhoods. The Advisory Committee shall determine its meeting schedule, which may be either on a regular basis or as conditions require. (P&Z)

XI. Security:

70. The applicant is to consult with the Crime Prevention Unit of the Alexandria Police Department regarding the layout of the building as well as security hardware, and alarms for the building. The number for the Crime Prevention Unit is 703-838-4520.
71. A security survey for any on site construction trailer(s) shall be completed as soon as the trailers are located on the site. (Police)
72. The proposed shrubbery is to have a maximum height of 36 inches when it matures, unless required for screening and not located within six feet of walkways. (Police)
73. All trees, except evergreens for screening, are to be limbed up a minimum of six feet as they mature to allow for natural surveillance. (Police)

74. Trees are not to be planted under or near light poles. (Police)
75. The parking garage shall include security systems, including an alarm system if appropriate and cameras placed throughout the garages and monitored by the staff at the main school alarm headquarters, all to the satisfaction of the Chief of Police. (Police)
76. The interior walls, ceilings, and pillars in the garage are to be painted white. (Police)
77. Gates installed to restrict vehicular access at the rear and side of the building and at Woods Avenue shall be accessible to both emergency vehicles and the Police, and shall be equipped with the following measures to the satisfaction of the director of Code Enforcement:
 - i. A siren activation system which will unlock and open the security gates upon initiation of a yelp signal from the fire apparatus siren.
 - ii. A key override system keyed to the Knox Box system for the City of Alexandria which will unlock and open the security gates upon insertion of a Knox Box Key approved by the City of Alexandria.
 - iii. Gates shall open upon activation of the school's fire alarm system. (P&Z) (Police) (Code)

XII. Emergency Access:

78. All structures shall be required to have an approved automatic fire detection and suppression system in accordance with the Uniform Statewide Building Code. (Code)
79. All Emergency Vehicle Easement (EVE) areas shall be maintained at a minimum of 22 feet in width, a minimum of R-25 turning radii, vehicle loading conforming to AAHSTO Loading Standard H-20, and shall be recorded in the land records. (P&Z)
80. Detailed information shall be provided to the Director of Code Enforcement regarding the proposed reinforced turf product(s) to be used to demonstrate sufficiently that the product chosen will withstand the proposed installation, grade, location, and use. (Code)
81. All fire hydrants shall conform to the City of Alexandria specifications for fire hydrants as detailed in the Water and Fire Requirements for Site Plans and New Construction. (Code).
82. All hydrants shall be located so the distance measured from the hydrant to the most remote point of vehicular access on the site is 300 feet (Code). Also, there shall be at most 600' between hydrants and at least one hydrant within 100' of each FDC. (Code)
83. Parallel and perpendicular parking shall be prohibited on all paved areas located next to the school structure (Code).

XIII. Lighting:

84. The applicant shall submit a detailed lighting plan, to include all existing and proposed street lights and site lights, indicating the types of fixture, mounting heights and strength of fixture in Lumens or Watts and providing manufacturers' specifications for the fixtures and lighting analysis to verify that lighting is consistent with city standards for illumination level and uniformity and acceptable to the Departments of T&ES and P&Z, in consultation with the Police. Particular attention must be paid with regard to the lighting for the parking garage to achieve the goals of security and aesthetics, and to minimize impact to neighboring properties. Pole mounted lights shall be minimized on the garage upper level; rooftop lighting in bollards or roof mounted fixtures shall be the preferred option. (P&Z) (T&ES)
85. No permanent stadium lighting shall be installed at the School stadium or on any other athletic fields, including the proposed new practice field. (P&Z)
86. Site lighting (e.g., parking garage lighting, parking lot lighting, and perimeter security lighting) shall be directed to the school property and away from residential neighborhoods to the maximum extent possible. Representatives of adjacent neighborhoods will have an opportunity to review the proposed lighting plan prior to release of the final site plan. (P&Z)

XIV. Phasing:

87. The applicant shall prepare and submit with the first final site plan a detailed phasing plan and construction management plan for the entire project for review and approval by the Directors of P&Z, T&ES RP&CA and Code Enforcement, whose approval is required prior to the submission of the second final site plan. At a minimum, the plan shall include the following:
 - a. Phasing for each portion of the project and for each required public service, installation or improvement (streets, traffic signals, vehicular traffic circulation, including service vehicles, student pick-up and drop-off sidewalks, etc.).
 - b. A Traffic Control Plan detailing proposed controls to traffic movement, lane closures, construction entrances, haul routes, and storage and staging.
 - c. A plan for temporary pedestrian and vehicular circulation during construction. The plan shall identify temporary sidewalks, fencing around the site and any other features necessary to ensure safe pedestrian and vehicular travel around the site during construction, (including temporary sidewalks), including methods for constructing the parking garage without disturbing pedestrian access from

- d. completed portions of the project, and a plan for school bus loading an unloading, A plan for parking for students, teachers, staff and visitors, with signage, pedestrian access, and proper installation.
 - e. A plan for parking for construction workers. The developer shall secure off-street locations for all construction workers to park without charge and shall provide transit subsidies to workers for utilizing transit, van pools, or another method of providing for construction workers to arrive at the site. This plan shall set forth, for example, the location of the parking to be provided at various stages of construction, the number of spaces will be provided, the estimated number of construction workers to be assigned to the work site, and mechanisms which will be used to encourage the use of Metro, carpooling, vanpooling, and other similar efforts.
 - f. Provisions for the following:
 - i. Temporary utility plan
 - ii. Temporary stormwater management facilities
 - iii. Safety and security issues
 - iv. Noise, and impacts on neighbors
 - v. Emergency vehicle access and fire department water supply
 - vi. Tree protection
 - g. All other phasing details deemed necessary by the Directors of P&Z, T&ES RP&CA and Code Enforcement. (P&Z) (T&ES)
88. Before commencing any clearing or grading of the site, the applicant shall hold a meeting with members of the Construction Advisory Committee to review the hauling route, location of construction worker parking, plan for temporary pedestrian and vehicular circulation, and hours and overall schedule for construction. The Departments of P&Z, RP&CA and T&ES shall be notified of the date of the meeting upon issuance of the permit. Copies of plans showing the hauling route, construction worker parking and temporary pedestrian and vehicular circulation shall be posted in the construction trailer and given to each subcontractor before they commence work on the project. (P&Z) (RP&CA) (T&ES)
89. A temporary informational sign shall be installed on the site prior to approval of the final site plan for the project and shall be displayed until construction is complete; the sign shall notify the public of the nature of the upcoming project and shall provide a phone number for public questions regarding the project. (P&Z)
90. Temporary construction trailer(s) or modular buildings shall be permitted and be subject to the approval of the Director of P&Z, RP&CA and Code Enforcement. (P&Z) (Code)

CITY DEPARTMENT COMMENTS

Legend: C - code requirement R - recommendation S - suggestion F - finding

Transportation and Environmental Services

- C- 1 Bond for the public improvements must be posted prior to release of the plan, if applicable.
- C- 2 All downspouts must be connected to a storm sewer by continuous underground pipe.
- C- 3 Plans and profiles of utilities and roads in public easements and/or public right-of-way must be approved prior to release of the plan.
- C- 4 All drainage facilities must be designed to the satisfaction of T&ES. Drainage divide maps and computations must be provided for approval.
- C- 5 All utilities serving this site to be underground.
- C- 6 Provide site lighting plan to meet minimum city standards.
- C- 7 Must comply with Article XIII of zoning ordinance, including regulations on adequate stormwater outfall, pre and post stormwater runoff computations for two and ten year storms, and water quality treatment.
- C- 8 The building related operation and maintenance shall comply with the City of Alexandria's Noise Control Code, Title 11, Chapter 5, which sets the maximum permissible noise levels as measured at the property line.
- C- 9 The applicant must comply with the City of Alexandria, Erosion and Sediment Control Code, Section 5, Chapter 4. This includes naming a Responsible Land Disturber on the Erosion and Sediment Control sheets prior to engaging in land disturbing activities in accordance with Virginia Erosion and Sediment Control Law.
- C- 10 All required permits from Virginia Department of Environmental Quality, Environmental Protection Agency, Army Corps of Engineers, Virginia Marine Resources must be in place for all project construction and mitigation work prior to release of the final site plan. This includes the new state requirement for a VPDES permit for all construction activities greater than one acre.
- F-1 Indicate pipe sizes and add flow arrows for storm and sanitary sewer lines.
- F-2 Indicate horizontal clearances between utilities in rear of building. Provide minimum 5' clearances between utilities to adequately construct and maintain each utility.

Code Enforcement

- C-1 A comprehensive geological study is required to determine site appropriateness for the proposed structure. Acknowledged by applicant. Study is underway. Plan should be submitted as part of preliminary site plan submittal. Report submitted to Code Enforcement Engineering. Report will be reviewed at time of submission of building plans.
- C-2 All independent automatic sprinkler systems shall be equipped with two independent fire department connections (FDC). Each FDC shall be located on an opposite side of the structure where appropriate and to the satisfaction of the fire department. Acknowledged by applicant. Plans do not show locations of fire department connections (FDC) for garage and high school. FDC locations shall be shown on plans. See C-2 above. Fire line for garage sprinkler system is not shown. Show fire line on plans. **Condition met, fire line shown on plans.**
- C-3 Fire flow data for the affected area will need to be provided at the time of plan submittal. Acknowledged by applicant. The applicant should provide the required fire flow data as early as possible in order to identify and address any potential fire flow issues that may arise. **Not submitted. A Fire Flow Analysis shall not consist solely of information provided by Virginia American Water Company. See attached specifications for what information is required for a Fire Flow Analysis.**
- C-4 Fire Hydrants shall be located at least 40 feet from all buildings serviced by the hydrant. Agreed by applicant. Site requires additional hydrants and relocation of several proposed hydrants. See attached plan. Revised hydrant distribution is acceptable. However, revised plan show extended median for bus loop which now obstructs hydrant accessibility. Applicant shall clarify how extended median and traffic pattern is to work. The new configuration may require an additional fire hydrant to provide proper coverage as a result of this change. **Relocate fire hydrant by bleachers as shown on attached plan. Hydrant should be located next to school building by corner entering bus loop. Revised bus loop plan is acceptable for fire apparatus access.**
- C-5 Hydrants shall be located within 100 feet of each FDC. FDCs are not shown on plan. FDCs shall be shown on plan for proper review. Condition met.
- C-6 Revised EVE plan provides 360 degree EVE access. Extend transition area at rear corner of school by shops to provide a more entrance and recovery into corner turn. **Revised rear turning easement by shop area is acceptable. The proposed gates along the rear E.V.E. shall be equipped with the following measures subject to approval by the Director of Code Enforcement:**
- **A siren activation system which will unlock and open the security gates upon initiation of a yelp signal from the fire apparatus siren.**

- A key override system keyed to the Knox Box system for the City of Alexandria which will unlock and open the security gates upon insertion of a Know Box Key approved by the City of Alexandria.
- Gates shall open upon activation of the school's fire alarm system.

- C-7 Construction phasing of this project shall incorporate the highest safety provisions for the safeguarding of the student and adult population of the existing facility during the construction of the new structure. Acknowledged by applicant. The preliminary phasing plan does not address emergency vehicle access to the sites. The applicant shall address fire access to both the proposed structure, existing structure and proposed trailers. Sufficient fire access shall be provided not only to access these areas but to turnaround and depart these areas. Fire access areas shall conform to H-20 vehicle rating standards. **Phasing plan submitted does not address H20 loading concerns. Plan should also show hydrant accessibility to meet 300 ft. maximum travel distance to effected structures and trailers. The proposed plans PH1-4 and C-19 are inconsistent with each other. Sheet C-19 shows the use of the track as an E.V.E. PH-1 through 4 show differing routes used as a E.V.E. The plans shall complement each other in order to conduct a proper review. The proposed plan (C-19) requires an additional hydrant to be installed beyond the proposed hydrant to meet the proper hydrant coverage criteria. See attached drawings. The hydrant shall be installed near the south bleachers. Verify the track meets H-20 loading for fire apparatus. Ghosted drawings obstruct the proposed E.V.E. in the track. No trailers shall obstruct the track area. The proposed use of the shop hydrant as part of the hydrant coverage is not acceptable. The hydrant will be obstructed by fencing and is not readily accessible to fire apparatus. The applicant shall look at alternative means to meet the hydrant coverage issues in this area. The temporary E.V.E. at the rear of the existing school shall be extended as noted on plans PH-1 and PH-2.**
- C-8 Section B-B shows a pedestrian bridge from the parking structure to the main school building. This bridge is not shown on any other sheets in the submitted plans. Reflect all overhead structures in submitted drawings. Applicant states there will be no pedestrian bridge in this area. **Condition met.**
- C-9 A separate tap is required for the building fire service connection. Show fire tap on plans. Fire tap shown for high school. No fire tap shown for garage. Show tap for garage fire line. **Condition met. Fire tap shown on plans.**
- C-10 New construction must comply with the current edition of the Uniform Statewide Building Code (USBC). Show note on plans. Note not shown on plans. **Show note on site plan in addition to sheet LS1.01.**
- C-11 The developer shall provide a building code analysis with the following building code data on the plan: a) use group; b) number of stories; c) type of construction; d) floor area

per floor ; e) fire protection plan. Provide analysis on plans. Analysis provided.
Condition met.

- C-12 The developer shall provide a separate Fire Service Plan which illustrates: a) emergency ingress/egress routes to the site; b) two fire department connections (FDC) to the building, one on each side/end of the building; c) fire hydrants located within on hundred (100) feet of each FDC; d) on site fire hydrants spaced with a maximum distance of three hundred (300) feet between hydrants and the most remote point of vehicular access on site; e) emergency vehicle easements (EVE) around the building with a twenty-two (22) foot minimum width; f) all Fire Service Plan elements are subject to the approval of the Director of Code Enforcement. Include fire service plans as part of complete plan set. Fire Service plan provided. **Condition met.**
- C-13 The final site plans shall show placement of fire easement signs. **Not Shown to date.**
- C-14 A Certificate of occupancy shall be obtained prior to any occupancy of the building or portion thereof, in accordance with USBC 118.0.
- C-15 A fire prevention code permit is required for the proposed operation. **Agreed by applicant.**
- C-16 This structure contains mixed use groups and is subject to the mixed use and occupancy requirements of USBC. **Agreed by applicant.**
- C-17 Required exits, parking, and accessibility within the building for persons with disabilities must comply with USBC Chapter 11. Handicapped accessible bathrooms shall also be provided. Show accessibility items on plan. Indicate if handicap parking in garage is for staff and visitors. If provided for staff only, additional handicap parking will be required. Handicap accessible restroom facilities will be required for temporary trailers. **Condition met. Handicap parking in garage serves both staff and visitors per applicant. Applicant acknowledges provision for handicap facilities for temporary trailers.**
- C-18 The public parking garage floor must comply with USBC and drain through oil separators or traps to avoid accumulation of explosive vapors in building drains or sewers as provided for in the plumbing code. Floors of public garages must be graded to drain through oil separators or traps to avoid accumulation of explosive vapors in building drains or sewers. **Agreed by applicant.**
- C-19 Prior to the issuance of a demolition permit or land disturbance permit, a rodent abatement plan shall be submitted to Code Enforcement that will outline the steps that will taken to prevent the spread of rodents from the construction site to the surrounding community and sewers. Show note on plans. Note not shown on plans. **Condition Met,**

Shown as note on sheets C-03 through C-05.

- C-20 Roof drainage systems must be installed so as neither to impact upon, nor cause erosion/damage to adjacent property. **Agreed by applicant.**
- C-21 Sheeting and shoring shall not extend beyond the property line; except when the developer has obtained a written release from adjacent property owners which has been recorded in the land records; or through an approved encroachment process. **Agreed by applicant.**
- C-22 A soils report must be submitted with the building permit application. **Agreed by applicant.**
- C-23 The applicant shall provide a continuous Emergency Vehicle Easement (EVE) around the entire building, including the rear and sides. **This has been met.**
- C-24 Roadways and paved areas located next to the school structure shall be deemed emergency vehicle easements. **This has been met.**

Police

- F-1 No lighting plan submitted.

The following recommendation related to site lighting has not been included as a condition; rather, staff has recommended that the applicant prepare a lighting plan to the satisfaction of the Director of T&ES in consultation with the Chief of Police, which will likely result in lower lighting levels than recommended by the Police.

- R-3 The lighting for sidewalks, parking lots, and all common areas is to be a minimum of 2.0 foot candles minimum maintained. Lighting for the garage is to be a minimum of 2.0 foot candles minimum maintained. These lights are to be controlled by timers to go off at a time decided by school officials, depending on activities on any given night.

The following recommendation related to trees and shrubs has been included as conditions, but modified to allow for evergreen trees not to be limbed, and for shrubs used for screening, for example, adjacent to neighboring homes, to be taller than 36 inches.

- R-7 Shrubbery is to have a maximum of 36 inches when it matures. Trees planted on the site in most cases are to be limbed up a minimum of six feet as they mature. This will allow for the optimum level of natural surveillance.

Recreation, Parks and Cultural Activities

- F-1 The outdoor play area for the day care facility should be reviewed, including the 95 grade spot at the entrance stairs.

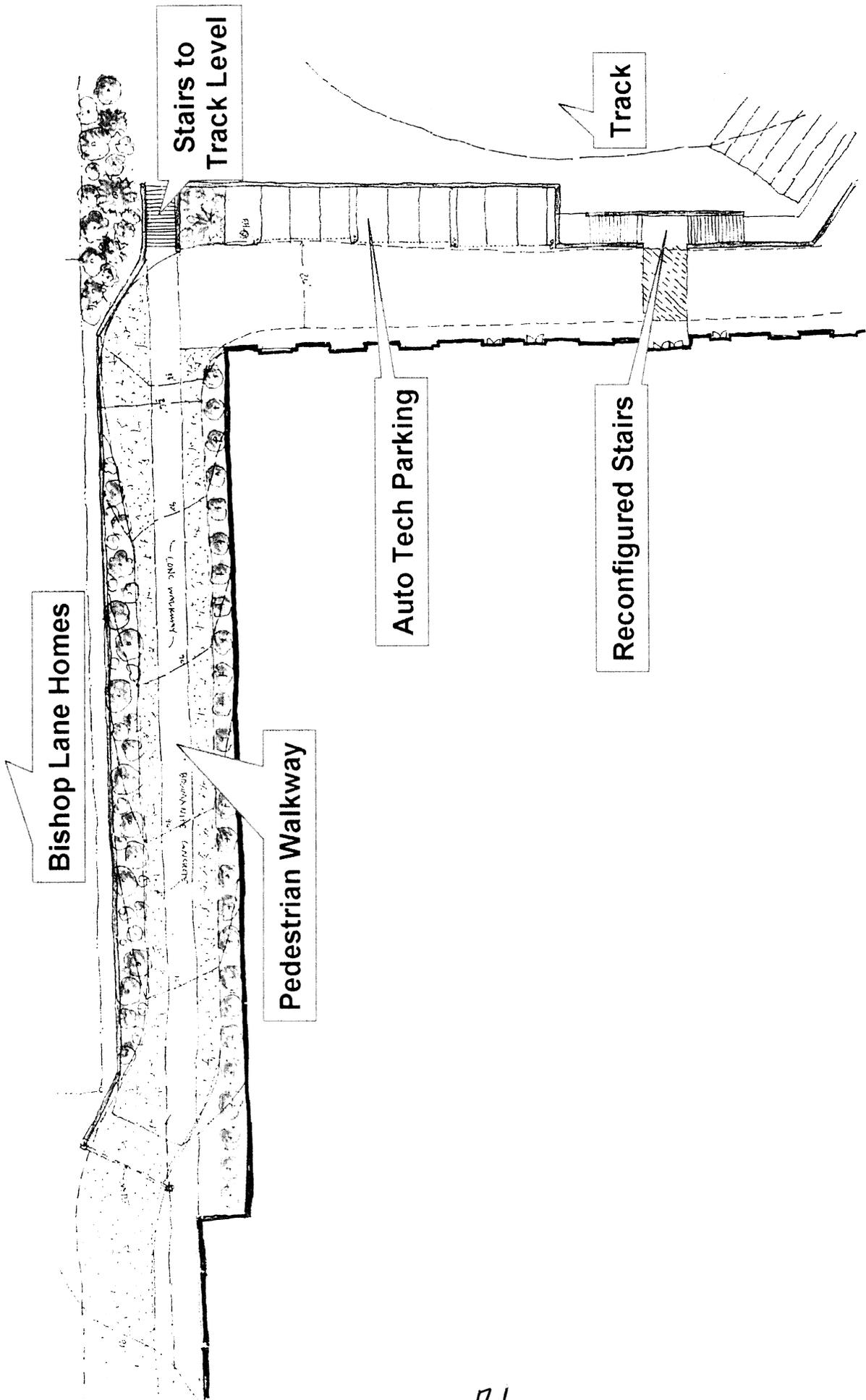


Figure 1
South Pedestrian Access

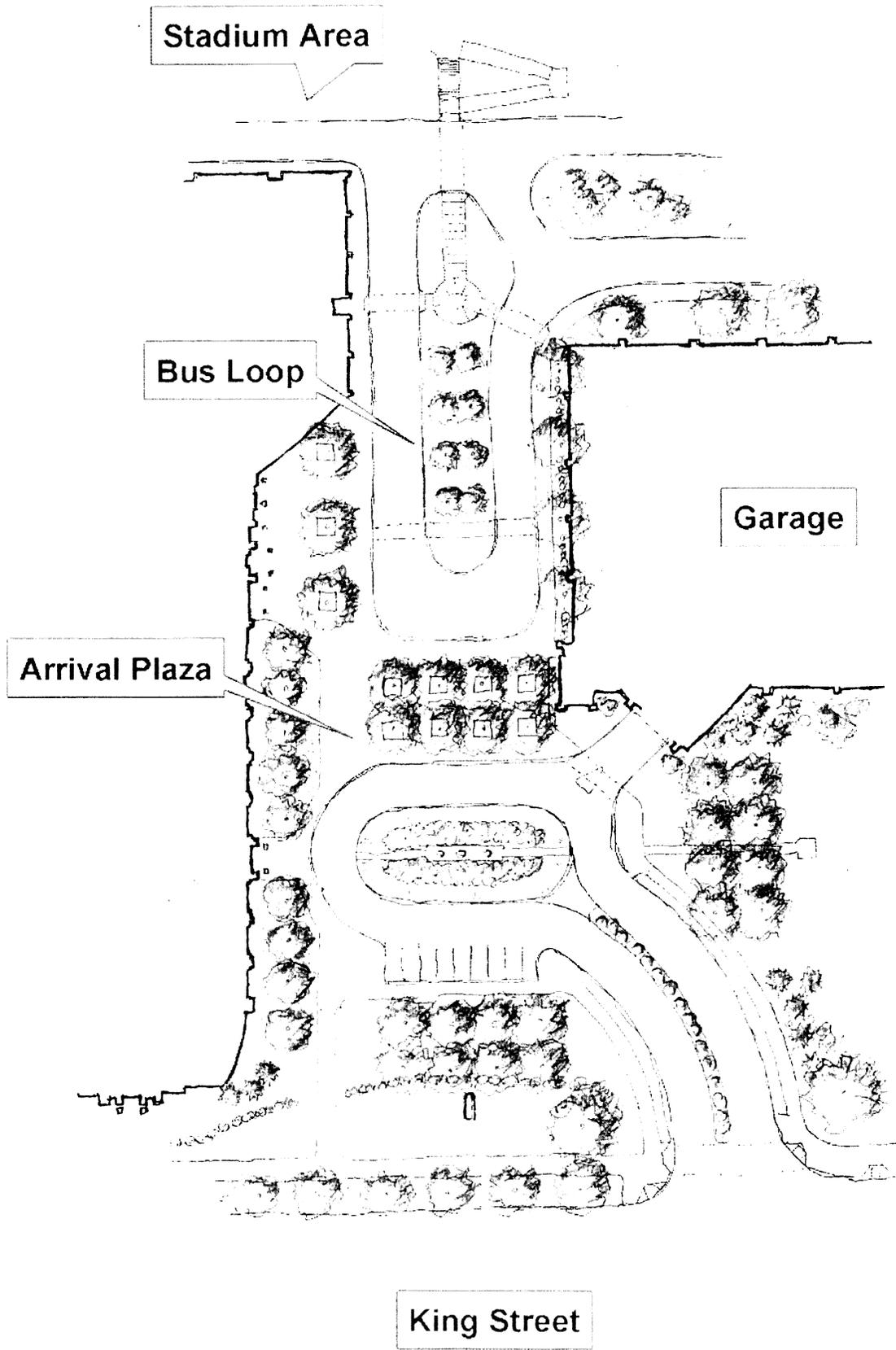


Figure 2
Central Vehicle and Pedestrian Plaz

FIRE FLOW REQUIREMENTS

ESTIMATES OF FIRE FLOW REQUIREMENTS SHALL BE BASED ON THE METHODOLOGY DESCRIBED IN THE INSURANCE SERVICES OFFICE'S (ISO) *FIRE SUPPRESSION RATING SCHEDULE*. THIS METHODOLOGY CONSIDERS BUILDING CONSTRUCTION, OCCUPANCY, ADJACENT EXPOSED BUILDINGS, AND COMMUNICATION PATHS BETWEEN BUILDINGS. (SEE EXAMPLE CALCULATION IN APPENDIX A)

1. ONE AND TWO FAMILY DWELLINGS

THE FIRE FLOW REQUIRED SHALL BE BASED ON THE MINIMUM EXPOSURE DISTANCE AS FOLLOWS:

<u>MINIMUM EXPOSURE DISTANCE</u>	<u>FIRE FLOW (GPM)</u>
0 FT. - 10 FT.	1,500 - 2,000
11 FT. - 30 FT.	1,000 - 1,500
31 FT. AND GREATER	1,000

2. TOWNHOUSES OR MULTIPLEX UNITS

TOWNHOUSES OR MULTIPLEX UNITS (RESIDENTIAL OR PROFESSIONAL) WHERE INDIVIDUAL UNITS ARE NOT SEPARATED BY TWO-HOUR FIRE, PARTY, OR SEPARATION WALLS REQUIRE A FLOW OF 2,500 GPM.

TOWNHOUSES (RESIDENTIAL OR PROFESSIONAL) WHERE INDIVIDUAL UNITS ARE SEPARATED BY A MINIMUM ONE-HOUR FIRE, PARTY, OR SEPARATION WALLS AND APPROVED FIRE SPRINKLER SYSTEMS ESTABLISH FIRE FLOW REQUIREMENTS BASED ON CALCULATIONS FOR **OTHER USES** AS DESCRIBED IN SECTION 3.

MULTIPLEX UNITS (RESIDENTIAL OR PROFESSIONAL) WHERE INDIVIDUAL UNITS ARE SEPARATED BY TWO-HOUR FIRE, PARTY OR SEPARATION WALLS AND APPROVED FIRE SPRINKLER SYSTEMS ESTABLISH FIRE FLOW REQUIREMENTS BASED ON CALCULATIONS FOR **OTHER USES** AS DESCRIBED IN SECTION 3.

NOTE: THE CODE ENFORCEMENT BUREAU RESERVES THE RIGHT TO INCREASE THE REQUIRED FIRE FLOW IF BUILDING CONSTRUCTION ISSUES OR ACCESS FACTORS PRESENT AN UNUSUAL FIRE OR LIFE SAFETY CHALLENGE.

3. OTHER USES

FIRE FLOW REQUIREMENTS ESTABLISHED BY THE PROCEDURES AND FORMULA FOR NEEDED FIRE FLOW DELINEATED BELOW IS BASED ON THE INSURANCE SERVICES OFFICE (ISO) METHODOLOGY.

4. COMPUTATION OF NEEDED FIRE FLOW

THE NEEDED FIRE FLOW SHALL BE CALCULATED AT A MINIMUM 20 PSI RESIDUAL PRESSURE ON THE WATER SYSTEM. THE BASIC FORMULA IS: $NFF_1 = (C_1)(O_1)(X + P)_1$

(A) C_1 = CONSTRUCTION FACTOR / WHERE $C_1 = 18F \sqrt{A_1}$

F = COEFFICIENT RELATED TO TYPE OF CONSTRUCTION:

- F = 1.5 FOR WOOD FRAME CONSTRUCTION (1996 VUSBC TYPES 5A & 5B)
- F = 1.0 FOR ORDINARY CONSTRUCTION (1996 VUSBC TYPES 3A & 3B)
- F = 0.9 FOR HEAVY TIMBER CONSTRUCTION (1996 VUSBC TYPE 4)
- F = 0.8 FOR NONCOMBUSTIBLE CONSTRUCTION (1996 VUSBC TYPES 2A, 2B, AND 2C)
- F = 0.6 FOR FIRE-RESISTIVE CONSTRUCTION (1996 VUSBC TYPES 1A & 1B)

A = EFFECTIVE BUILDING AREA:

A = THE TOTAL AREA OF THE LARGEST FLOOR PLUS:

- CONSTRUCTION TYPE 3 THROUGH 5-50% OF ALL OTHER FLOORS.
- 1) CONSTRUCTION TYPE 1 & 2-25% OF THE AREA NOT EXCEEDING THE OTHER TWO LARGEST FLOORS WHEN ALL VERTICAL OPENINGS HAVE AT LEAST 1½-HOUR FIRE-RATED PROTECTION OR, 2) 50% OF THE AREA NOT EXCEEDING EIGHT OTHER FLOORS WHEN THE VERTICAL OPENINGS ARE UNPROTECTED OR HAVE LESS THAN 1½-HOUR PROTECTION.

(B) O_i = OCCUPANCY FACTOR, WHICH REFLECTS THE COMBUSTIBILITY OF THE OCCUPANCY.

- $O = 0.75$ FOR NON-COMBUSTIBLE
- $O = 0.85$ FOR LIMITED COMBUSTIBLE
- $O = 1.00$ FOR COMBUSTIBLE
- $O = 1.15$ FOR FREE BURNING
- $O = 1.25$ FOR RAPID BURNING

(C) $(X + P)_i$ = EXPOSURE AND COMMUNICATION FACTORS

$$X_i = \frac{1}{1 + \sum_{i=1}^N X_i + P_i}$$

VALUES FOR X AND P ARE DETERMINED FROM CHARTS CONTAINING FACTORS FOR TYPE OF SEPARATION OR CONNECTIONS, SEPARATION DISTANCE.

(D) ADD 500 GPM TO TOTAL FIRE FLOW FOR BUILDINGS WITH A WOOD ROOF.

5. MINIMUM FLOW

FIRE FLOW SHALL NEVER BE LESS THAN 500 GPM FOR A STRUCTURE. FIRE FLOW REQUIRED FOR SINGLE-FAMILY DETACHED DWELLINGS SHALL NEVER BE LESS THAN 1,000 GPM. BOTH VALUES ARE ABSOLUTE MINIMUMS AFTER ALL REDUCTIONS ARE TAKEN.

6. MAXIMUM FLOW

THE MAXIMUM FIRE FLOW SHALL BE AS FOLLOWS, EXCEPT FOR STRUCTURES REQUIRING SPECIAL CONSIDERATION AS DESCRIBED IN SECTION 8:

<u>CONSTRUCTION TYPE</u>	<u>FLOW IN GPM</u>
WOOD, HEAVY TIMBER, ORDINARY CONSTRUCTION	8,000
NONCOMBUSTIBLE OR FIRE-RESISTIVE CONSTRUCTION	6,000

7. REDUCTIONS BASED ON SPRINKLER PROTECTION

THE VALUE OBTAINED FROM THE FORMULA IN SECTION 4, **COMPUTATION OF NEEDED FIRE FLOW**, MAY BE REDUCED BY 50 PERCENT WHEN THE STRUCTURE UNDER CONSIDERATION IS PROTECTED THROUGHOUT WITH AN APPROVED AUTOMATIC SPRINKLER SYSTEM PER THE REQUIREMENTS IN THE **VIRGINIA UNIFORM STATEWIDE BUILDING CODE** AND THE CURRENTLY REFERENCED EDITION OF **NFPA 13 STANDARD FOR THE INSTALLATION OF SPRINKLER SYSTEMS** OR OTHER RELATED FIRE SPRINKLER SYSTEM DESIGN AND INSTALLATION CODES. REDUCTIONS ARE NOT PERMITTED FOR STRUCTURES WITH PARTIAL PROTECTION. REDUCTIONS FOR INSTALLATIONS BASED ON NFPA 13D OR NFPA 13R DESIGNS SHALL BE APPROVED BY THE DIRECTOR OF CODE ENFORCEMENT.

8. SPECIAL CONSIDERATION

THE ABOVE CALCULATION PROCEDURE DOES NOT APPLY TO THE FOLLOWING, WHICH REQUIRE SPECIAL CONSIDERATION AND DIRECT CONSULTATION WITH THE CODE ENFORCEMENT BUREAU:

- A. HIGH HAZARD STRUCTURES
- B. LUMBER YARDS
- C. PETROLEUM STORAGE
- D. REFINERIES
- E. CHEMICAL PLANTS
- F. GRAIN STORAGE
- G. POWER GENERATING FACILITIES
- h. Hazardous manufacturing processes
- i. Paint, flammable liquids storage
- j. High piled combustible storage

EXAMPLE FIRE FLOW ANALYSIS

A new cinema building will be constructed and has a footprint area of 77,680 square feet and a gross area of 134,320 square feet. The building is 3-stories, Type 1B construction, and is classified Use Group A-1 for theaters with the ground floor primarily movie theater seating. To the west of the proposed cinema is a hi-rise office building approximately 85 feet away. To the north and south there is on-grade parking and no structure within 100 feet. To the east there is a future structure planned and it will be within 30 feet of the cinema. All vertical openings are unprotected or have less than 1 ½ hour fire-rated protection. The facility will have full fire sprinkler protection based on the NFPA 13 standard.

$$\text{Needed Fire Flow} = \text{NFF}_i = (C_i)(O_i)(X + P)_i$$

(a) $C_i = \text{Construction Factor where } C_i = 18 F \sqrt{A_i}$

F = coefficient related to type of construction:

- F = 0.6 for fire-resistive construction (1996 VUSBC Types 1A & 1B)

A = effective building area = the total area of the largest floor plus 50% of the area not exceeding eight other floors when all vertical openings are unprotected or have at less than 1½-hour fire-rated protection for Construction Type 1 and 2.

$$A = 77,680 + (134,320 - 77,680) \times .50 = 106,000 \text{ square feet}$$

$$C = 18 \times .6 \times \sqrt{106,000} = 3516 \text{ gpm}$$

- (b) O_i = Occupancy Factor, which reflects the combustibility of the occupancy.
- **O = 1.15 for free burning based on a conservative design approach from undetermined plastic and fabric seating fixtures**
- (c) $(X + P)_i$ = Exposure and Communication Factors from Tables 330.A and 330.B.
 Values for X and P are determined from charts containing factors for type of separation
 or connections, separation distance.

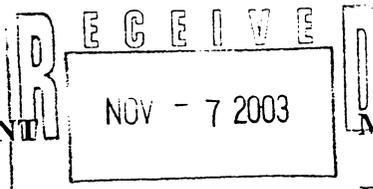
$$X_i = 1 + \sum_{i=1}^n X_i + P_i = 1.0 + \underbrace{(0.10 + 0.0 + 0.19 + 0.0)}_{\text{west north east south}} + 0 = 1.29$$

Needed Fire Flow = (C) x (O) x (1 + X_i + P₁) = 3,516 x 1.15 x 1.29 = 5250 gpm

NOTE: 50% reduction available since a full NFPA 13 sprinkler system will be installed . Therefore,

N. F. F. = 5250 x 0.50 = 2,625 gpm = 2,750 (rounding to the nearest 250 gpm increment)

AMENDED APPLICATION FOR:



Filed: 9/17/03
Revised: 11/7/03/

[X] MASTER PLAN AMENDMENT MPA # 2003-0006 IC

[X] ZONING MAP AMENDMENT P & Z ZONING COMPLIANCE REZ # 2003-0006

PROJECT NAME: T.C. Williams High School

PROPERTY LOCATION: 3300 King Street

APPLICANT Name: Alexandria City Public Schools

Address: 2000 N. Beauregard Street, Alexandria, VA 22311

PROPERTY OWNER Name: City of Alexandria, Virginia

Address: P.O. Box 178, Alexandria, VA 22313-1500

- Interest in property: Owner Contract Purchaser
- Developer Lessee Other School Board control of property

If property owner or applicant is being represented by an authorized agent such as an attorney, a realtor, or other person for which there is some form of compensation, does this agent or the business in which they are employed have a business license to operate in Alexandria, VA:

- yes: If yes, provide proof of current City business license.
- no: If no, said agent shall obtain a business license prior to filing application.

THE UNDERSIGNED certifies that the information supplied for this application is complete and accurate, and, pursuant to Section 301B of the Zoning Ordinance, hereby grants permission to the City of Alexandria, Virginia, to post placard notice in the property which is the subject of this application.

J. Howard Middleton, Jr.
Print Name of Applicant or Agent

Signature

Reed Smith LLP, 3110 Fairview Park Dr.
Mailing/Street Address

703-641-4225 Telephone # 703-641-4340 Fax #

Falls Church, VA 22042
City and State Zip Code

November 7, 2003
Date

FOR CITY STAFF USE ONLY:

Date application received: _____ Fee Paid: \$ _____
Date application completed: _____ Staff Reviewer: _____

ACTION - PLANNING COMMISSION: _____

ACTION - CITY COUNCIL: _____

MPA # 2003-0010

REZ # 2003-0006

SUBJECT PROPERTY

Provide the following information for each property for which an amendment is being requested.
(attach separate sheets if needed)

Address Tax Map-Blk-Lot	Land Use		Master Plan Designation		*Zoning Designation		Frontage (feet)	Land Area (acres)
	Existing/Proposed	Existing/Proposed	Existing/Proposed	Existing/Proposed	Existing/Proposed	Existing/Proposed		
<u>041.00-02-01</u>	<u>Public School</u>	<u>Public School</u>	<u>P</u>	<u>Inst.</u>	<u>POS</u>	<u>R-20</u>	<u>98.62 ft</u>	<u>8.5 acres*3.79</u>
_____	_____	_____	_____	_____	_____	_____	_____	_____
_____	_____	_____	_____	_____	_____	_____	_____	_____
_____	_____	_____	_____	_____	_____	_____	_____	_____
_____	_____	_____	_____	_____	_____	_____	_____	_____
TOTAL								

PROPERTY OWNERSHIP:

Individual Owner Corporation or Partnership Owner

Identify each person or individual with ownership interest. If corporation or partnership owner, identify each person with more than 10% interest in such corporation or partnership.

1 Name: City of Alexandria, Virginia Extent of Interest: 100%

Address: P.O. Box 178, Alexandria, VA 22313

2 Name: _____ Extent of Interest: _____

Address: _____

3 Name: _____ Extent of Interest: _____

Address: _____

4 Name: _____ Extent of Interest: _____

Address: _____

JUSTIFICATION FOR AMENDMENT:
 (attach separate sheets if needed)

1. Explain how and why any proposed amendment(s) to the Master Plan are desirable, beneficial to surrounding properties, in character with the applicable Small Area Plan and consistent with City policies:

The Alexandria City Public Schools (ACPS) is requesting amendments to the Taylor Run/Duke Street Small Area Plan (Small Area Plan) to accommodate the construction of the new T.C. Williams High School (TC) adjacent to its present location. TC is presently located in an area designated as Institutional in the Small Area Plan. This area totaling approximately 19.8 acres will remain Institutional. An area approximately 8.5 acres in size which is presently utilized for a portion of the TC parking area, temporary classroom trailers and practice fields is designated P. ACPS is requesting that this P designation for the 8.5 acres parcel be changed to Institutional. This constitutes an amendment to Map 8, page 22 of the Small Area Plan. It should be noted that on Map 8, the designation of P represents "Parks, Public Schools and Open Space"; therefore, the change in designation from P to Institutional may not even be necessary inasmuch as public schools and open space are included within the P designation.

ACPS is also requesting a change in Map 10 of the Small Area Plan. Map 10 specifies the zoning for the 8.5 acre parcel as Public Open Space (POS). ACPS requests that a portion of the POS zoned land (165,095 sq. ft.) presently utilized by TC for parking and temporary classroom trailers, be rezoned to the R-20 Zone.

2. Explain how and why the proposed amendment to the Zoning Map(s) is consistent with the proposed amendment to the Master Plan, or, if no amendment to the Master Plan is being requested, how the proposed zoning map amendment is consistent with the existing Master Plan:

The primary purpose of this application is to plan all the area presently used for TC for Institutional land use and to zone all of the TC site except for the practice fields on the southeast corner in the R-20 Zone. The R-20 Zone permits public school use. The zoning amendment will place the new T.C. Williams High School in conformity with the Small Area Plan.

At the present time, the existing T.C. Williams High School is in non-conformance with the Zoning Ordinance by exceeding the maximum floor area ratio permitted in the R-20 Zone on which the building is located. The proposed amendment retains the large parcel (19.8 acres) in the R-20 zone and rezones a portion of the POS land to R-20, thereby creating a new parcel 23.68 acres in size in the R-20 zone for the new TC building. This will allow a portion of the building to be constructed near Chinquapin Drive and permit a higher level of floor area ratio on the site. More than half of the 8.5 acre parcel (206,228 sq. ft.) would remain in the POS zone and be utilized for practice fields. This realignment of the zoning configuration would permit construction of the TC building nearer Chinquapin Drive where the parking lot is now located.

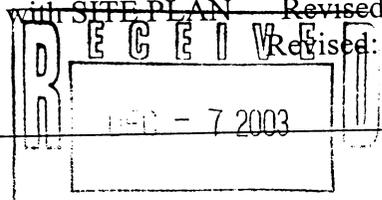
3. Explain how the property proposed for reclassification will be served adequately by essential public facilities and services such as highways, streets, parking spaces, police and fire, drainage structures, refuse disposal, water and sewers, and schools.

T.C. Williams High School has frontage along King Street, which is, of course, a major roadway extending from the Potomac River on the east to Leesburg and beyond to the west. The transportation plan designates King Street as an arterial and is sufficient to service the site. In addition, existing facilities for police and fire protection and utilities such as water and sewer are also adequate to service the site. A plan for the new High School will include appropriate and adequate drainage facilities.

4. If this application is for conditional zoning approval pursuant to Section 11-804 of the Zoning Ordinance, identify all proffered conditions that are to be considered part of this application (see Zoning Ordinance Section 11-804 for restrictions on conditional zoning):

Not applicable.

APPLICATION for DEVELOPMENT SPECIAL USE PERMIT with SITE PLAN DSUP # 2002-0044 Filed: 9/17/03 Revised: 11/7/03 Revised: 12/08/03



PROJECT NAME: T.C. Williams High School

PROPERTY LOCATION: 3300 King Street

TAX MAP REFERENCE: TM 32.04-09-07; TM 41.02-02-01; TM 032.00-09-07; TM 041.00-02-01 ZONE: R-20, POS

APPLICANT Name: Alexandria City Public Schools

Address: 2000 N. Beauregard Street, Alexandria, VA 22311

PROPERTY OWNER Name: City of Alexandria, Virginia

Address: P.O. Box 178, Alexandria, Virginia 22313-1500

SUMMARY OF PROPOSAL: Request special use permit with site plan and resubdivision for new T.C. Williams High School with increase in floor area ratio (FAR) and increase in height.

MODIFICATIONS REQUESTED: (1) Side yard setback modification of varying distances up to 25 feet (Sec. 3-106(A)(2)); (2) Front yard setback modification of varying distances up to 26 feet (Sec. 3-106 (A)(1))

SUP's REQUESTED: (1) Request increase in FAR and height for public school in R-20 Zone; (2) Request placement of public school trailers in R-20 zone and/or POS Zone in conjunction with T.C. Williams High School; (3) Request approval for freestanding school sign pursuant to Section 9-201(A)(8) of the Zoning Ordinance

THE UNDERSIGNED hereby applies for Development Site Plan, with Special Use Permit, approval in accordance with the provisions of the Zoning Ordinance of the City of Alexandria, Virginia.

THE UNDERSIGNED, having obtained permission from the property owner, hereby grants permission to the City of Alexandria to post placard notice on the property for which this application is requested, pursuant to Article XI, Section 11-301(B) of the 1992 Zoning Ordinance of the City of Alexandria, Virginia.

THE UNDERSIGNED also attests that all of the information herein provided and specifically including all surveys, drawings, etc., required of the applicant are true, correct and accurate to the best of his knowledge and belief.

J. Howard Middleton, Jr. Print Name of Applicant or Agent

Signature

Reed Smith LLP, 3110 Fairview Park Dr. Mailing/Street Address Falls Church, VA 22042 City and State Zip Code

703-641-4225 Telephone # 703-641-4340 Fax # December 08, 2003 Date

DO NOT WRITE BELOW THIS LINE - OFFICE USE ONLY

Application Received: Fee Paid & Date: \$

Received Plans for Completeness: Received Plans for Preliminary:

ACTION - PLANNING COMMISSION:

ACTION - CITY COUNCIL: 82

Development Special Use Permit with Site Plan (DSUP) #

All applicants must complete this form.

Supplemental forms are required for child care facilities, restaurants, automobile oriented uses and freestanding signs requiring special use permit approval.

1. The applicant is the (*check one*):

Owner

Contract Purchaser

Lessee

Other: School Board control of property

State the name, address and percent of ownership of any person or entity owning an interest in the applicant, unless the entity is a corporation or partnership in which case identify each owner of more than ten percent.

Political Subdivision of Commonwealth of Virginia

If property owner or applicant is being represented by an authorized agent such as an attorney, realtor, or other person for which there is some form of compensation, does this agent or the business in which the agent is employed have a business license to operate in the City of Alexandria, Virginia?

Yes. Provide proof of current City business license

No. The agent shall obtain a business license prior to filing application, if required by the City Code.

Development Special Use Permit with Site Plan (DSUP)

NARRATIVE DESCRIPTION

2. The applicant shall describe below the nature of the request in detail so that the Planning Commission and City Council can understand the nature of the operation and the use, including such items as the nature of the activity, the number and type of patrons, the number of employees, the hours, how parking is to be provided for employees and patrons, and whether the use will generate any noise. If not appropriate to the request, delete pages _____.
(Attach additional sheets if necessary)

The purpose of this Special Use Permit Application is to request an increase in floor area ratio (FAR) and height for the new T.C. Williams High School (TC). Located on King Street within the Taylor Run/Duke Street Small Area Plan, the TC site is in a mixed-use area, including residential, institutional, church, commercial and park use. T.C. Williams presently houses tenth through twelfth grade students in a building with approximately 355,000 square of floor area. The Alexandria City Public Schools (ACPS) proposes to construct a new T.C. Williams High School with approximately 458,000 net square feet of floor area and a parking structure of approximately 72,000 net square feet with 416 parking spaces. The existing TC high school building will be demolished.

Zoning

The total land area controlled by the Alexandria School Board for high school use is approximately twenty-eight acres, which is a part of a larger parcel (approximately fifty-one acres) owned by the City of Alexandria. The Alexandria School Board controlled area (twenty-eight acres) is presently zoned R-20 (19.8 acres) and POS (8.5 acres). The accompanying Master Plan and Zoning Map Amendment requests that the entire twenty-eight acres be planned for Institutional use and that a portion of the POS land be rezoned to the R-20 Zone. Taking into account the new 23.67 acre parcel zoned R-20, the new T.C. Williams High

School will be constructed with a floor area ratio of 0.51, which exceeds the allowable FAR in the R-20 Zone. This Application therefore requests a Special Use Permit to increase the floor area above that allowable in an R-20 Zone, in accordance with the text amendment recently approved by the City Council. Along with the realignment of the zoning line, a resubdivision is requested creating two lots, both controlled by the School Board: one lot zoned R-20, the second, POS.

The new TC will be constructed to various heights at different points of the building. To the rear of the building closest to the residential area, the height will be fifteen feet. The height will then rise to thirty feet for the new gymnasium and continuing further to the north, away from the residential zoned area, the building will rise to a height of approximately fifty-three feet. The R-20 Zone permits a building for church or school use to rise to forty feet. ACPS is therefore requesting a Special Use Permit to increase the height to approximately fifty-three feet in accordance with the recent text amendment approved by City Council.

ACPS is also requesting a side yard setback modification along the lot line adjacent to Chinquapin Drive. This modification, of varying distances up to 25 feet, is along the Drive and the park and has no adverse impact on any neighboring uses. In addition, the applicant is requesting a front yard setback modification along King Street. The setback requirement is 70 ft.; the modification request allows the building at the two corner pods to extend to a point 44.3 ft. and 58.6 ft., respectively, from the King Street property line, with the middle portion of the building set back 105.2 ft. from King Street.

Prior to 1992, the 8.5 acre parcel now zoned POS was zoned residential and could be taken into account in the calculation of FAR for the high school. Before construction of the existing school building, the 19.8 acre site was zoned R-8. In light of the two rezonings, the present existing school building is in non-conformance with the floor area ratio requirement in the Zoning Ordinance.

As noted above, the School Board controlled property utilized for TC (28.4 acres) is part of a larger tract owned by the City of Alexandria with a total of fifty-one acres of land. The new larger high school building will be part of a 51 acre tract for public uses, including TC, the football stadium, recreation center, soccer fields, tennis courts and other play areas, as well as open space and wooded area.

New High School

ACPS initiated its study to determine the high school needs for the City of Alexandria for the long term future. This process was initiated early in 2001 and a charrette was held in May. Over the course of the next year, meetings were held with City agencies, the Educational Specifications Committee, a Steering Committee and the Seminary Hill Civic Association Task Force. A joint School Board/City Council meeting to discuss T.C. Williams' options was held on September 17, 2002. A Town Meeting was held on September 19, 2002. The culmination of this long process was the decision to construct a new T.C. Williams High School on the existing site. The new high school will be constructed just to the east of the existing building, which will be demolished, and a parking structure will then be constructed.

The new TC will be constructed to accommodate approximately 2500 students in grades 10 through 12. The building plans will reflect a facility with approximately 458,000 square feet of floor space, all as shown on the accompanying documentation, including site plans, landscape plans and building elevations. The building will consist of a core academic area with separate pods and a total of approximately 100 classrooms; a special services area; a media center; a communications cluster; visual arts area; physical education athletics area; a career technical education center; a student center; building and ground services; and an administration area. All facilities are shown on the site plan.

The building is designed to effectuate the Educational Program Design Principles adopted by the T.C. Williams Educational Program Committee:

1. To establish smaller learning communities so that every student enjoys the support of at least one teacher advocate.
2. To foster effective working relationships by ensuring that all teachers enjoy the opportunity to collaborate on instruction.
3. To ensure a flexible, student-centered educational program that recognizes the individual learning styles of each student.
4. To recognize and celebrate the talents of each individual, as well as the rich diversity of the entire school community.
5. To guarantee that every student graduates with a diploma and/or certificate indicating preparation for postsecondary educational or career options that support/lifelong learning.
6. To assist students in developing effective technical skills which support and enhance all aspects of the curriculum.
7. To ensure that every student is connected to the community as a part of his/her high school experience.
8. To implement family involvement initiatives that encourage collaboration and positive student outcomes.

The new T.C. Williams High School will be a state-of-the-art high school housed in an architecturally attractive building as shown on the elevations. We therefore submit that the Special Use Permit is fully justified on the basis of the education needs of the community and the zoning analysis presented in this Application.

Transportation

In addition to the new school facilities, there will be improved access onto the site. As shown on the site plan, school buses will enter at a location opposite Radford Street and proceed around to the rear of the parking structure and to the front of the building on the northwest side. There will be approximately 30 buses arriving at TC on a typical day. Care will be taken in the administration of the transportation system to insure that the buses only remain at the site for a short period of time to avoid inconvenience to the neighboring community. No cars will enter the bus entrance, except on weekends and special events. Parents and family members will drop off students at the main entrance of the building or from the Chinquapin Drive entrance on the eastern side of the building. It is anticipated that allowing students to be dropped off at two locations will ease the traffic situation in the morning and afternoon hours.

A traffic analysis is included with this Application in a study entitled "T.C. Williams High School, Traffic Impact Study, City of Alexandria, Virginia" prepared by Wells & Associates, LLC, dated March 4, 2003. In summary, the traffic study concludes that the driveway intersections at Radford Street and Kenwood Avenue will operate at acceptable levels of service. The intersection at Chinquapin Drive will operate at capacity during peak hours; however, the demand should be lessened by the addition of a second drop off/pick up location

off Kenwood Avenue at the main entrance to the building, as stated above. A copy of the study is enclosed with this Application.

The parking structure will contain 416 spaces with an additional 28 surface parking spaces near the main entrance at Kenwood Avenue and in the rear of the building. Parking studies have been prepared by the transportation consultant and are submitted along with the Traffic Impact Study. However, the number of spaces available has been adjusted during the planning process, and the total parking needs will change depending on major events, arrangements for use of Chinquapin Drive for parking and availability of off-site locations.

The off-street parking spaces provided will be sufficient with a strong parking management plan. A limited number of students will be permitted to use the parking structure, with most of the spaces designed for faculty and staff use. It is our understanding that a certain number of spaces will continue to be available along Chinquapin Drive; and ACPS staff will continue to cooperate with City staff on parking needs for both school and recreational facility use.

Key elements of a parking management plan will include the following points:

1. Student parking permits will be required for parking on the high school parking lots and Chinquapin Drive. Permits will be limited when the new school is built.
2. Employees and students will be encouraged to carpool whenever possible.
3. The school system will provide incentives to encourage employees to use public transportation such as Dash passes or Metro Check which allows

employees to purchase fare cards or vouchers for Metro/Metro bus and Dash.

4. Certain adult education classes will be relocated from T.C. Williams to another ACPS facility.
5. The schedule for some Adult Education and high school education programs will be staggered.
6. Scheduling of major events will be controlled in order to avoid peak crowds, thereby decreasing the number of spaces needed.
7. Additional scheduling options to limit the peak number of spaces needed will be explored.
8. ACPS will continue working with the Department of Recreation, Parks and Cultural Affairs to create opportunities for programs on both sites to share parking facilities as needed.
9. Students and staff will be encouraged to ride bicycles to school and work. Bicycle racks will be provided to provide a secure place to store the bicycle during the day.

In addition, ACPS is communicating with owners of other off-street parking areas in the neighborhood to make arrangements for special events. These arrangements will ease the parking situation in the vicinity during the infrequent occasions when special events are held at the new high school.

Summary

T.C. Williams High School has provided an excellent education in the City of Alexandria for many years. However, in the near future, the physical facility will limit the ability of ACPS to continue to provide the superior educational

experience. The classrooms are becoming inadequate, the design of the building no longer affords an opportunity to institute new educational techniques and the facility does not permit the implementation of the technological advancements that are necessary for contemporary education of high school students. The new building will allow an increase in the number of students to the expected level of approximately 2500 and will enable ACPS to implement new educational techniques. Also, the new building has an attractive architectural design, with landscaping on all sides of the building. The landscape plan will not only provide an attractive frontage along King Street, but also provide an attractive buffer for the adjacent residential neighborhoods.

In light of these reasons, ACPS respectfully requests approval of the Special Use Permit Application.

Development Special Use Permit with Site Plan (DSUP) #

3. How many patrons, clients, pupils and other such users do you expect?
Specify time period (i.e., day, hour or shift).

Approximately 2500 students (daytime); in addition, evening Adult Education Classes with an unknown number of students at this time.

4. How many employees, staff and other personnel do you expect?
Specify time period (i.e., day, hour or shift).

Approximately 300 faculty and staff.

5. Describe the proposed hours and days of operation of the proposed use:

Day	Hours	Day	Hours
<u>Monday-Friday</u>	<u>8:00am-4:00pm</u>		
<u>After school student activities</u>			
<u>Evening Adult Education Program</u>			

6. Describe any potential noise emanating from the proposed use:

- A. Describe the noise levels anticipated from all mechanical equipment and patrons.

Usual noise from mechanical equipment. School activity is within the building except for use of play fields. In addition, a public address system is frequently utilized for sports activity outside and for announcements inside the school building.

- B. How will the noise from patrons be controlled?

A plan will be devised to ensure that announcements will be made only within the School building; in addition, the public address system for outdoor sporting events will be adjusted to diminish the sound extending into the neighborhood.

7. Describe any potential odors emanating from the proposed use and plans to control them:

The only odors emanating from the building would be from the kitchen and food preparation. Modern equipment will be utilized to keep the kitchen odors within a defined area inside the building.

Development Special Use Permit with Site Plan (DSUP) #

8. Provide information regarding trash and litter generated by the use:

A. What type of trash and garbage will be generated by the use?

School trash from staff and students; additional trash from kitchen activity.

B. How much trash and garbage will be generated by the use?

Normal institutional trash will be collected by City sanitation officials.

C. How often will trash be collected?

Daily collection of trash by City of Alexandria

D. How will you prevent littering on the property, streets and nearby properties?

Property is kept free of litter by staff.

9. Will any hazardous materials, as defined by the state or federal government, be handled, stored, or generated on the property?

Yes. No.

If yes, provide the name, monthly quantity, and specific disposal method below:

10. Will any organic compounds, for example paint, ink, lacquer thinner, or cleaning or degreasing solvent, be handled, stored or generated on the property?

Yes. No.

If yes, provide the name, monthly quantity, and specific disposal method below:

Development Special Use Permit with Site Plan (DSUP) #

- C. Where is the required parking located? (*check one*) [] on-site [] off-site.

If the required parking will be located off-site, where will it be located:

Pursuant to section 8-200(C) of the zoning ordinance, commercial and industrial uses may provide off-site parking within 500 feet of the proposed use, provided that the off-site parking is located on land zoned for commercial or industrial uses. All other uses must provide parking on-site, except that off-street parking may be provided within 300 feet of the use with a special use permit.

- D. If a reduction in the required parking is requested, pursuant to section 8-100(A)(4) or (5) of the zoning ordinance, complete the PARKING REDUCTION SUPPLEMENTAL APPLICATION.

14. Provide information regarding loading and unloading facilities for the use:

- A. How many loading spaces are required for the use, per section 8-200 (B) of the zoning ordinance? N/A

- B. How many loading spaces are available for the use? 2

- C. Where are off-street loading facilities located? south (rear) side of building near the stadium.

- D. During what hours of the day do you expect loading/unloading operations to occur?

Occasional delivery over course of daytime hours; delivery early morning for kitchen supplies.

- E. How frequently are loading/unloading operations expected to occur, per day or per week, as appropriate?

Occasional, as necessary, including each morning for kitchen supplies.

15. Is street access to the subject property adequate or are any street improvements, such as a new turning lane, necessary to minimize impacts on traffic flow?

Access is from King Street, a four lane arterial roadway. A right turn lane along King Street at the Kenwood Avenue entrance will be constructed. Improvements at the entrances opposite Radford Street and at Chinquapin Drive will be completed in accordance with the Site Plan.

Rationale for Building a New T. C. Williams High School

Why a New T.C.? From a Facility Perspective ...

- As the world changes around us, so must our schools to make sure our students will be equipped with the skills and knowledge to succeed, no matter if they pursue post-secondary education or go directly into the workplace.
- The current building has served thousands of students and community groups well over the past 38 years. And it shows! Ceilings drip, students swelter or freeze in windowless classrooms that lack multiple outlets for today's technology and teachers scramble for what little planning space they can find. Clearly it's time for a new facility that can provide our students with a 21st century education.
- When we examined in detail the cost of renovating versus rebuilding this facility, we learned that the two prices were comparable. By choosing to totally rebuild the facility instead of making piecemeal renovations, the Alexandria City School Board opted to provide a completely new and state of the art learning environment that will be filled with high tech classrooms, inviting spaces for reading and working, as well as a cafeteria that will be able to accommodate all students.
- Integrated into this new facility will be "sustainable design" features that include underground water storage tanks that will capture, sanitize, and recycle rainwater for toilet flushing, for cooling, and for irrigating athletic fields. Another feature will be the use of ceramic tile on hallway walls instead of paint. In addition, Moseley Architects has recommended more than 30 other sustainable design elements to incorporate into the new building. These include erosion and sediment control, reduced light pollution and increased use of natural daylight, recycled building materials, and water efficient landscaping.
- The combination of these elements could earn the building a silver LEED--or Leadership in Energy and Environmental Design--certification rating. This level of certification could be the highest attained by any high school in the country, which could make T.C. Williams a national model for other schools.

Why a New T.C.? From an Educational Program Perspective ...

- Research clearly shows that students perform better in smaller learning communities. Students in the new T.C. will be organized in smaller learning groups for their core courses and then take their elective and specialty courses with students from across the entire school.

- In high school, one of the most important factors in a student's success is making sure that his or her relationship with the school is a positive one. For this reason, we will be flexible in how we offer classes. For example, we plan to offer courses outside of the traditional school day to accommodate those students who work to support their families.
- We will be building upon our strong partnerships with the Northern Virginia Community College and area businesses to provide more opportunities for dual high school/college credit and apprenticeships.
- And through a new facility, we will have a decentralized administrative and guidance support program that places adults throughout the building so that students can easily interact with them and build positive relationships to ensure their success at all levels.
- Principal John Porter and Executive Director of Secondary Programs Margee Walsh have been working with the T.C. Educational Program Committee over the past 18 months to plan the programmatic organization of the new T.C. The staff has spent many hours working to develop an educational program that is innovative, flexible, and most importantly, prepares students for entering the 21st century workplace.
- This past summer T. C. won a federal Smaller Learning Communities Planning Grant that supports ongoing program development with the T. C. staff as well as with families, students and community stakeholders. During the months of February 2004 through May 2004, many focus groups and other information sharing activities will occur so that the viewpoints of all interested groups can be included in the planning process.
- Later this spring T. C. will apply for a two-year federal Smaller Learning Communities Implementation Grant that will provide \$250,000 annually to support staff, student, family and community training in building effective smaller learning communities that preserve all of the rich tradition and success that is "Titan Pride."
- In late spring Mr. Porter will build a committee of alumni, community leaders and current students who will work together to advise us on what significant aspects of the current building need to be preserved in the new building, including such items as the Gary Bertier Gymnasium, the Stacie Bradley Memorial Hall, the Earl Lloyd exhibit, and other important aspects of the high school's history.

Chronology

- This building represents a large financial investment, as well as a huge emotional investment, by our community. The leaders of this school division have been, and will continue to be, mindful of the incredible responsibility that we have undertaken to create

a new T.C. Williams that will serve our community for at least another 50 years. Over the last three years, we have met frequently with neighbors, civic associations, City agency heads, and every person or group that has expressed an interest in this new building. Our dialogue has been honest, positive, and productive.

- In May 2001, a large group representing diverse interests came together in a two-day charrette to discuss the possibilities of what a new T.C. could look like.
- About a year later, a group that represented the community, the City, and the schools was convened to explore and settle on the educational specifications of the building. These specifications represented broad decisions on what a high school should look and feel like to meet the needs of students for the next 50 years.
- Then, based on this information, a conversation began among stakeholders about whether T.C. should be renovated or rebuilt. These stakeholders included the City Council, City agency heads, School Board members, civic association groups, and parents.
- In the fall of 2002, more than 250 community members attended a town meeting hosted by the School Board to review and comment on the site options for the building.
- Following the town meeting, a joint session of the School Board and City Council continued the discussion about where to site the building. Out of that meeting, came guiding principles that we have incorporated into our discussions and actions.
- In November of 2002, a public hearing was held where members of the Alexandria community voiced their opinions and support of the project.
- In December of 2002, the Alexandria City School Board unanimously voted to rebuild the T.C. Williams High School on its current site, and to incorporate sustainable design concepts into the building.
- Throughout the past eight months, the architects have worked closely with school staff and City staff to create a building that will serve the diverse needs mentioned in the planning meetings.
- This past Fall a joint public hearing was held with the City's Planning and Zoning Department and the schools. Several hundred interested citizens joined us in the T. C. cafeteria to discuss the site plan, the high school and garage structures, and the construction phasing schedule.
- We have agreed to form a joint committee with neighbors most directly affected by the building plans. This committee will serve as a conduit for resolving quickly any concern that develops during the construction process. A similar committee will be formed when the new building opens so that we can maintain this all-important dialog.

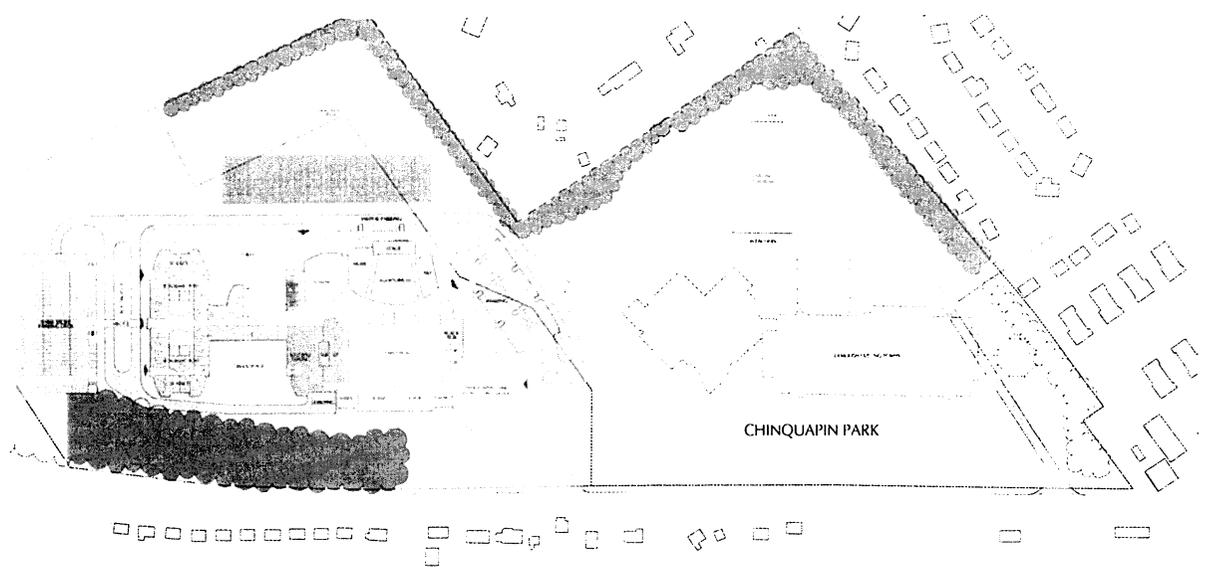
- The Mayor and the School Board Chairman decided to form a task force consisting of the Mayor, a member of City Council, the School Board Chairman, the Superintendent of Schools, and the City Manager to monitor the progress of the project.
- School division leadership appreciates the outstanding cooperation we have received from the Planning and Zoning Department, as well as related City agencies, as we have worked to finalize the submission of the zoning request, the special use permit, site plans and detailed construction documents. We remain committed to giving this project our full attention and consistent stewardship. We would like to thank members of the Planning Commission, the Mayor, members of City Council and the citizens of Alexandria for their generous support of this endeavor.

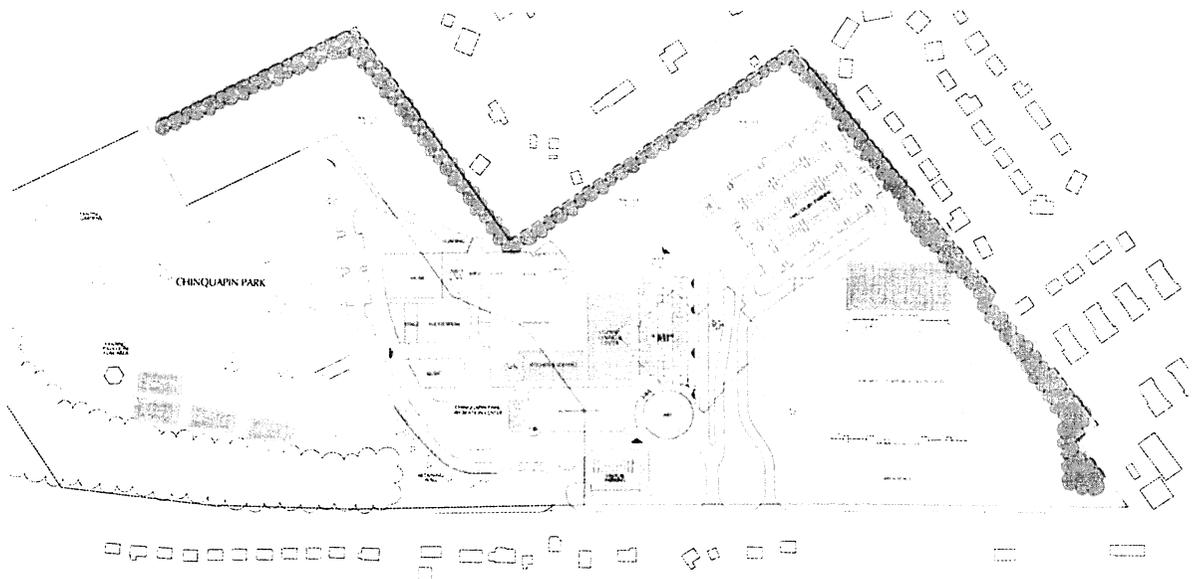
T. C. WILLIAMS HIGH SCHOOL PLANNING Revised 12/17/03
MEETINGS AND ACTIVITIES

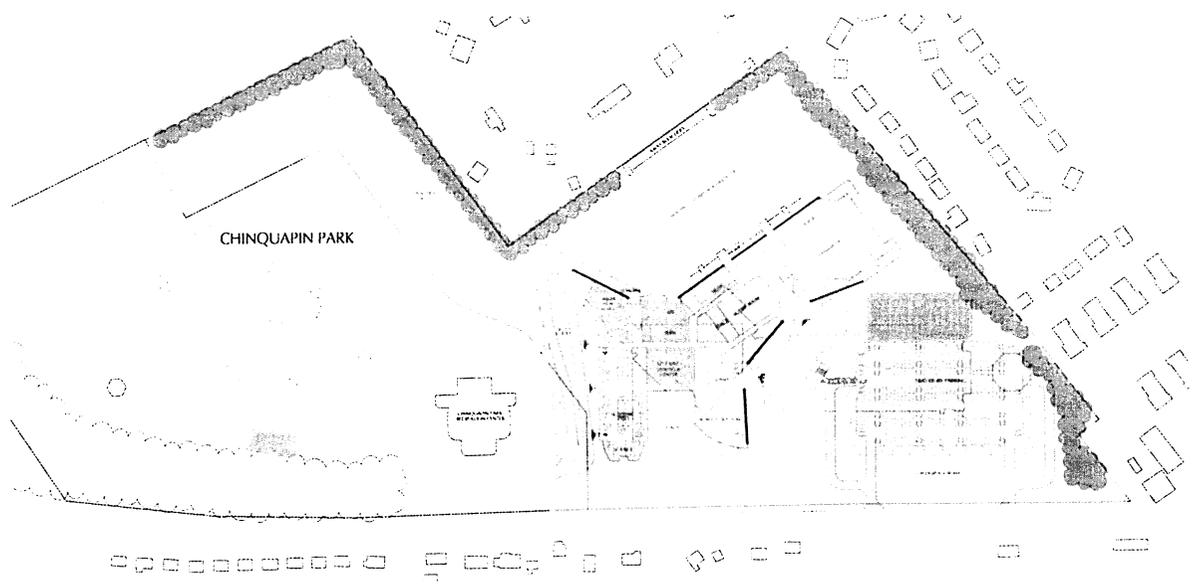
DATE	ACTIVITY/MEETING
December 10, 2000	Administration Retreat on T. C. Williams
May 17 & 18, 2001	Charrette on T. C. Williams
January 30, 2002	Meeting with City Agencies to Discuss the T. C. Williams and Minnie Howard Projects
March 11, 2002	Educational Specifications Committee Meeting
March 19, 2002	Educational Specifications Committee Meeting
March 20, 2002	Educational Specifications Committee Meeting
April 11, 2002	Educational Specifications Committee Meeting
April 12, 2002	Educational Specifications Committee Meeting
May 15, 2002	Steering Committee Meeting
June 5, 2002	Steering Committee Meeting
June 7, 2002	T. C. Williams - 6 options Shared with Planning and Zoning and other City Departments
June 13, 2002	School Board Work Session on T. C. Williams and Minnie Howard
August 6, 2002	Steering Committee Meeting
August 7, 2002	Meet with Seminary Hills Civic Association Task Force
August 24, 2002	School Board Retreat
September 10, 2002	Meet with City Agencies on T. C. Williams Options
September 17, 2002	Joint School Board/City Council Meeting on T. C. Williams Site Options
September 19, 2002	Town Meeting
September 23, 2002	City/Schools Meeting
September 24, 2002	T. C. Williams and Minnie Howard Concepts Reviewed with Planning and Zoning and other City Departments
October 3, 2002	Meet with Recreation Department and other City Departments on Joint Uses

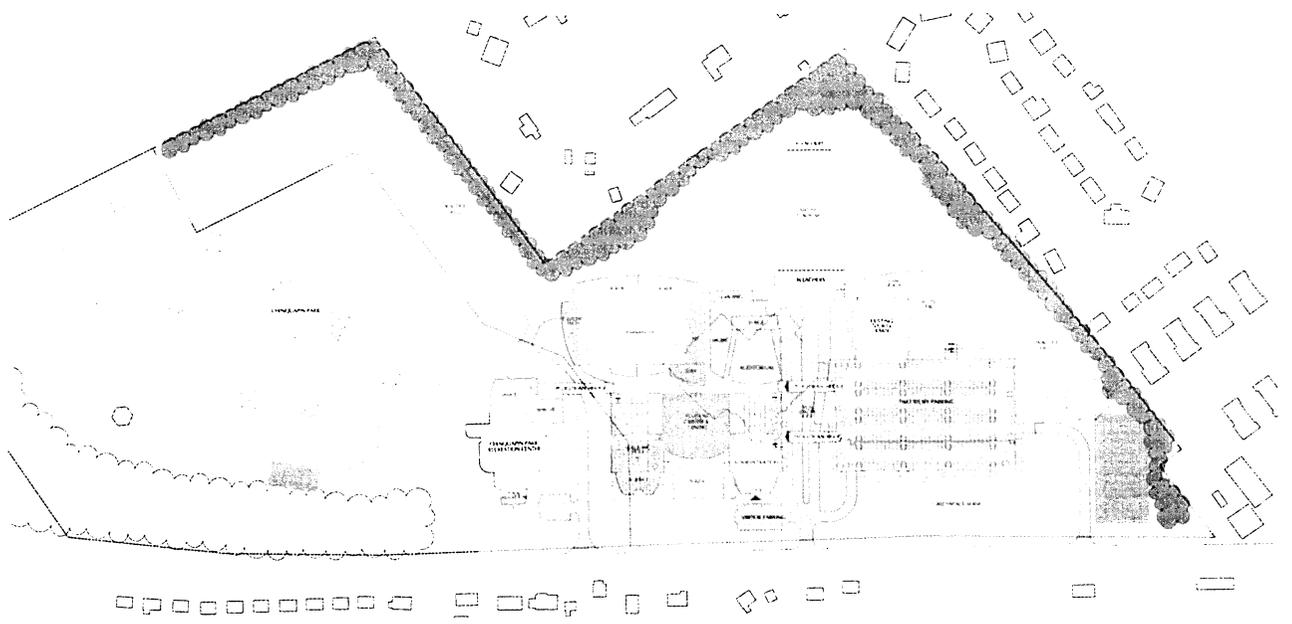
DATE	ACTIVITY/MEETING
October 15, 2002	School Board Work Session on T. C. Williams Options
October 28, 2002	City/Schools Meeting
November 7, 2002	Public Hearing at T. C. Williams
November 19, 2002	Meeting with Planning and Zoning Department to Outline City Development Review Process and Time Line
November 25, 2002	City/Schools Meeting
December 11, 2002	Meeting with Planning and Zoning Department to Discuss Approval Process Time Line
December 19, 2002	Meet with Recreation Department and other City Departments on Joint Uses
January 13, 2003	Meet with Recreation Department and other City Agencies on Joint Uses
January 22, 2003	Green Forum on T. C. Williams
January 27, 2003	City/Schools Meeting
February 24, 2003	City/Schools Meeting
February 25, 2003	School Board Work Session on LEED Certification
February 27, 2003	Meet with Planning and Zoning Department
March 6, 2003	Meet with Planning and Zoning Department and other City Agencies
March 12, 2003	Meet with Seminary Hills Civic Association
March 17, 2003	Meet with Seminary Heights Civic Association and Oakland Baptist Church
March 21, 2003	Parking Subcommittee Meets
March 31, 2003	City/Schools Meeting
April 2, 2003	Meet with Recreation Department and Planning and Zoning on Joint Uses of Chinquapin Fields and Parking
April 3, 2003	Parking Subcommittee Meets

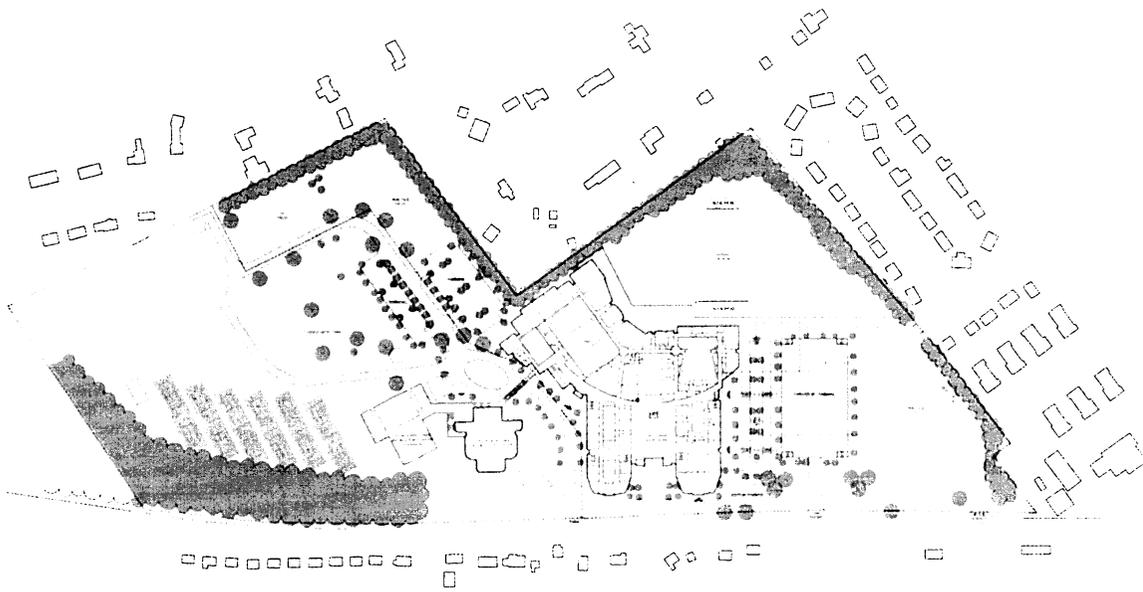
DATE	ACTIVITY/MEETING
April 15, 2003	Meet with Recreation Department and Planning and Zoning on Joint Uses of Chinquapin Fields and Parking
June 18, 2003	Meeting with Woods Avenue and Woods Place Neighbors at Oakland Baptist Church
June 24, 2003	City Council Work Session on the T. C. Williams Project
September 10, 2003	Meeting with Seminary Hills Civic Association Task Force
November 10, 2003	Meeting with North Ridge Civic Association
November 17, 2003	Meeting with Seminary Hills Civic Association Task Force
December 16, 2003	Meeting with Seminary Hills Civic Association Task Force

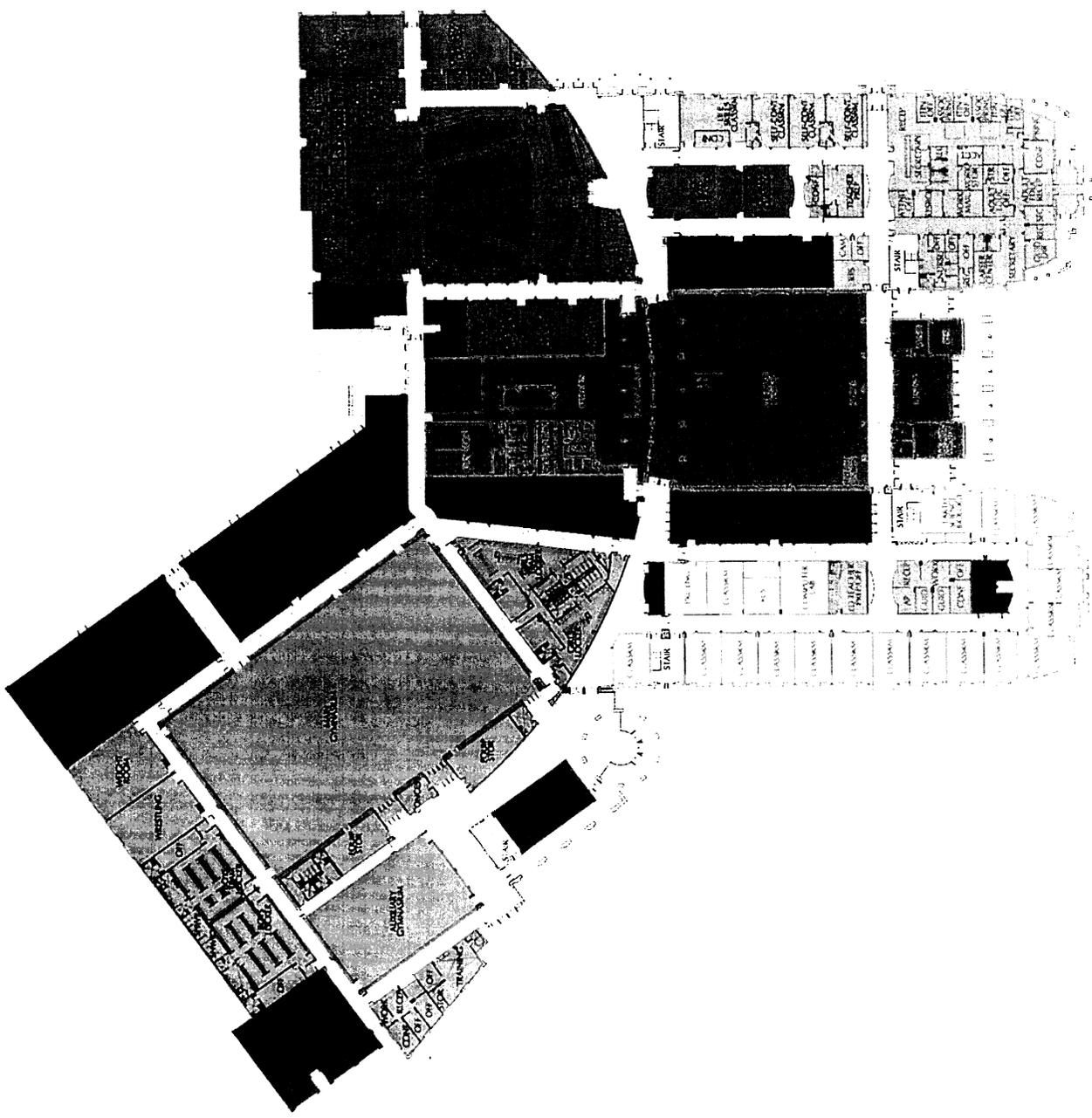






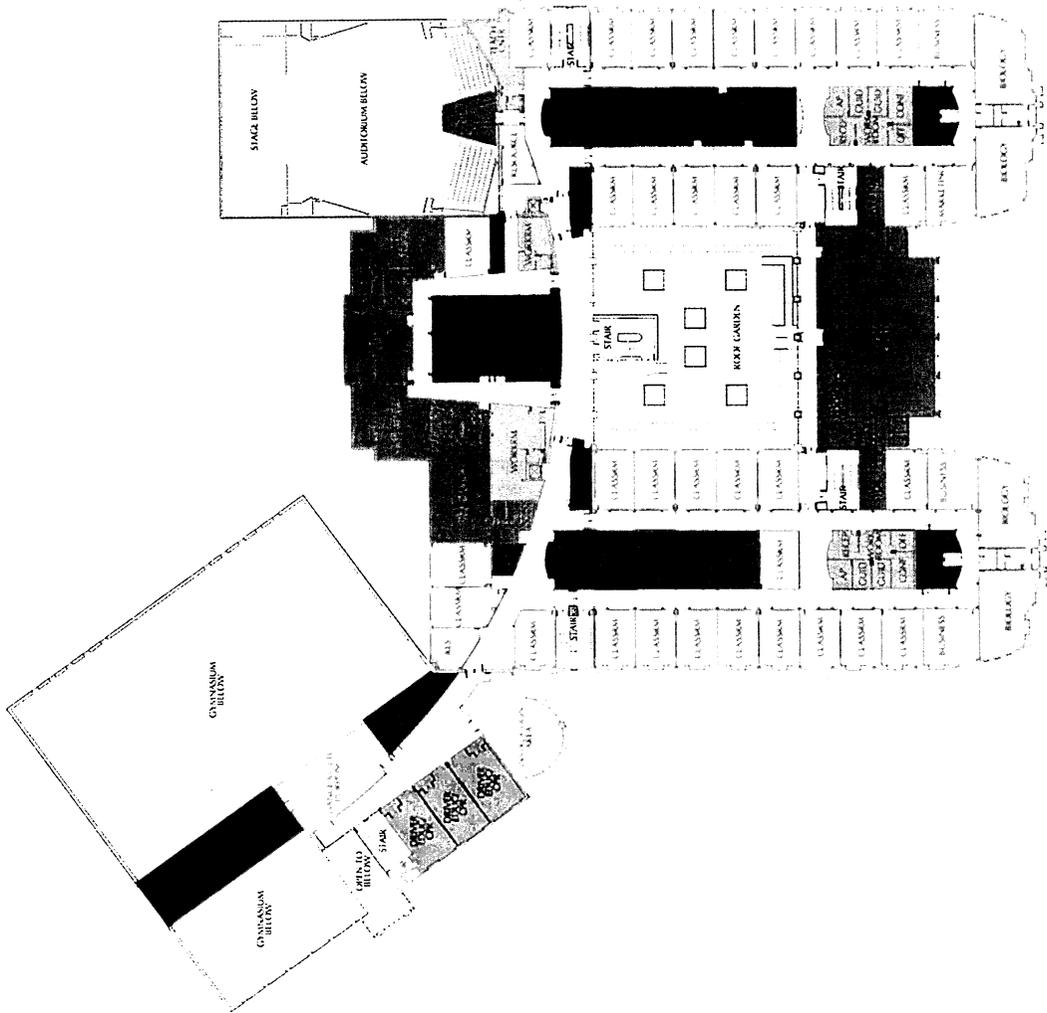


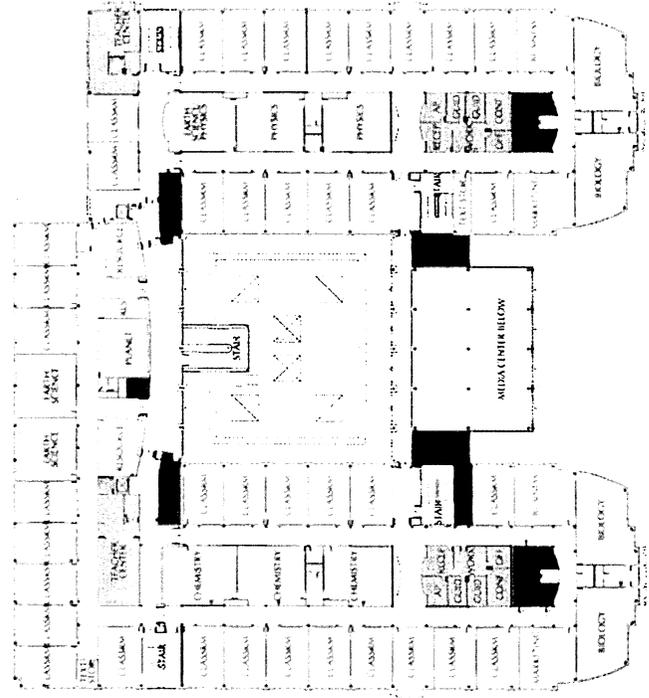




FIRST FLOOR PLAN







 WELLS & ASSOCIATES, LLCTRAFFIC, TRANSPORTATION, and PARKING CONSULTANTS

MEMORANDUM

TO: Russell Smith
ADTEK Engineers, Inc.

FROM: Lawrence Sefcik
Wells & Associates, LLC

DATE: March 7, 2003

RE: Revised Parking Supply
T.C. Williams High School
City of Alexandria, Virginia

This memorandum is a follow-up to our project team meeting yesterday concerning the parking needs for T.C. Williams High School. The meeting discussed the Technical Memoranda dated January 22, 2003, and February 14, 2003, prepared by Wells and Associates included in the Traffic Impact Study dated March 4, 2003.

Information discussed and agreed to at this team meeting (see attached Parking Management Plan from Alexandria City Public Schools (ACPS) affected some of the parking study assumptions used to derive the required number of parking spaces for the school. Below are some of the discussion points:

- A major on-site parking area will be provided on the west side of the site. The school will work jointly with the recreation department to determine if additional parking would be advantageous on the east side of the site.
- The adult education program plans to accommodate an additional 65 students (instead of 150).
- A maximum of 180 student parking permits (spaces) will be issued and monitored closely to prevent additional students from driving (237 spaces was initial projected demand).
- The scheduling of large simultaneous events (i.e. adult education and boy's basketball game) will be controlled.

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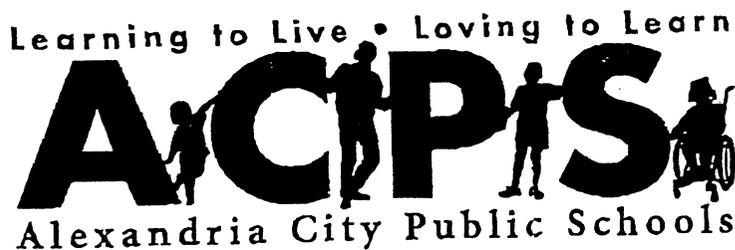
Based on the above information and the Parking Management Plan, a total of 565 minimum parking spaces are required during daytime school use. This includes 385 spaces for staff (including visitors, faculty, custodial, and nursing program). The current site plan (as of March, 2003) shows 428 parking spaces on site. This includes a 380 space parking garage on the west side of the proposed school building, 22 spaces in the visitor lot in front of the school, and 26 spaces in back of the school. Chinquapin Drive can accommodate approximately 225 parking spaces and can be used to provide additional parking area.

During the evening use for the adult education program, the student population may be increased by up to 65 students, or a total of 500 students (as compared to 585 students initially). A future minimum supply of 360 spaces is needed to adequately accommodate the parking demand of the adult education program. An extracurricular activity such as a boy's basketball game would require 320 parking spaces. If the peak periods of both events occurred simultaneously, 680 parking spaces would be required. Although the current site plan shows a total on-site parking capacity of 428 spaces, the additional parking could be accommodated via Chinquapin Drive and the neighborhood streets. The adult education and the extracurricular activity parking demand can each be accommodated by the proposed 428 spaces as long as these events do not occur simultaneously. The Parking Management Plan will limit the frequency of these events occurring simultaneously (currently 15-20 times per year).

In summary, a future minimum parking supply of 565 parking spaces are needed to accommodate parking future anticipated parking demand during the day based on the ACPS Parking Management Plan. This will easily accommodate the 360 spaces needed for adult education program, and 320 spacing for an extracurricular event (i.e. Boy's basketball game) as long as these large events do not occur simultaneously.

Let me know if you have any questions.

Attachment



MEMORANDUM

TO: Lawrence E. Sefcik
Wells & Associates, LLC

FROM: John D. Johnson, III *JDJ*
Assistant Superintendent
Financial and Administrative Services

DATE: March 6, 2003

RE: Parking Management Plan for the New
T. C. Williams High School Site

Beauregard Street
Alexandria, Virginia 22311

Phone: (703) 824-6600

Fax: (703) 824-6699

(703) 824-6666

www.acps.k12.va.us

Superintendent of Schools
L. Perry

Board

Chairman
L. Eaton

Member
J. Johnson

Member
D. Cheatham

Member
M. Danforth

Member
Dolyn H. Lewis

Member
O. Wilkoff

This is to certify our discussions today regarding the Parking Management Plan for T. C. Williams High School. Beginning with the numbers you provided for peak usage on 15-20 nights per year, we fine-tuned our assumptions for daytime and nighttime usage for staff, students and visitors. See the attached fact sheet.

The following decisions have been made relative to managing the use of parking on the site.

1. Limit student parking permits to 180. Current requests do not exceed 155 for daytime use.
2. Re-locate several Adult Education programs to other ACPS schools, thereby decreasing the number of spaces needed in the evening.
3. Stagger the schedule for some Adult Education and high school education programs.
4. Control the scheduling of major events in order to avoid peak crowds, thereby decreasing the number of spaces needed by at least another 100.
5. Continue cooperative arrangements for use of space in neighboring lots for such large events as graduation, College Night, Titan Expo, and other events that require overflow capacity.
6. Explore additional scheduling options to limit the peak number of spaces needed.

7. Continue working with the Department of Recreation, Parks and Cultural Affairs to create opportunities for programs on both sites to share parking facilities as needed.

As we continue discussions with City agencies we will look at additional steps that can be taken to minimize the number of parking spaces needed to support ACPS educational and extracurricular events.

C: Rebecca L. Perry
Howard Middleton
James Copeland
Billy Riggs
Russell Smith
John Porter
Mark Krause
Mathew Pasquale
A.K. Johnson
Margaret Walsh

T. C. Williams Parking Facts**Actual Parking Use Today**

Staff/Nurses/Visitors	255
Student	85
Chinquapin Loop	95
Other Streets	<u>20</u>
Total	455

Student Permit Parking = 155

Current Parking Space Capacity

Staff/Nurses/Visitors	255
Student	85
Chinquapin Loop	<u>225</u>
Total	565 Spaces

Future Parking Needs

	<u>Parking Study</u>	<u>Staff Recommendation</u>	<u>Site Plan Capacity</u>	
Staff/Nurses	385	385	W. Structure	380
Visitors	incl.	incl.	Visitor Lot	22
Student	<u>237</u>	<u>180*</u>	Rear Lot	<u>26</u>
Total	622	565	Subtotal	428
			Chinquapin Loop	<u>137</u>
			Total	565

*Current student permits = 155
+ 16% = 25
Total 180

Nighttime Use

	<u>Current</u>	<u>Future</u>
Adult Education	315	360
Extra Curricular	<u>274</u>	<u>320</u>
Total	589	680

Site Plan Capacity

West Parking Structure	380
Visitor Lot	22
Rear Lot	26
Chinquapin Loop	225
Neighborhood Streets	<u>27</u>
Total	680

March 6, 2003

December 10, 2003

**Response to Inquiries Regarding Parking on the
New T. C. Williams High School Site**

Currently and in the future significant administrative attention is directed to the issues surrounding daily and event parking at T. C. Williams High School. The purpose of this memorandum is to update the data regarding actual parking needs, event management, and ongoing supervision. This update reflects actual design documents and actual parking usage patterns.

1. **Allocation of Spaces:** Once the new construction is finalized, the school site will provide 431 parking spaces. These include 198 on the first level of the parking garage, 218 on the second level of the parking garage, 8 at the visitors' drop off circle and 7 in the rear of the school, to be used primarily by Food and Nutrition staff members as well as teachers in the fields of Auto Mechanics and Building Trades. Other spaces reflected on the site plan will be used in support of the automotive trade programs and will not be available for parking.

Currently there is a need for no more than 210 staff parking spaces. All 198 spaces on the first level will be assigned to staff during regular school hours. Up to 7 staff members will be assigned spaces in the rear of the building. The second floor of the parking garage will accommodate the additional 5 staff members, 2 or more visitors, and up to 40 adult student nurses who leave campus at 11:00 am for their onsite clinical work.

At approximately 12:00 noon up to 5 members of the Adult Education staff will arrive and utilize some of the spaces vacated by the student nurses.

This use pattern leaves at least 179 spaces for student parking. For the 2003-04 school year, 118 student parking passes have been issued. The remaining 61 spaces detailed in the plan accommodate future staff and student growth.

2. **Daytime Overflow Concern:** Based on the detail provided above, we do not anticipate any daytime overflow problem. A significant rationale for the type and capacity of the planned parking garage structure was to eliminate this concern. As noted above, anticipated growth should not exceed parking capacity on site. When and if this projection changes, the Administration of the High School is prepared to scale back student parking as low as is needed to accommodate staff. Students who live outside a 1.5 mile radius of the school are eligible to ride the ACPS bus to school.

The current parking fee is \$75.00 annually. Some of the students who purchase parking permits do leave school during the day in order to work. ACPS transportation is currently available to students hourly every day from noon to 6:00 pm. Students are encouraged to ride the bus to work. We do not anticipate any reduction in this service for students.

Enforcement of neighborhood parking regulations is a function of City agencies, not ACPS. ACPS does support student compliance with parking regulations through frequent announcements by the principal and supervisory activities by security monitors. As noted above, offsite student parking should not occur once construction of the parking garage has been completed.

- 3. Evening, Weekend and Special Event Overflow Concern:** For many years ACPS has discontinued the scheduling of outside activities at T. C. Williams High School on weeknights due to student activities and the Adult Education program.

When major school events require overflow parking, the ACPS Facilities Department and the T. C. Williams High School Facilities supervisor work closely with First Baptist Church, the Department of Recreation, Parks and Cultural Activities, and various neighborhood groups to ensure that additional spaces are available for patrons.

Attachments 1 and 4 detail the use of T. C. Williams High School on a nightly basis for the 2003-04 school year.

Attachment 2 is the form that is used by outside groups to schedule events at any ACPS facility. Planning for event parking needs is a regular part of our facilities management program. The scheduling form requires, among other safeguards, an estimate of the number of participants and expected audience count. Whenever these projections exceed what can be handled safely and appropriately, the request for event scheduling is denied.

With respect to the scheduling of Adult Education courses, we are in the process of determining which programs could be moved to alternative school sites during the construction period and still meet the needs of our adult students, many of whom actually depend on public transportation. Clearly once these programs have been relocated, they could also remain off the King Street site after occupancy. Once these plans have been finalized, we will share them publicly.

- 4. Shared Parking Agreements:** With respect to agreements we have with the owners of other large parking lots, the Scottish Rite Temple has provided the City Manager with a letter assuring cooperation on a pre-planned basis. As noted above, the First Baptist Church continues to work closely with ACPS and the High School to accommodate patrons of events held at T. C... Attachment 5

details the T. C. Williams Parking Management Plan and includes additional steps we will take to continue effective parking management.

5. **Confirmation of Number of Parking Spaces:** See note 1 regarding the capacity and assignment of the spaces in the parking garage.
6. **Additional Levels of Garage Parking:** As requested by the Department of Planning and Zoning in the design stages of this project and by the high school security team, the planned garage structure will not support additional levels of parking. This assurance respects the concerns regarding the overall site plan and contributes to a lower-cost parking solution than would have been required by designing and installing a structure that could support upper decks.
7. **Outside Users of the Parking at TC:** Attachment 3 details the parking agreement between APCS and the Oakland Baptist Church. We anticipate continuing to provide parking for them as indicated in the agreement.

The adjacent apartment residents currently are allowed to use the school parking lots from 11:00 pm to 7:00 am. This week in a meeting with City agencies it was determined that appropriate security mechanisms would be included in the garage such that the neighbors could safely continue overnight parking. Security cameras will be installed to monitor the deck at all times. Motion detectors will trigger a lighting system to indicate after-hours usage and provide a significant deterrent to unauthorized use.

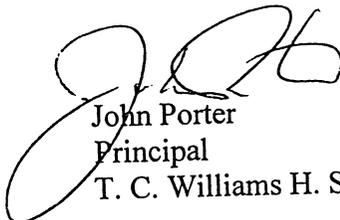
8. **School Hours:** Some school staff begin arriving as early as 7:00 am. Several Central kitchen staff may arrive as early as 5:45 am. Most daytime students do not arrive before 8:00 am. The regular school day ends at 3:15 pm. Additionally, night school and student activities may run as late as 10:00 pm. Building custodial staff are on duty until 11:00 pm.
9. **Bike Racks:** Currently there is bike rack space for 12 bicycles at T. C. We are committed to the goals of the "Bicycle Transportation and Multi-Use Trail Master Plan developed by the City. Once that plan has been implemented and safe bike lanes are available, we will discuss the placement of bicycle racks on the site of the new T. C.
10. **T. C. Williams Parking Management Plan:** As discussed in earlier meetings, ACPS is committed to implementing a Parking Management plan [see Attachment 5] for the King Street site that will be subject to ongoing review.

We appreciate the opportunity to collaborate on this critical part of the site plan process. Please let us know if additional information is required.

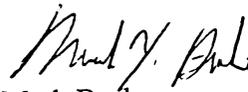
Respectfully,



Margee Walsh
Executive Director
Secondary Programs



John Porter
Principal
T. C. Williams H. S.



Mark Burke
Director
Planning and Construction

C: Rebecca Perry
Jay Johnson
Mark Krause

Phil Sunderland
Barbara Ross
Sandra Whitmore

(1)

Sunday Monday Tuesday Wednesday Thursday Friday Saturday

September 2003

Proj. Disc. meets every Tues. 3:15pm-4:15pm in the Mini Aud
 Key Club meets every Thursday in the Mini Aud from 7:00 pm to 8:00 pm

1 Labor Day
 2 School Begins
 STEP: 1st Day Orient.
 8:15am-11am Mini Aud
 3
 4
 5 FOOTBALL
 Fairfax Away
 7:30 pm
 6

7
 8 Academic Letter Awards 7:00 pm
 9
 10 Back to School Night 7:00 pm in Aud, Mini, Café, Gym, Music Wing
 11
 12 FOOTBALL
 Yorktown Away
 7:30 pm
 13

14
 15 Staff/ Underclass Pictures
 16 Staff/ Underclass Pictures
 17
 18
 19
 20 FOOTBALL: West Po. Home 1:00 pm

21
 22 Senior Makeup Pictures
 23 Senior Makeup Pictures
 24
 25 FOOTBALL: Annandale Away 7:30 pm
 26
 27

28 29 30

Sunday Monday Tuesday Wednesday Thursday Friday Saturday

October 2003

Proj. Disc. meets every Tues. 3:15pm -4:15pm in the Mini Aud

Key Club meets every Thursday in the Mini Aud from 7:00pm to 8:00 pm

4 FOOTBALL: Chantilly Home 1:30 pm
Homecoming Dance 8-12 pm in Gym

11

10 FOOTBALL Lake Braddock Away 7:30 pm

18 FOOTBALL: Robinson Home 1:00 pm

25 FOOTBALL: Centreville Home 1:00 pm

1 SCA Senate meeting 1 1/2^{hr} pd. in cafe

3

2

9 Staff/ Underclass Makeup Pictures

17

16 College Night 7:00 pm

24

23 STEP: Parent Dinner 6pm-9 pm in cafe

8

15

22

7

14

21

6

13 Staff Development

20

12

Student Holiday

29

Blood Drive 8am-3pm in Gym

27

31 FOOTBALL: West Springfield Away 7:30 pm

30

26

Sunday Monday Tuesday Wednesday Thursday Friday Saturday

November 2003

2 3 4 5 6 7 8

1st Quarter Ends
SCA Senate meeting
1 1/2nd pd in café

Early Release
Elementary Schools

FOOTBALL:
Hayfield Home 1:00 pm

Key Club Thurs in
Mini Aud 7pm-8pm

Proj. Disc. Tues.
3:15pm-4:15pm in
Mini Aud

Friday

1

Saturday

9 10 11 12 13 14 15

Teacher Work
Day

Early Release
Middle Schools

Early Release
Middle Schools

Wednesday

Thursday

Friday

Saturday

123

16 Tetra for
and Graham/Howe

17 Parent
Conferences / Early
Release T.C. Williams

18 STEP: Career
Fair 8:15am -
3:15pm in Mini Aud.

20

21

22

19 Parent
Conferences / Early
Release T.C. Williams

20 STEP: Career
Fair 8:15am -
3:15pm in Mini Aud.

21

22

23

24 Winter Sports
Parent Orient.
6:45-9pm C10, C11,
E10, E11

25 Staff
Development
Student Holiday

26

27

28

Thanksgiving Holiday

Thanksgiving Holiday

Thanksgiving Holiday

30

Stage Construction - Drama Dept

Sunday Monday Tuesday Wednesday Thursday Friday Saturday

December 2003

Proj. Disc. every Tues
3:15pm-4:15pm in Mini
Aud

Key Club meets every
Thursday in the Mini
Aud from 7:00pm to
8:00 pm

Fall Play Performance
6 8pm in Aud.

Fall Play Performance
4 8pm Aud
STEP: Honors Ass. 8:15am-
11am Mini
Financial Aid Wksp. 6:30pm
-8:30pm Mini Aud

Fall Play Dress Reh.
3 SCA Senate Meeting
1 1/2nd pd. cafe.

Fall Play Dress
1 Rehearsal

Fall Play Dress
2 Rehearsal

7 Fall Play
Performance 2pm
Matinee in Aud.

12 13

11

10

9

8

124

Assembly

*Church
Sec*

14

15

16 Choir Reh. 6pm-
9:30 pm in Aud
and Music Wing.

17 Winter Concert
Choir 6pm-9pm
Aud, Music Wing and
green room

18 STEP: Family
Fun Night 6pm - 9
pm in Cafe and Gym

19 20

21

*Church
Sec.*

22 Winter Holiday or
Snow Makeup
Day #1

23

24 Winter Holiday

25 Winter Holiday

26 Winter Holiday 27

28

29 Winter Holiday

30 Winter Holiday

31 Winter Holiday

Sunday

Monday

Tuesday

Wednesday

Thursday

Friday

Saturday

Key Club every
Thursday in the
Mini Aud from
7:00pm- 8:00pm

Proj. Disc. every
Tues 3:15pm-
4:15pm in Mini
Aud

January 2004

3

2

1

10

9

8

7 SCA Senate 1st/2nd
pd. Cafe.

6

5 School Resumes

4

*Get
to
Calyan*

17

16

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22

21

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19 Martin
Luther King Jr.
Holiday

18

31

30 Student Holiday or
Snow Makeup
Day #2

29

28

27^{2nd} Quarter Ends

26

25

125

Sunday

Monday

Tuesday

Wednesday

Thursday

Friday

Saturday

February 2004

1

2

3

4 SCA Senate Meeting
1st / 2nd pd in Caf .

5 Early Release
Elementary Schools

7

Key Club meets every Thursday in the Mini Aud from 7:00pm to 8:00 pm

6 Early Release
Elementary Schools

8

9 Early Release
Middle Schools

10

Early Release
Middle Schools

11 STEP: Honors
Ass. 8:15am -
11am in Mini Aud.

12

Early Release

14

13 Parent
Conferences / Early
Release T.C. Williams

T. C. Williams
Parent/Conferences

15

16 President's Day
Holiday

17

18

19

20

21 Step Show 3pm -
8pm in Aud.

22

23

24

25

26 STEP: Black
History Program
8:15am - 11am in Mini
Aud.

27

28

29

March 2004

Sunday Monday Tuesday Wednesday Thursday Friday Saturday

Proj. Disc. every Tues 3:15pm-4:15pm in Mini Aud

Key Club meets every Thursday in the Mini Aud from 7:00pm to 8:00 pm

5 *Hessling*
Set up

6 *Hessling*

4 Spring Sports Parent Orient. 6:45pm -9pm in Music Wing, C10, C11, E10, E11

3 SCA Senate meeting 1st / 2nd pd. in Cafe.

20 *Appl Gym*
 3-6-30
 ← →

19 Winter Sports Recog. Night 6:15 pm - 10pm. Music Wing, C11, C12, E11

27 SAT
 Senior Citizens Prom 6pm - 10:30 pm in Cafe

25 STEP: Health Fair 8:15am-3:15pm in Mini Aud

17 Choir Concert 6pm - 9:30pm Aud and Music Wing

16 Choir Reh. in Aud and Music Wing

24 Blood Drive 8am-3:15pm in Gym

31 SCA Senate meeting 1st / 2nd pd in Cafe

30

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127

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27

Sunday Monday Tuesday Wednesday Thursday Friday Saturday

April 2004

*Set
Construction*

Key Club meets every Thursday in the Mini Aud from 7:00 pm to 8:00 pm

Proj. Disc. every Tues 3:15pm -4:15pm in Mini Aud

1
2 *Moving set up*
3 *Moving Fair*

4
5 Spring Break or Snow Make-up Day #4
6 Spring Break or Snow Make-up Day #5
7 Spring Break
8
9
10

11
12 School Resumes
13
14
15
16
17

18
19
20
21 SCA Senate meeting 1st / 2nd pd. in cafe
22 STEP: Honors Ass. 8:15am-11am in Mini Aud
23
24 *Cheerleading set up*

25 Titan Expo. 7am-8pm. Out-gym, parking lot, Chinquapin, stadium.
26
27
28
29
30

Rain- Aud, Mini. Café, Gym, Sports Barn

May 2004

Sunday

Monday

Tuesday

Wednesday

Thursday

Friday

1

Key Club Thurs in Mini
Aud from 7pm-8 pm

Proj. Disc. Tues in Mini
Aud 3:15pm-4:15 pm

8 Spring Play
Performance 8pm Aud

7 Spring Play
Performance 8pm
Aud

6 Spring Play
Performance 8pm
Aud

5 Spring Play Dress
Reh.

SCA Senate meeting. 1st
/ 2nd pd in Cafe

4 Spring Play Dress
Reh.

3 Spring Play Dress
Reh.

2

15

14

13

12

11

10

9 Spring Play
Performance 8pm
Aud

129

16 Black Scholars'
Awards Program
1pm-5pm Aud

18

19

20

Spring Choral Concert
6pm-9:30pm Aud and
Music Wing

TC Choir Reh. 6pm-
9:30pm in Aud, Music
Wing, green room

22

21

29

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27

26

25

24

23

31 Memorial Day Holiday

30

ALEXANDRIA CITY PUBLIC SCHOOLS
 2000 North Beauregard Street
 Alexandria, Virginia 22311

PERMIT FOR USE OF SCHOOL FACILITIES

Instructions: Application must be filed with the office of Educational Facilities not less than ten (10) working days before intended use.
Please Type or Print

SCHOOL REQUESTED: _____ REQUESTED DATE(S): _____
 ORGANIZATION: _____
 STREET: _____
 CITY: _____ STATE: _____ ZIP: _____
 REPRESENTATIVE: _____ HOME PHONE: _____
 TITLE OR POSITION: _____ WORK PHONE: _____

ESTIMATED ATTENDANCE	TIME	PURPOSE
Participants _____ Audience _____	BUILDING TO BE OPEN _____ A.M. _____ P.M. BUILDING TO BE CLOSED _____ A.M. _____ P.M.	
REQUIREMENTS		
SPACE	SERVICE PERSONNEL (Number)	SPECIAL EQUIPMENT/SERVICE (Please Be Specific)
Auditorium <input type="checkbox"/> Cafeteria <input type="checkbox"/> Kitchen <input type="checkbox"/> Classroom <input type="checkbox"/> Gym <input type="checkbox"/> Other: _____	Bldg. Eng. <input type="checkbox"/> Custodian <input type="checkbox"/> Food Services: <input type="checkbox"/> Manager <input type="checkbox"/> Other <input type="checkbox"/> P.A. OP. <input type="checkbox"/>	Police: Yes <input type="checkbox"/> No <input type="checkbox"/> Number _____ Equipment: _____ Other: _____
Will you attend? _____ If no, who will be in charge? _____ Is this organization Profit <input type="checkbox"/> Non-Profit <input type="checkbox"/> Will fees be collected? Yes <input type="checkbox"/> No <input type="checkbox"/> Organization has liability coverage? Yes <input type="checkbox"/> No <input type="checkbox"/> Individual \$ _____ Total \$ _____		

The undersigned certifies that he/she is familiar with the Alexandria School Board regulations and responsibilities of renters as stated on the reverse side of this agreement. And that he/she is the legally authorized representative to act for and accept such responsibility for the organization. Final approval of this permit is contingent upon receipt of this permit by the applicant duly appointed and signed by the Building Principal and Community Services Manager.

Signature		Date						
OFFICE USE ONLY								
PERSONNEL COSTS				SPACE/EQUIPMENT COSTS				
1. Housekeeping/Maintenance	NUMBER	HOURS	RATE	AMOUNT	NUMBER	HOURS	RATE	AMOUNT
Bldg. Eng.	_____ x	_____ x	_____ \$	_____ \$	Auditorium	_____ x	_____ x	_____ \$
Custodian	_____ x	_____ x	_____ \$	_____ \$	Cafeteria	_____ x	_____ x	_____ \$
P.A. OP.	_____ x	_____ x	_____ \$	_____ \$	Caf./Kitchen	_____ x	_____ x	_____ \$
			TOTAL	_____ \$	Classrooms	_____ x	_____ x	_____ \$
					Gymnasium	_____ x	_____ x	_____ \$
							TOTAL	_____ \$
The minimum fee for the use of the facility and services in the amount of \$ _____ shall be paid on or before the scheduled event. Checks are to be made payable to the Alexandria School Board and sent to the Community Services Manager, 2000 North Beauregard Street, Alexandria, Virginia 22311.								
<input type="checkbox"/> Approved Date _____			<input type="checkbox"/> Approved Date _____					
<input type="checkbox"/> Disapproved _____ (Signature of Principal)			<input type="checkbox"/> Disapproved _____ (Signature of Community Services Manager)					

**POLICY AND REGULATIONS GOVERNING THE USE OF SCHOOL FACILITIES
AT TIMES WHEN NOT REQUIRED FOR SCHOOL PURPOSES**

1. **GENERAL.** The public school buildings of Alexandria City are available to community groups at any time when the facilities to be used are not required for school purposes. School functions will not be postponed or canceled to make facilities available to community groups. The School Board reserves the right, on all occasions, through the accredited representatives, to ascertain the use to which the facilities are to be put and to determine whether such use is proper and acceptable under ordinary standards applying to such facilities and the acceptance and general feeling of the community. The School Board, through the executive offices, also reserves the right to see that the requirements that follow shall be properly adhered to.

2. **PROCEDURES FOR RENTAL PERMITS.** The applying organization will contact the school whose facility is being requested to clear and reserve proposed dates, times and complete the permit form (schools are expected to assist in completing rental form). Forms will be sent to the Office for Educational Facilities of the School Administrative Offices for final approval and determining the minimum fees. Applicants will then be notified of the disposition of the application and informed of the minimum fee which is due and payable on or before the day the event is to be held.

3. **RENTAL BY SERIES OF DATES.** Groups may rent facilities for a series of dates over extended periods through one permit, provided that all dates and all facilities to be used are included on the permit. Should any changes be made which alter the fees payable, a new permit must be issued to support the increase or decrease in fees.

4. **DURATION OF EACH RENTAL.** Evening rental fees shall cover a period not to exceed four hours and shall not extend beyond 11:00 P.M. Buildings shall not be opened before 8:00 A.M. nor remain open later than 1:00 P.M. for Sunday morning religious activities.

5. **USE OF EQUIPMENT AND FACILITIES.** (a) Stage equipment provided: Speaker's stand or table, not more than 25 chairs, footlights, overhead lights, backdrop, front curtain and piano if available. The custodian or school stage crews are not obligated to assist in changing scenery. Alterations to electrical service panels or electrical equipment is forbidden unless request for such alterations is first approved by the School Board Office of Educational Facilities. Any costs incurred for such alterations and to restore to the same condition shall be paid by the organization requesting the change. Such alterations must be made in accordance with the electrical Code for the City of Alexandria. (b) It is assumed that all rentals include the use of adjacent or men's and women's toilets. (c) Pianos shall not be moved unless permission is granted by the principal. Heavy uprights and grand pianos may be moved only by professional piano movers whose services shall be arranged for and paid by the renter. This includes return of the piano to its original location. (d) Classroom rental includes only the use of furniture, chalkboards, wardrobe space for coats, and toilets, if any. (e) Building equipment, such as storage cabinets, and supplies shall not be used. School equipment (other than that discussed) is not available for rental or loan. (f) It is expected that equipment and facilities will be left in their same condition as found prior to use. Expense of restoring same shall be at renter expense.

6. **CUSTODIAL SERVICE.** Custodial service shall include only unlocking and locking doors, operating ceiling lights, providing heat, setting up chairs, and normal clean-up. Groups receiving free rentals are obligated to aid the custodian in setting up and removing chairs, and assisting with the general clean-up.

7. **DECORATIONS.** Decorations such as flower baskets, potted plants, and flag standards are permitted. No other decorations are permitted.

8. **POSTERS OR ADVERTISING.** Posters or advertising or otherwise, either within the building or outside, is not permitted.

9. **SMOKING, DRINKING, DISORDERLY CONDUCT.** Smoking, or use of tobacco in any form, in any school building shall not be permitted. Drinking, disorderly conduct, and willful destruction of property are forbidden by City Ordinance. Violators will be prosecuted in accordance with the law. Refreshments of any kind may not be brought in, served or consumed in the building.

10. **GAMES OF CHANCE. DOOR PRIZES.** Games of chance or the giving away of door prizes when an admission fee is charged will not be permitted.

11. **ACCIDENTS AND LIABILITIES.** The Alexandria City School Board, the City of Alexandria or any of its employees cannot be held liable in case of accident and shall be held harmless by the renting groups, such requirement to be part of the rental agreement.

12. **POLICE SUPERVISION.** When any event held on School Board property is open to the general public, the sponsoring person or organization must obtain police supervision and is responsible for the payment for said supervision.

13. **USE OF SCHOOL BUILDING WITHOUT CHARGE.** The Alexandria City Council of Parent-Teacher Associations and all local parent-teacher associations which have been duly established in accordance with the provisions of the National Congress of Parent-Teacher Associations as specified in its published manual, and which local PTA units are allied with one or more public schools in Alexandria City and the Alexandria City Council of PTA's shall make rent-free use of public school facilities in Alexandria City (including buildings, grounds, duplicating equipment, audio-visual aids, furniture and fixtures, utilities and other miscellaneous items) for their monthly business meetings and other activities as their respective executive committee and/or memberships may authorize, provided, of course, that such activities or business meetings shall not conflict or interfere with activities sponsored by the public schools themselves. EXCEPT THAT NORMAL FEES MAY APPLY ON SATURDAYS AND SUNDAYS.

City government organizations may use school facilities to implement their established programs and/or for government purposes without charge unless custodial services are needed outside regular hours, in which case the charge for such services shall be based on the cost of custodial services and out-of-pocket expenditures.

Group A—Include all organizations whose mailing address or function is not within the city limits of Alexandria as well as all local profit organizations whether or not a charitable purpose is claimed. Political organizations involving candidates for office other than local offices or State Legislators are included in this classification.

Group B—Includes all non-profit organizations and civic organizations whose mailing address or function is within the city limits of Alexandria. Religious organizations serving or which consist predominantly of the city of Alexandria residents are classified in this group. Organizations whose functions to present candidates for local or state elected office are within this classification.

Group C—This group includes Parent-Teacher Groups, Local Education Groups, Boy Scouts, Girl Scouts, Camp Fire Girls, City Recreation Department and other school related organizations. Generally, facility fees will be waived if proper arrangements for such services can be made through the Office for Educational Facilities and the school principal.

ALEXANDRIA

CITY PUBLIC SCHOOLS

2000 N. Beauregard Street
Alexandria, Virginia 22311

Telephone: (703) 824-6600
Facsimile: (703) 824-6611
TDD: (703) 824-6666

March 3, 1997

Superintendent of Schools
Herbert M. Berg, Ed.D.

School Board Chairman
Claire M. Eberwein

Vice Chairman
Henry Sidney Brooks

School Board Members
Raymond S. Cole
Mary M. Danforth
V. Rodger Digilio
Patricia A. M. Hennig
Bernadette Johnson
Stephen J. Kenealy
Charles W. Linderman

Clerk of the School Board
Billie W. Hughes

Reverend Tyrone Queen
Oakland Baptist Church
3408 King Street
Alexandria, VA 22302

RE: Parking at T.C. Williams High School

Dear Reverend Queen:

This letter serves as an update to previous correspondence dated December 15, 1995 concerning permission to park in the T. C. Williams High School parking lot.

You have permission to use T. C. Williams parking areas during non-school hours, sufficient to your need, for regular church services or for special events/occasions. I must emphasize that permission is for use during non-school hours:

MONDAY - FRIDAY
SEPTEMBER THROUGH JUNE
7:00 A.M. - 4:00 P.M.

If you have additional questions, please feel free to contact me at 824-6686.

Sincerely,



Larry H. Gilbertson
Director of Educational Facilities

LHG/jb
cc: Mr. Price

ALEXANDRIA

CITY PUBLIC SCHOOLS

2000 N. Beauregard Street
Alexandria, Virginia 22311

Telephone: (703) 824-6000
Facsimile: (703) 820-8491
TDD: (703) 824-0000

December 15, 1995

Superintendent of Schools
Herbert M. Berg, Ed.D.

School Board Chairman
Stephen J. Kenealy

Vice Chairman
Charles W. Linderman

School Board Members
Henry S. Brooks
Vernon J. Collins, Sr.
Mary M. Danforth
V. Rodger Digilio
Claire M. Eberwein
Patricia A. M. Hennig
Bernadette Johnson-Green

Clerk of the School Board
Billie W. Hughes

Reverend Tyrone Queen
Oakland Baptist Church
3408 King Street
Alexandria, VA 22302

RE: Parking at T. C. Williams High School

Dear Reverend Queen:

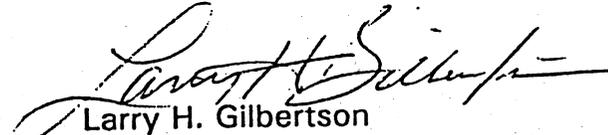
This letter serves as authorization to continue to use the T. C. Williams parking lot for congregational parking.

You are reminded that this authorization is for use during non-school hours. Use during school hours is not possible with the number of school staff and student parking requirements.

You are also reminded to ask your congregation to assist in keeping the lot clean and free of litter.

If you have additional questions, feel free to contact me at 824-6688.

Sincerely,



Larry H. Gilbertson
Director of Educational Facilities

LHG/jb

ATTACHMENT 4



T.C. Williams High School

3330 King Street, Alexandria, Virginia 22302
(703) 824-6800 Fax: (703) 824-6826

Fax Transmittal

To: *Jay Johnson & Margee Walsh*
Fax #: *703-824-6699*
Date: *3/4/03*
Re: *TC Calendars*
From: *Susan Stambaugh*
Pages (to include cover sheet) *10*
Comments:

**T.C. Williams Activities
After School and Evening**

Club/Group Mtgs. Athletic Practices	Sep-Jun	M-F	3:30-5:30	Entire Building
NIGHT SCHOOL	8/26-1/21 1/27-6/11	M-Th M-Th	6:30-9:30 pm 6:30-9:30 pm	Classrooms Classrooms

ATHLETIC EVENTS (highlighted on attached)

REC. DEPT.	Sep-June		6:00-10:00 pm	Gym Sports Barn
	Winter Camp Spring Camp Summer Camp (rain)			

**MAJOR HIGHLIGHTS
(*night school cancelled)**

SAT	10/12 11/2, 12/7, 1/25, 4/5, 5/3, 6/7	Sa	7:30AM - 2:00 PM	25+ classroom
New Student Orientation	8/21	W	7:00-9:00 PM	Main Aud
Academic Letter Awards*	9/9	M	6:30-9:00 PM	Main Aud
Back-to-School Night	9/12	Th	6:30-10:00 PM	Entire School
Homecoming Dance	10/5	Sa	8:00-12:00 mid	Gym
Parent Conferences	10/10	Th	1:30-8:00 PM	Entire School
College Night*	10/17	Th	6:00-10:30 PM	Entire School
Financial Aid Workshop	12/3	Tu	6:30-9:00 PM	Mini-Aud
Parent Orientation	1/22		7:00-9:00 PM	Cafeteria
Parent Conferences*	2/13	Th	1:30-8:00 PM	Entire School
SFA Telethon	2/23	Su	Afternoon-Evening	Entire School
	2/22	Sa	All day - setup	
Student Recognition Night*	5/28	W	6:30-10:00 PM	Main Aud
SFA Awards*	6/4	W	6:30-9:00	Main Aud
Graduation	6/17		All day	Entire School
Summer School	Jul-Aug		All day	Entire School

OTHER AFTERNOON/EVENING EVENTS

Rules Clinic	8/26	M	6:00-7:00 PM	Mini-Aud
Fall Sports Orientation	8/27	T	7:00-9:00 PM	Main Aud Classrooms Classroom
Volleyball Parents Meeting	8/28	W	6:30-7:30 PM	Classroom
Port City Players	9/5-9/14 9/20-9/21 9/24 9/27-9/28	Th-Sa-Sa F-Sa T F-Sa	6:30-11:00 PM	Mini-Aud
PTA Council	9/11	W	7:00-9:30 PM	Cafeteria
Step BTS Night	9/12	Th	6:00-8:30 PM	Mini-Aud
City Council/School Board	9/17	T	6:00-10:00 PM	Cafeteria
PTSA/Architects	9/18	W	7:00-10:00 PM	Cafeteria
Key Club	9/19	Th	7:00-9:00 PM	Mini-Aud
Campana Center Picnic	10/4	F	3:30-8:00 PM	Sports Barn
NHS Induction	10/15	T	7:30-9:00 PM	Mini-Aud
EAA Board of Directors	10/15	T	4:30-6:00 PM	Main Aud
SPED Lock-In	10/18	F	7:00 PM - 8:00 AM	Cafeteria
Cheerleaders	10/19	Sa	9:00-12:00 AM	Gym
Band Concert	10/30	W	7:00-9:00 PM	Main Aud
JROTC Drill Meet	11/9	Sa	6:30 AM - 6:30 PM	Main Gym Cafeteria Classrooms
Fall Sports Recognition	11/21	Th	6:30-10:00 PM	Main Aud Cafeteria Mini-Aud Band, Chorus, Classrooms
Winter Sports Orientation	11/25	M	6:30-9:00 PM	Main Aud Gym Classrooms
Winter Play (rehearsal & set construction)	11/1-12/4		after school & weekends	Main Aud
Winter Play Performance	12/5-12/8	Th-Su	5:00-11:00 PM	Main Aud
Winter Band Concert	12/10	Tu	7:30-9:30 PM	Main Aud
Winter Choral Concert	12/12	Th	7:30-9:30 PM	Main Aud
Basket Bingo (Fin. Acad.)	12/13	F	6:00-9:30 PM	Cafeteria
STEP Family Night	12/18	W	6:00-9:00 PM	Cafeteria
Winter Orchestra Concert	12/19	Th	6:30-9:30 PM	Main Aud
Dance Auditions (Spr. Play)	1/6, 1/7, 1/9		3:30-5:30 PM	Main Aud
One Act Play Rehearsal	1/21-1/24		3:30-8:30 PM	Main Aud
Night of One Acts	1/25	Sa	5:30-10:00 PM	Main Aud
All-City Orchestra	1/30	Th	6:30-9:30 PM	Main Aud
Forensic Meet	2/1	Sa	All Day	Entire Bldg.
National Honor Society	2/3	M	6:00-8:00 PM	Mini-Aud

District Chorus Festival	2/6-2/8	Th-Sa	All Day	Main Aud Cafeteria Mini-Aud Music Wing Classrooms
Parent Conference	2/13	Th	1:30-8:00 PM	Entire Bldg.
Rowing Referees	2/15	Sa	7:00 AM - 4:00 PM	Mini-Aud
Titan Auditions	2/19	W	6:00-9:00 PM	Main Aud
SFA Telethon/Setup	2/21-2/23	F-Su	3:30-?	Entire Bldg.
Fant-Asia Rehearsal	2/24, 3/6	Mo, Th	3:30-5:30 PM	Main Aud
Pyramid Concert	3/4	Tu	3:30-8:30 PM	Main Aud
Fant-Asia Performance	3/7	F	3:30-8:30 PM	Main Aud
Orchestra Rehearsal	3/10	M	6:30-9:30 PM	Main Aud
Spring Sports Orientation	3/11	Tu	7:00-9:00 PM	Music Wing Classrooms
Orchestra Concert	3/12	W	7:00-9:00 PM	Main Aud
Families First	3/14-3/15	F, Sa	6:00-10:00 PM	Main Aud
Choir Rehearsal	3/18	Tu	6:30-9:30 PM	Main Aud Music Wing
Choir Concert	3/19	W	6:30-9:30 PM	Main Aud Music Wing
Gong Show	3/20	Th	5:00-10:00 PM	Main Aud
Winter Sports Recognition	3/21	F	6:30-10:00 PM	Entire Bldg.
Play Rehearsal/Set Const.	3/24-5/2		3:30-5:30 PM	Main Aud
Sr. Citizens Prom	3/28	F	3:30-9:00 PM	Cafeteria
Housing Fair/Setup	3/28-3/29	F-Sa	9:00 PM (Fr) 7:00 AM-6:00 PM (Sa)	Cafeteria
Cheerleading Comp. Prac.	4/22	Tu	3:30-5:30 PM	Gym
Cheerleading Competition	4/26	Sa	8:00-3:00	Gym
Titan Expo	4/27	Su	All Day	Entire Bldg.
Play Dress Rehearsal	5/5-5/7	M-W	5:30-10:30 PM	Main Aud
Spring Play	5/8-5/11	Th-Su	5:30-10:30 PM	Main Aud
LPN Graduation	5/13	Tu	5:00-10:00 PM	Main Aud
Band Concert	5/14	W	3:30-9:30 PM	Main Aud
Diversity Night/Rehearsal	5/15-5/16	Th-F	5:30-9:30 PM	Main Aud
Choral Concert/Rehearsal	5/20-5/21	Tu-W	6:30-9:00 PM	Main Aud
Jazz Band Concert	5/22	Th	6:30-9:00 PM	Main Aud
Nat. Forensic/Debate	5/24	Sa	7:00 AM - 10:00 PM	Entire Bldg.
Orchestra Rehearsal	5/27	Tu	6:30-9:00 PM	Main Aud
Orchestra Concert	5/29	Th	6:30-9:30 PM	Main Aud
STEP Recognition Night	6/4	W	6:00-9:00 PM	Mini-Aud
Spring Sports Awards	6/6	F	6:30-10:00 PM	Main Aud

2002 FALL ATHLETIC SCHEDULE - Sports Hotline TC1-5304

T.C. Williams High School - 3330 King Street - Alexandria, Virginia 22302

8/7/02

CHEERLEADING

- 3 Patriot District Semis @ Lake Braddock - 7 pm
- 3 Patriot District Finals @ Hayfield - 7 pm
- Regional Semis @ Robinson - 7 pm
- Regional Finals @ Robinson - 7 pm
- Notes - Leola Battle, Leslie Jones

CROSS COUNTRY

TE	OPPONENT	LOCATION	TIME
	Monroe Parker Invits	Burke Lake	4:15 pm
	Ann-Hay-WPot	Wakefield Pk	4:30 pm
	Great Meadows	The Plains, VA	9:00 am
	Penn State Invitational	Penn State Univ	TBA
	Spiked Shoes	Baltimore	9:00 am
	Ann-LB-WS	South Run	4:30 pm
	Brentsville Relays	TBA	9:00 am
	William & Mary Invitational	Williamsburg, VA	9:00 am
0	Alex. City Championship	TBA	3:30 pm
2	Glory Days Invitational	Bull Run Park	11:00 am
6,17	Patriot District Festival	Burke Lake	4:30 pm
14	Patriot District Champ	Burke Lake	4:15 pm
10	Northern Region Champ	Burke Lake	3:30 pm
1	VA-AAA State	G. Meadows	TBA

Head Coach - Mike Hughes

FIELD HOCKEY

Some games are played at Minnie Howard 9th Grade Center

TE	OPPONENT	LOCATION	TIME	JV
3	Herndon (SC)	Away	3:00 pm	4:15 pm
5	Jefferson (SC)	Home	3:00 pm	4:15 pm
9	Robinson	Home	7:30 pm	6:15 pm
	West Potomac	Away	7:30 pm	6:15 pm
	Annandale	Away	7:30 pm	6:15 pm
3	Episcopal	Away	4:30 pm	5:45 pm
0	Lake Braddock	Home	7:30 pm	6:15 pm
8	Hayfield	Away	7:30 pm	6:15 pm
4	Robinson	Away	7:30 pm	6:15 pm
6	West Potomac	Home	7:30 pm	6:15 pm
0	Annandale	Home	7:30 pm	6:15 pm
2	Lake Braddock	Away	7:30 pm	6:15 pm
4	West Springfield	Home	4:30 pm	5:45 pm
7	St Step/St Agnes	Away	4:30 pm	5:45 pm
10	Hayfield	Home	7:30 pm	6:15 pm
16	West Springfield	Away	7:30 pm	6:15 pm

23-31 Northern Region Tournament @ Oakton HS TBA

Head Coach - Bob Evenson

VARSITY FOOTBALL

TE	OPPONENT	LOCATION	TIME
12	Hugonot (SC)	@ Univ. of Richmond	5:00 pm
17	Gonzaga (SC)	Home	4:00 pm
1	Chantilly	Away	7:30 pm
4	Mt. Vernon	Home	1:00 pm
10	Lake Braddock	Away	7:30 pm
17	Westfield	Away	7:30 pm
15	West Springfield	Homecoming	1:30 pm
12	Hayfield	Home	2:00 pm
18	Robinson	Away	7:30 pm
26	Centreville	Home	1:00 pm
11	Annandale	Away	7:30 pm
19	West Potomac	Home	1:30 pm
15	Regional Semis	TBA	TBA
23	Regional Final	TBA	TBA
30	State Semis	TBA	TBA
17	State Final	TBA	TBA

Head Coach - Riki Ellison

JUNIOR VARSITY FOOTBALL

DATE	OPPONENT	LOCATION	TIME
→ 9/4	Chantilly (SC)	Home	5:00 pm
9/12	Mt. Vernon (SC)	Away	7:30 pm
9/19	West Potomac	Away	7:30 pm
→ 9/26	Westfield	Home	5:00 pm
10/3	West Springfield	Away	7:30 pm
10/10	Hayfield	Away	7:30 pm
→ 10/17	Robinson	Home	5:00 pm
10/24	Centreville	Away	7:30 pm
→ 10/31	Annandale	Home	5:00 pm
11/7	Bishop Ireton	Away	4:30 pm

Head Coach - John Morehead

FRESHMAN FOOTBALL

DATE	OPPONENT	LOCATION	TIME
→ 9/5	Chantilly (SC)	Home	5:00 pm
9/12	Mt. Vernon (SC)	Away	6:00 pm
9/19	West Potomac	Away	6:00 pm
→ 9/25	Westfield	Home	5:00 pm
10/3	West Springfield	Away	6:00 pm
10/10	Hayfield	Away	6:00 pm
→ 10/16	Robinson	Home	5:00 pm
10/24	Centreville	Away	6:00 pm
→ 10/30	Annandale	Home	5:00 pm
→ 11/6	Bishop Ireton	Home	4:30 pm

Head Coach - Thurston McClain

GOLF

Home matches are played at Greendale Golf Course

DATE	OPPONENT	LOCATION	TIME
8/13,14	Quantico Invitational	@ Quantico Marines Base	8:30 am
8/19,20	Herndon Pre-season	Centennial	TBA
8/26	West Springfield	Greendale	4:00 pm
8/29	Robinson	Twin Lakes	4:00 pm
9/5	West Potomac	Greendale	4:00 pm
9/9	Annandale	Greendale	4:00 pm
9/12	Hayfield	Ft. Belvoir	4:00 pm
9/18	Lake Braddock	Twin Lakes	4:00 pm

Head Coach - Larry Mitchell

GIRLS TENNIS

DATE	OPPONENT	LOCATION	TIME
→ 8/19	West Springfield	Home	4:00 pm
8/21	Robinson	Away	4:00 pm
→ 8/26	Annandale	Home	4:00 pm
8/28	Hayfield	Away	4:00 pm
→ 8/30	West Potomac	Home	4:00 pm
9/3	Lake Braddock	Away	4:30 pm
9/5	West Springfield	Away	4:30 pm
→ 9/9	Robinson	Home	4:30 pm
9/13	Annandale	Away	4:30 pm
→ 9/19	Hayfield	Home	4:30 pm
9/24	West Potomac	Away	4:30 pm
→ 9/27	Lake Braddock	Home	4:30 pm
10/2-7	District Tournament	Wakefield Park	TBA

Head Coach - Chavez Mabry

OVER

2002 FALL ATHLETIC SCHEDULE - Sports Hotline TC1-5304

T.C. Williams High School - 3330 King Street - Alexandria, Virginia 22302

8/7/02

		<u>VOLLEYBALL-GIRLS</u>			
<u>DATE</u>	<u>OPPONENT</u>	<u>LOCATION</u>	<u>TIME</u>	<u>VAR</u>	<u>JV</u>
→ 8/26	Annandale (SC)	Home	5:15 pm		4:00 pm
8/27	W. Springfield (SC)	Away	7:15 pm		6:00 pm
9/4	Chantilly	Away	7:15 pm		6:00 pm
→ 9/5	Falls Church	Home	7:15 pm		6:00 pm
→ 9/10	West Potomac	Home	7:15 pm		6:00 pm
9/12	Robinson	Away	7:15 pm		6:00 pm
→ 9/17	Lake Braddock	Home	7:15 pm		6:00 pm
9/19	Annandale	Away	7:15 pm		6:00 pm
→ 9/24	Hayfield	Home	7:15 pm		6:00 pm
9/26	West Springfield	Away	7:15 pm		6:00 pm
→ 10/1	Edison	Home	7:15 pm		6:00 pm
10/3	West Potomac	Away	7:15 pm		6:00 pm
→ 10/8	Robinson	Home	7:15 pm		6:00 pm
10/10	Lake Braddock	Away	7:15 pm		6:00 pm
→ 10/16	Annandale	Home	7:15 pm		6:00 pm
10/17	Hayfield	Away	7:15 pm		6:00 pm
→ 10/22	West Springfield	Home	7:15 pm		6:00 pm
10/24	Woodson	Away	7:15 pm		6:00 pm
10/28-11/2	District Tournament @ Robinson		TBA		
11/4-9	Regional Tournament @ Westfield		TBA		
11/12-16	State Tournament		TBA		TBA
<i>Head Coach - Rhea Butler</i>					

SPORTS HOTLINE - TC1-5304

2002-2003 WINTER ATHLETIC SCHEDULE - Sports Hotline TC1-5304

T.C. Williams High School - 3330 King Street - Alexandria, Virginia 22302

12/10/02

VARSITY BOYS BASKETBALL

OPPONENT	LOCATION	TIME
RE Lee	Away	7:45 PM
Mount Vernon	Home	7:45 PM
Yorktown	Home	7:45 PM
St. Stephens/St. Agnes	Away	7:30 PM
16 W&L	Away	7:45 PM
South Lakes	Away	7:45 PM
West Springfield	Home	7:45 PM
28 2002 Slam Festival	Harrisburg, Pa.	TBA
Lake Braddock	Away	7:45 PM
West Potomac	Away	7:45 PM
Robinson	Home	7:45 PM
Hayfield	Away	7:45 PM
Annandale	Home	7:45 PM
West Springfield	Away	7:45 PM
Lake Braddock	Home	7:45 PM
West Potomac	Home	7:45 PM
Henrico	Away	4:00 PM
Robinson	Away	7:45 PM
Edison	Home	7:45 PM
Hayfield	Home	7:45 PM
Annandale	Away	7:45 PM
7-21 District Tournament	Lake Braddock	T.B.A.
1-28 Regional Tourn.	T.B.A.	T.B.A.

VARSITY GIRLS BASKETBALL

OPPONENT	LOCATION	TIME
RE Lee	Home	7:45 PM
Mount Vernon	Away	7:45 PM
Yorktown	Away	7:45 PM
16 W&L	Home	7:45 PM
South Lakes	Home	7:45 PM
Paul VI	Away	7:45 PM
6-28 Stafford Classic	Away	TBA
Lake Braddock	Home	7:45 PM
West Potomac	Home	7:45 PM
Robinson	Away	7:45 PM
West Springfield	Away	7:45 PM
Hayfield	Home	7:45 PM
Annandale	Away	7:45 PM
West Springfield	Home	7:45 PM
Lake Braddock	Away	7:45 PM
West Potomac	Away	7:45 PM
Robinson	Home	7:45 PM
Edison	Away	7:45 PM
Hayfield	Away	7:45 PM
Annandale	Home	7:45 PM
7-21 District Tournament	Lake Braddock	T.B.A.
1-28 Regional Tourn	T.B.A.	T.B.A.

=Reschedule

FRESHMAN / JV BOYS BASKETBALL

DATE	OPPONENT	LOCATION	TIME	JV
12/03	RE Lee	Away	4:30 PM	6:00 PM
12/10	Mount Vernon	Home	4:30 PM	6:00 PM
12/13	Yorktown	Home	4:30 PM	6:00 PM
R12/16	W&L	Away	4:30 PM	6:00 PM
12/17	South Lakes	Away	4:30 PM	6:00 PM
12/20	West Springfield	Home	4:30 PM	6:00 PM
01/06	Lake Braddock	Away	4:30 PM	6:00 PM
01/08	West Potomac	Away	4:30 PM	6:00 PM
01/10	Robinson	Home	4:30 PM	6:00 PM
01/17	Hayfield	Away	4:30 PM	6:00 PM
01/21	Annandale	Home	4:30 PM	6:00 PM
01/24	West Springfield	Away	4:30 PM	6:00 PM
01/28	Lake Braddock	Home	4:30 PM	6:00 PM
01/31	West Potomac	Home	4:30 PM	6:00 PM
02/01	Henrico	Away	4:00 PM	
02/04	Robinson	Away	4:30 PM	6:00 PM
02/07	Edison	Home	4:30 PM	6:00 PM
02/11	Hayfield	Home	4:30 PM	6:00 PM
02/13	Annandale	Away	4:30 PM	6:00 PM

FRESHMAN / JV GIRLS BASKETBALL

DATE	OPPONENT	LOCATION	TIME	JV
12/03	Re Lee	Home	4:30 PM	6:00 PM
12/10	Mount Vernon	Away	4:30 PM	6:00 PM
12/13	Yorktown	Away	4:30 PM	6:00 PM
R12/16	W&L	Home	4:30 PM	6:00 PM
12/17	South Lakes	Home	4:30 PM	6:00 PM
12/20	Paul VI	Away	4:30 PM	6:00 PM
01/06	Lake Braddock	Home	4:30 PM	6:00 PM
01/08	West Potomac	Home	4:30 PM	6:00 PM
01/10	Robinson	Away	4:30 PM	6:00 PM
01/13	West Springfield	Away	4:30 PM	6:00 PM
01/17	Hayfield	Home	4:30 PM	6:00 PM
01/21	Annandale	Away	4:30 PM	6:00 PM
01/24	West Springfield	Home	4:30 PM	6:00 PM
01/28	Lake Braddock	Away	4:30 PM	6:00 PM
01/31	West Potomac	Away	4:30 PM	6:00 PM
02/04	Robinson	Home	4:30 PM	6:00 PM
02/07	Edison	Away	4:30 PM	6:00 PM
02/11	Hayfield	Away	4:30 PM	6:00 PM
02/14	Annandale	Home	4:30 PM	6:00 PM

INDOOR TRACK & FIELD

DATE	OPPONENT	LOCATION	TIME
12/14	STU-FC-MTVERN	E.H.S.	7:30 AM
	Mason Relays	G.M.U.	9:00 AM
12/21	F.I.T. INVITATIONAL	G.M.U.	8:00 AM
	WOOD-MARSH-MTV	E.H.S.	7:30 AM
R-1/4	WFLD-WS-LB	E.H.S.	7:30 AM
01/11	MAD-RELEE-HAY	E.H.S.	7:30 AM
	Montgomery Invite	P.G.C.	TBA
01/18	ED-MAD-MARSH	E.H.S.	7:30 AM
01/25	FC-CENT-HERN	E.H.S.	7:30 AM
02/01	JEFF-SL-FX-LANG	E.H.S.	7:30 AM
02/08	Patriot Dist Champ	E.H.S.	7:30 AM
02/22	Northern Regional	G.M.U.	7:30 AM
02/28-3/1VA	State AAA Champ	G.M.U.	8:00 AM

R=Reschedule

2002-2003 WINTER ATHLETIC SCHEDULE - Sports Hotline TC1-5304

T.C. Williams High School - 3330 King Street - Alexandria, Virginia 22302

12/10/02

SWIM & DIVE

<u>DATE</u>	<u>OPPONENT</u>	<u>LOCATION</u>	<u>TIME</u>
12/07	Robinson	Mt. Vernon	2:30 PM
12/13	Edison	Lee District	8:30 PM
12/20	Annandale	Mount Vernon	6:30 PM
01/10	Hayfield	Lee District	6:00 PM
R-1/11	West Springfield	Mt. Vernon	6:30 PM
01/17	West Potomac	Lee District	8:30 PM
01/24	Lake Braddock	Wakefield Pk	6:00 PM
01/31, 2/1	District Meet	Lee District	T.B.A.
02/05	Regional Dive-Girls	Oak Marr	T.B.A.
02/06	Regional Dive-Boys	Oak Marr	T.B.A.
02/07, 08	Regional Swim/Dive	Oak Marr	T.B.A.
02/14, 15	State Swim/Dive	T.B.A.	T.B.A.

Makeup - Snow Date #1 January 11, 2003 for Meet #1 or #2 or #3
 Snow Date #2 January 18, 2003 for Meet #4 or additional
 date for Dec Meets, Snow Date #3 January 25, 2003 for Meet
 #5 or #6 (No make-up for meet #7)

WRESTLING

<u>DATE</u>	<u>OPPONENT</u>	<u>LOCATION</u>	<u>TIME</u>	
			VAR	JV
12/06, 07	No. Reg. Classic	Fairfax	10:00 am	_____
→ 12/11	JV Jamboree	Home	_____	5:30 am
→ 12/14	JV Jamboree	Madison	9:00 am	_____
12/18	JV Jamboree	Marshall	_____	6:00 pm
12/21	NJ Holiday Tourn	Moorestown HS	TBA	TBA
12/27, 28	HHHE Tourn	HHHE HS	TBA	TBA
→ 01/07	W&L	Home	7:30 pm	6:00 pm
→ 01/11	WDS/FC/GM	Home	1:00 pm	_____
01/15	South Lakes	Away	7:30 pm	6:00 pm
01/18	MARS/WP	Marshall	1:00 pm	_____
01/18	JV Jamboree	Robinson	_____	9:00 am
01/25	WS/LB/ANN	L. Braddock	2:00 pm	_____
01/25	JV Jamboree	Annandale	_____	9:00 am
→ 01/29	Yorktown	Home	7:30 pm	6:00 pm
→ 02/01	WAKE/ED/STU	Home	1:00 pm	_____
02/05	ROB/MTV	Robinson	5:30 pm	_____
02/14, 15	Fat Dist Tourn	Annandale	TBA	_____
02/20-21	No. Reg Tourn	Robinson	TBA	_____
02/28-3/1	AAA State Tour	D. Smith	TBA	_____

**Winter Sports Student/Athlete Recognition
 Night - March 21, 2003 6:45 pm TC
 Williams Main Auditorium**

R=Reschedule

SPORTS HOTLINE - TC1-5304

2003 Spring Sports Schedules

2/13/03

BASEBALL-VARSITY

Home games played @ Simpson Field

DATE	OPPONENT	LOCATION	TIME
5	Paul IV	Home	6:00 pm
7	Edison	Home	6:00 pm
9	Bishop Ireton	Away	1:00 pm
2	West Springfield	Home	6:00 pm
6	Robinson	Away	7:00 pm
8	Cardoza (D.C.)	Home	5:00 pm
1	Annandale	Home	7:00 pm
4	Hayfield	Away	6:00 pm
7	West Potomac	Home	6:00 pm
9	Lake Braddock	Away	7:00 pm
1	Washington & Lee	Home	2:00 pm
4	Snow	Home	1:00 pm
5	St. Thomas (V.I.)	Home	TBA
6	West Springfield	Away	6:00 pm
3	Robinson	Home	7:00 pm
5	Osbourne	Away	6:00 pm
0	Annandale	Away	7:00 pm
12	Hayfield	Home	6:00 pm
15	West Potomac	Away	6:00 pm
19	Lake Braddock	Home	7:00 pm
14,16,19	District Tourm	Hayfield	TBA
23,26,28,30	Regional Tourm	TBA	TBA
1,6,7	State Tourm	TBA	TBA

Head Coach: Mike Witman

BASEBALL-JUNIOR VARSITY

Home games played @ Simpson Field

DATE	OPPONENT	LOCATION	TIME
19	Edison	Away	5:00 p.m.
21	Washington & Lee	Home	6:00 p.m.
22	Bishop Ireton	Home	11:00 am.
24	Mt. Vernon	Away	5:00 p.m.
26	West Springfield	Away	6:00 p.m.
28	Robinson	Home	6:00 p.m.
04	Annandale	Away	6:00 p.m.
07	Hayfield	Home	6:00 p.m.
09	West Potomac	Away	6:00 p.m.
10	Lake Braddock	Home	6:00 p.m.
12-16	Hylton Tournament	Hylton HS	TBA
23	West Springfield	Home	6:00 p.m.
25	Robinson	Away	6:00 p.m.
02	Annandale	Home	6:00 p.m.
05	Hayfield	Away	6:00 p.m.
07	West Potomac	Home	6:00 p.m.
09	Lake Braddock	Away	6:00 p.m.

Head Coach: Mike Witman

CREIK

DATE	OPPONENT	LOCATION	TIME
2/29	Run vs Row	Home	7:00 a.m.
	Yorktown - W&L		
4/04	TJ-St. Albans-No. Cath.	Ocoquan	TBA
4/12	St. Andrews	St. Andrews	1:00 p.m.
4/19	St. Andrews	St. Andrews	1:00 p.m.
5/26	Virginia Championships	Ocoquan	TBA
5/03	Anacostia Classic (lwr boats)	Anacostia	TBA
5/10	NCASRA Championships	Ocoquan	TBA
5/16,17	Statesbury	Phila. Pa.	TBA
5/23,24	SRA Championships	Camden, NJ	TBA
5/30,6/1	CSSRA Championships	Ontario, Can	TBA

Head Coach-Boys: Mike Penn
Head Coach-Girls: Steve Weir

LACROSSE-BOYS-Varsity/Junior Varsity

DATE	OPPONENT	LOCATION	VAR	JV
03/17	Falls Church	Home	4:30 p.m.	6:00 p.m.
03/19	Wakefield	Home	4:30 p.m.	-----
03/21	Washington-Lee	Away	5:30 p.m.	-----
03/27	Marshall	Home	4:30 p.m.	6:00 p.m.
03/31	Yorktown	@ Kenmore M.S.	5:00 p.m.	-----
04/04	Lake Braddock	Away	7:15 p.m.	5:30 p.m.
04/08	Mt. Vernon	Home	-----	4:30 pm
04/10	West Potomac	Home	4:30 p.m.	6:00 pm-
04/23	McLean	Away	7:00 pm	5:30 p.m.
04/25	Annandale	Home	4:30 p.m.	6:00 pm
04/29	George Mason	Home	4:30 pm	6:00 p.m.
05/01	Robinson	Away	7:15 p.m.	5:30 p.m.
05/05	West Springfield	Away	7:15 p.m.	5:30 p.m.
05/09	Hayfield	Home	4:30 p.m.	6:00 p.m.
05/12,13,15	District Tourm.	TBA	TBA	TBA
05/21,28,30,06/3	Regional Tourm.	TBA	TBA	TBA
06/08	State Tourm.	TBA	TBA	TBA

Head Coach Varsity: Charles Juris
Head Coach JV:

LACROSSE-GIRLS-Varsity/Junior Varsity

DATE	OPPONENT	LOCATION	VAR	JV
03/17	Falls Church	Away	7:15 pm	6:00 pm
03/19	Wakefield	Away	7:15 pm	-----
03/19	McLean	Away	-----	5:00 pm
03/21	Washington-Lee	Home	4:30 pm	-----
03/25	Episcopal	Away	4:15 pm	-----
03/27	Marshall	Away	7:15 pm	5:00 pm
03/31	Yorktown	Home	4:30 pm	-----
04/02	West Potomac	Away	7:15 pm	6:00 pm
04/04	Lake Braddock	Home	4:30 pm	5:30 pm
04/07	George Mason	Away	4:30 pm	5:45 pm
04/21	Hyde School	Home	-----	5:00 pm
04/25	Annandale	Away	7:15 pm	6:00 pm
05/01	Robinson	Home	5:00 pm	6:15 pm
05/05	West Springfield	Home	5:00 pm	6:15 pm
05/09	Hayfield	Away	7:15 pm	6:00 pm
05/12,13,15	District Tourm.	TBA	TBA	TBA
05/21,28,30,06/3	Reg Tourm.	TBA	TBA	TBA
06/07	State Tourm.	TBA	TBA	TBA

Head Coach Varsity: Megan Watson
Head Coach JV: TBA

OUTDOOR TRACK & FIELD

DATE	OPPONENT	LOCATION	TIME
03/26	W. Potomac-L. Braddock	L. Braddock	5:00 p.m.
03/28,29	Manchester Relays	Richmond	TBA
04/02	Robinson-Hayfield	Hayfield	5:00 p.m.
04/05	Scudder Invit.	W. Potomac	9:00 a.m.
04/09	Hayfield-W. Potomac	Home	5:00 p.m.
04/12	Morgan State Invite.	Morgan State	TBA
04/23	Annandale-W. Springfield	Annandale	5:00 p.m.
04/24-26	Penn Relays	Phila. Pa.	TBA
05/03	TC Williams Invite.	Home	9:00 a.m.
05/07	W. Potomac-Robinson	Robinson	5:00 p.m.
05/10	TJ Penultimate	T. Jefferson	9:00 a.m.
05/14,15	District Meet	Home	4:00 p.m.
5/22,23	Regional Meet	L. Braddock	4:00 p.m.
5/30,31	State Meet	Richmond	1:00 p.m.

Head Coach: Teresa Henderson

Over

BOYS SOCCER

OPPONENT	LOCATION	TIME
		VAR JV
Yorktown	Away	7:30 p.m. 6:00 pm
JEB Stuart ←	Home	4:15 p.m. 6:00 pm
Langley ←	Home	4:15 p.m. 6:00 p.m.
Mt. Vernon	Away	7:00 p.m. 5:30 p.m.
Washington-Lee	Away	7:00 p.m. 5:30 p.m.
West Springfield ←	Home	4:30 pm 6:15 p.m.
16 McLean Invite	McLean	TBA
Robinson	Away*	7:00 p.m. -----
Robinson ←	Home*	----- 4:30 p.m.
Falls Church	Away	7:00 p.m. 5:30 p.m.
Annandale ←	Home	4:30 p.m. 6:15 p.m.
Hayfield	Away	7:00 p.m. 5:30 p.m.
West Potomac ←	Home	4:30 p.m. 6:15 p.m.
Lake Braddock	Away	7:00 p.m. 5:30 p.m.
14,16 District Tour.	TBA	TBA
22,28,30 Regional Tour.	TBA	TBA
State Tour.	TBA	TBA

Coach: Marilyn Nickley
Games with Robinson are doubleheaders

GIRLS SOCCER

OPPONENT	LOCATION	TIME
		VAR JV
8 Madison	Home ←	4:00 pm 5:30 pm
10 Centreville	Away	7:00 p.m. 5:30 p.m.
16 Yorktown	Home ←	4:00 p.m. 5:45 p.m.
18 JEB Stuart	Away	7:00 p.m. 5:30 p.m.
11 Langley	Away	7:00 p.m. 5:30 p.m.
13 Mt. Vernon	Home ←	4:30 p.m. 6:15 p.m.
17 Washington-Lee	Home ←	4:30 p.m. 6:15 p.m.
11 West Springfield	Away	7:00 p.m. 5:30 p.m.
22 Robinson	Away*	5:30 p.m. -----
22 Robinson	Home* ←	----- 6:15 p.m.
24 Falls Church	Home ←	4:30 p.m. 6:15 p.m.
28 Annandale	Away	7:00 p.m. 5:30 p.m.
02 Hayfield	Home ←	4:30 p.m. 6:15 p.m.
06 West Potomac	Away	7:00 p.m. 5:30 p.m.
08 Lake Braddock	Home ←	4:30 p.m. 6:15 p.m.
12,14,16 District Tour.	TBA	TBA
20,22,28,30 Regional Tour.	TBA	TBA
1A State Tour.	TBA	TBA

Games with Robinson are doubleheaders
Head Coach Varsity: Emily Porterfield
Head Coach JV: Bill Harsanlak

SOFTBALL-VARSITY

Home games played @ Minnie Howard School

DATE	OPPONENT	LOCATION	VAR	JV
03/19	JEB Stuart	Home	6:00 p.m.	6:00 pm
03/26	West Springfield	Home	7:00 p.m.	5:00 pm
03/28	Robinson	Away	7:00 p.m.	5:00 pm
04/04	Annandale	Home	7:00 p.m.	5:00 pm
04/05	Alexandria Tour.	Hensley Field	2/4:00 p.m.	-----
04/07	Hayfield	Away	7:00 p.m.	5:00 pm
04/09	West Potomac	Home	7:00 p.m.	5:00 pm
04/11	Lake Braddock	Away	7:00 p.m.	5:00 pm
04/23	West Springfield	Away	7:00 p.m.	5:00 pm
04/24	JEB Stuart	Away	5:30 p.m.	5:00 pm
04/25	Robinson	Home	7:00 p.m.	5:00 pm
05/02	Annandale	Away	7:00 p.m.	5:00 pm
05/05	Hayfield	Home	7:00 p.m.	5:00 pm
05/07	West Potomac	Away	7:00 p.m.	5:00 pm
05/09	Lake Braddock	Home	7:00 p.m.	5:00 pm
05/16,19,20	District Tour.	TBA	TBA	TBA
05/23,26,28	Regional Tour	TBA	TBA	TBA
06/3,6,7	State Tour.	TBA	TBA	TBA

Head Coach Varsity: Dee Marrara
Head Coach JV: Dawn Crawford

TENNIS-BOYS

DATE	OPPONENT	LOCATION	TIME
03/19	West Springfield	Home ←	4:30 p.m.
03/21	Robinson	Away	4:30 p.m.
03/26	Annandale	Home ←	4:30 p.m.
03/28	Hayfield	Away	4:30 p.m.
04/1	West Potomac	Home ←	4:30 p.m.
04/3	Lake Braddock	Away	4:30 p.m.
04/7	West Springfield	Away	4:30 p.m.
04/9	Robinson	Home ←	4:30 p.m.
04/22	Annandale	Away	4:30 p.m.
04/24	Hayfield	Home ←	4:30 p.m.
04/28	West Potomac	Away	4:30 p.m.
04/30	Lake Braddock	Home ←	4:30 p.m.
05/5-9	District Tour	West Potomac	TBA
05/12-21	Regional Tour	Wakefield Pk	TBA
TBA	State Tour	TBA	TBA

Head Coach: Maurice Willacy

Sports Hotline 703-TC1-5304

Website - www.acps.k12.va.us/tcw
jump to: athletics

T. C. Williams Parking Management Plan

The following steps will be taken to manage and limit parking at the new high school:

1. The new high school is planned to have a capacity of 431 parking spaces. The spaces are broken down as follows:

416 Parking Structure spaces
8 Visitor @ circle
<u>7 Rear</u>
431 Total

The parking structure will allow 198 spaces on the lower level and 218 spaces in the upper level. The structure will allow staff parking in the first level and partially in the second level. The remaining spaces will allow visitor and student permit parking. The total number of permits issued will not exceed the difference of 431 spaces minus the number of staff and a minimum of 15 visitor spaces.

2. Employees and students will be encouraged to carpool whenever possible.
3. The school system will investigate incentives to encourage employees to use public transportation such as Dash passes or Metro Check which allows employees to purchase fare cards or vouchers for Metro/Metro bus and Dash with pretax dollars through payroll deduction.
4. Certain adult education classes may be relocated from T.C. Williams to another ACPS facility.
5. Stagger the schedule for some Adult Education and high school education programs.
6. Control the scheduling of major events in order to avoid peak crowds, thereby decreasing the number of spaces needed by at least another 100.
7. Continue cooperative arrangements for use of space in neighboring lots for such large events as graduation, College Night, Titan Expo, and other events that require overflow capacity.
8. Explore additional scheduling options to limit the peak number of spaces needed.
9. Continue working with the Department of Recreation, Parks and Cultural Affairs to create opportunities for programs on both sites to share parking facilities as needed.
10. Students and staff will be encouraged to ride bicycles to school and work. Bicycle racks will be provided to provide a secure place to store the bicycle during the day.
11. Appoint an administrator to coordinate parking management supervision and activities.
12. T. C. Williams Parking Management Plan will be reviewed on a yearly basis and changed to reflect varying parking use patterns with the goal to accommodate all daily parking on-site.



WELLS & ASSOCIATES, LLC

TRAFFIC, TRANSPORTATION, and PARKING CONSULTANTS

TECHNICAL MEMORANDUM

TO: James Copeland
Moseley Architects

FROM: Lawrence Sefcik
Wells & Associates, LLC

DATE: November 26, 2003

RE: T.C. Williams High School
Traffic Impact Study Addendum
City of Alexandria, Virginia

INTRODUCTION

This memorandum updates the T.C. Williams High School Traffic Impact Study (TIS) prepared by Wells & Associates, dated March 4, 2003 in conjunction with the proposed reconstruction and expansion of T.C. Williams High School. The addendum is based on the revised site plan, prepared by ADTEK Engineers, Inc., dated November 7, 2003. This site is located in the City of Alexandria, Virginia, as shown on Figure 1 found in the appendix.

This school serves all high school students (grades 10-12) in the City of Alexandria. The school capacity will be increased to accommodate 2,500 students and 300 staff. The existing surface parking lots in front of the school will be relocated and replaced with a proposed two-story parking garage on the west side of the school site. A new school bus staging area will be constructed between the new building and parking garage structure, and along the south side of the parking garage with access at the west driveway location. Two pick-up/drop-off areas are proposed. One will be located in front of the school opposite Kenwood Avenue; the other along Chinquapin Drive. Two access points to the school site are proposed opposite Radford Street and Kenwood Avenue. Although Chinquapin Drive does not provide direct access to the site, it runs along the east side and is utilized by the school.

145

Work is scheduled to begin in Summer, 2004. The main building will be completed for occupancy by September, 2007. The parking structure and playfields are expected to be completed by Spring, 2008. For purposes of his study, 2007 was used as the school building project completion year.

Specific tasks completed in this addendum included the following:

- 1.) Review the revised site plan for the renovation/expansion of T.C. Williams High School including the new proposed parking areas and student pick-up/drop-off areas.
- 2.) Estimation of the total future traffic utilizing the following key intersections based on the revised site plan:
 - a. King Street/Chinquapin Drive
 - b. King Street/Kenwood Avenue
 - c. King Street/Radford Street
 - d. Kenwood Avenue/Braddock Road
- 3.) Analysis of the levels of service (LOS) at the key intersections for the AM and PM school peak hour periods under total future conditions.
- 4.) Determination of whether a right turn lane is warranted on Chinquapin Drive based on future levels of service and the revised site plan, which includes two student pick-up/drop-off areas.
- 5.) Evaluation of whether police control and/or signal control will be appropriate at the King Street/Radford Street/proposed school bus driveway intersection. This included an evaluation of the Peak Hour Warrant, as listed in the Manual On Uniform Traffic Control Devices (MUTCD).
- 6.) Review of the on-site traffic circulation in front of the school at the proposed student pick-up/drop-off and parking garage areas. This included an evaluation of

the anticipated length of queue during AM and PM school peak hours.

- 7.) Review the alignment of the proposed school entrance opposite Kenwood Avenue.

ANALYSIS

Site Trip Generation and Distribution

As stated in the March 4, 2003 TIS, the expansion of the school is expected to generate an additional 140 trips during the AM peak hour and 76 trips during the school PM peak hour. The distribution of these additional site-generated trips on roadways serving the school site was determined based on existing traffic counts, on-site observations, proposed parking areas, and engineering judgment. The distribution between the access points to the school was based on existing traffic counts, the proposed locations of the student pick-up/drop-off areas, the revised site plan dated November 7, 2003, and the proposed parking area.

Approximately 36 percent of all site-generated traffic approaches/departs the site on eastbound King Street, approximately 34 percent on westbound King Street, approximately 3 percent on Radford Street, and approximately 27 percent on Kenwood Avenue. It is further estimated that of all the traffic that will approach the site, approximately 9 percent will utilize the west driveway, approximately 76 percent the middle driveway opposite Kenwood Avenue, and approximately 15 percent will utilize Chinguapin Drive for the student pick-up/drop-off area and some parking along Chinguapin Drive.

The existing site volumes were re-adjusted to reflect the expected change in traffic patterns due to the relocation of parking and student pick-up/drop-off areas.

The resulting assignments of site-generated traffic (existing and additional) and directional distributions are found in the appendix on Figure 9 (Revised).

Total Future Traffic Forecasts

Total future traffic forecasts were derived by adjusting the background traffic volumes discussed previously to reflect the anticipated traffic pattern changes at the site due to the relocation of parking lots and relocation of student pick-up/drop-off area to Chinquapin Drive. This is shown on Figure 10 (Revised) found in the appendix.

Future Intersection Levels of Service

Total future levels of service were calculated based on the total future traffic forecasts shown on Figure 10 (Revised), the proposed lane use and traffic controls shown on Figure 5 (Revised), and the Highway Capacity Manual, as required by the City of Alexandria. The results are presented in the appendix and summarized in Table 1.

Table 1 indicates that the signalized key intersections will operate at an overall LOS "D" or better during both peak periods. All critical movements at the unsignalized key intersections will operate at LOS "D" or better except for the northbound movement at the King Street/Chinquapin Drive intersection. The northbound shared left/right movement on Chinquapin Drive will operate at LOS "E" during the AM peak period and LOS "F" during the PM peak period. If two outbound lanes are provided, the northbound right turn lane will operate at a LOS "B" during both peak periods. The northbound left turn lane will continue to operate at LOS "F" during the PM peak period. An additional lane is recommended to reduce delays and queues.

Two northbound lanes are proposed exiting the school site opposite Kenwood Avenue. This approach will operate at LOS "B" during both peak periods and should adequately accommodate anticipated school traffic.

Vehicle Queues

Existing. On Tuesday, February 11, 2003, field observations of queues in front of the school entrance were conducted during the AM peak and school PM peak periods.

The roadway in front of the school at the main building entrance is divided with a raised concrete median. This section of roadway is one-way in the eastbound direction. Buses use the curbside to pick-up and discharge students. The other side of this divided roadway is primarily used as a pick-up/drop-off area. The queue typically begins near the main doorway and extends west past the divided section. The maximum queue length of 15 vehicles (or 375 feet) was observed during the school PM peak period. Vehicles were also observed (maximum of 6 vehicles) in a queue on Chinquapin Drive and in the Chinquapin Park Recreation Center parking lot.

Future. As noted in the March 4, 2003 study, school enrollment will increase from 2,147 to a capacity of 2,500 students or by 16 percent. Queues were also assumed to increase in proportion to the increase in student population.

The proposed student pick-up/drop-off area, according to the revised site plan dated November 7, 2003, proposes two areas. One will be located in front of the school opposite Kenwood Drive; the other along Chinquapin Drive. The existing maximum observed queue in front of the school is 375 feet (or 15 vehicles) which occurred during the school PM peak period. The maximum queue on Chinquapin Drive is 150 feet (or six vehicles). When the queues are combined and increased to reflect anticipated expansion (16 percent), a maximum queue of approximately 600 feet (or 24 vehicles) during the school PM peak period is expected.

Three floors of academic classrooms are proposed on the Chinquapin Drive side of the school building. A circular turnaround area on Chinquapin Drive near the end of the school site is also proposed. Based on this information, it is estimated that the student pick-up/drop-off queue length will be evenly split (or 300 feet) between this location along Chinquapin Drive and the proposed area in front of the school.

The available storage length on Chinguapin Drive between King Street and the proposed south end of the drop-off/pick-up area is approximately 400 feet from King Street. This includes the approximately 125 foot separate staging area to be used for this maneuver. Another 150 feet between the south end of the staging area and the beginning of the proposed traffic circle could also be utilized for pick-up/drop-off area. Parking restrictions are currently in place along Chinguapin Drive adjacent to the school property and may need to be revisited in order to accommodate pick-up/drop-off vehicles. The anticipated queue can be accommodated by removing existing parking restrictions along the 31-foot wide Chinguapin Drive, striping the road to delineate pick-up/drop-off areas, and/or directing traffic via a traffic control person/police officer.

The available storage length for the pick-up/drop-off area in front of the school between the east end of the proposed visitor parking lot and the parking garage entrance is approximately 200 feet. Another 150 feet exists between the parking garage and King Street. Queues of 300 feet are expected. If both of the areas are utilized, then queues can be accommodated without spillover onto King Street.

Traffic Signal/Police Control

The school bus entrance opposite Radford Street will be utilized primarily by school buses. During the school PM peak period, 29 buses will exit at approximately the same time. In order to assess whether a signal would be appropriate, an analysis was conducted in accordance with the methodology found in the Manual on Uniform Traffic Control Devices (MUTCD), Millennium Edition. One or more of the eight warrants must be satisfied before a new signal is considered for installation; however, satisfaction of a warrant does not in itself justify the need for a new signal. A new traffic signal should improve the overall safety or operation of the intersection.

For the scope of this study, only the Peak Hour Warrant was analyzed since if this warrant is not met, it is highly unlikely that the other volume warrants will be met. Since pedestrians

can cross at Kenwood Avenue in front of the school with pedestrian activated buttons, the pedestrian warrant was not analyzed at this location.

Total future forecasts, as shown on figure 10 (Revised) in the appendix was utilized to evaluate both the AM and school PM peak hours. These volumes were plotted on the Peak Hour Warrant graph as shown in the appendix. The maximum minor street approach volume is 49 vehicles, well below the 100 volume threshold. Based on this analysis, The Peak Hour Warrant was not met under total future conditions at the King Street/Radford Street/School Bus Driveway intersection.

All buses are expected to exit the site at the same time during the school PM peak. If a Special Resource Officer or police department personnel could stop traffic momentarily at this location, then school bus traffic could easily execute left and right turn maneuvers. This school PM "peak" period should only last a few minutes. A similar situation occurs at nearby Minnie Howard Ninth Grade Center. A Special Resource Officer stops traffic on Braddock Road to allow buses to exit. This operation was observed to function efficiently.

School buses arrived randomly during the AM peak period and should not encounter excessive delay either entering or exiting the facility. A LOS "D" or better is expected for all critical movements during the AM peak period at this location.

On-site Traffic Operations In Front of the School

Based on the revised site plan for the project, a review of the on-site traffic circulation in front of the school opposite Kenwood Avenue indicated the following:

1. Driving confusion may result upon exiting the parking garage as to whether to proceed around the circular driveway or simply access the main access drive to King Street. This area should be designated "one way" at all times to eliminate confusion. Signs and pavement markings are needed to clarify this situation.

2. Vehicle queues in front of the school are anticipated to be contained on site if the area between the parking garage and King Street is utilized.
3. Pedestrian and vehicular mixing will occur if the student pick-up/drop-off area is allowed between the parking garage and King Street.
4. All exiting parking garage traffic would be forced around the circular driveway in front of the school mixing vehicular traffic waiting to pick-up/drop-off students with vehicles wanting to exit the site.
5. If the main school driveway directly accessed the parking garage on more direct alignment, the queuing/visitor area could be distanced to provide clearer access.
6. Congestion is likely to occur in this area given the close proximity of the parking garage entrance, visitor parking, pick-up/drop-off area, and pedestrian activity.

Road Alignment

Southbound Kenwood Avenue, as it intersects King Street opposite the proposed main school driveway, is skewed. The proposed site plan shows the double yellow line being shifted approximately ten feet to the west to better align the roadway with the proposed school entrance.

The maximum future forecasted through volume from Kenwood Avenue to T.C. Williams High School driveway is 147 vehicles during the AM peak period. Most traffic should be local and familiar with the geometrics.

Additional pavement markings including a painted median, channelization island and pavement marking arrows could be installed to clarify and help direct motorists through the intersection. A sketch (Figure 11) is included in the appendix that shows the proposed alignment improvements utilizing pavement markings.

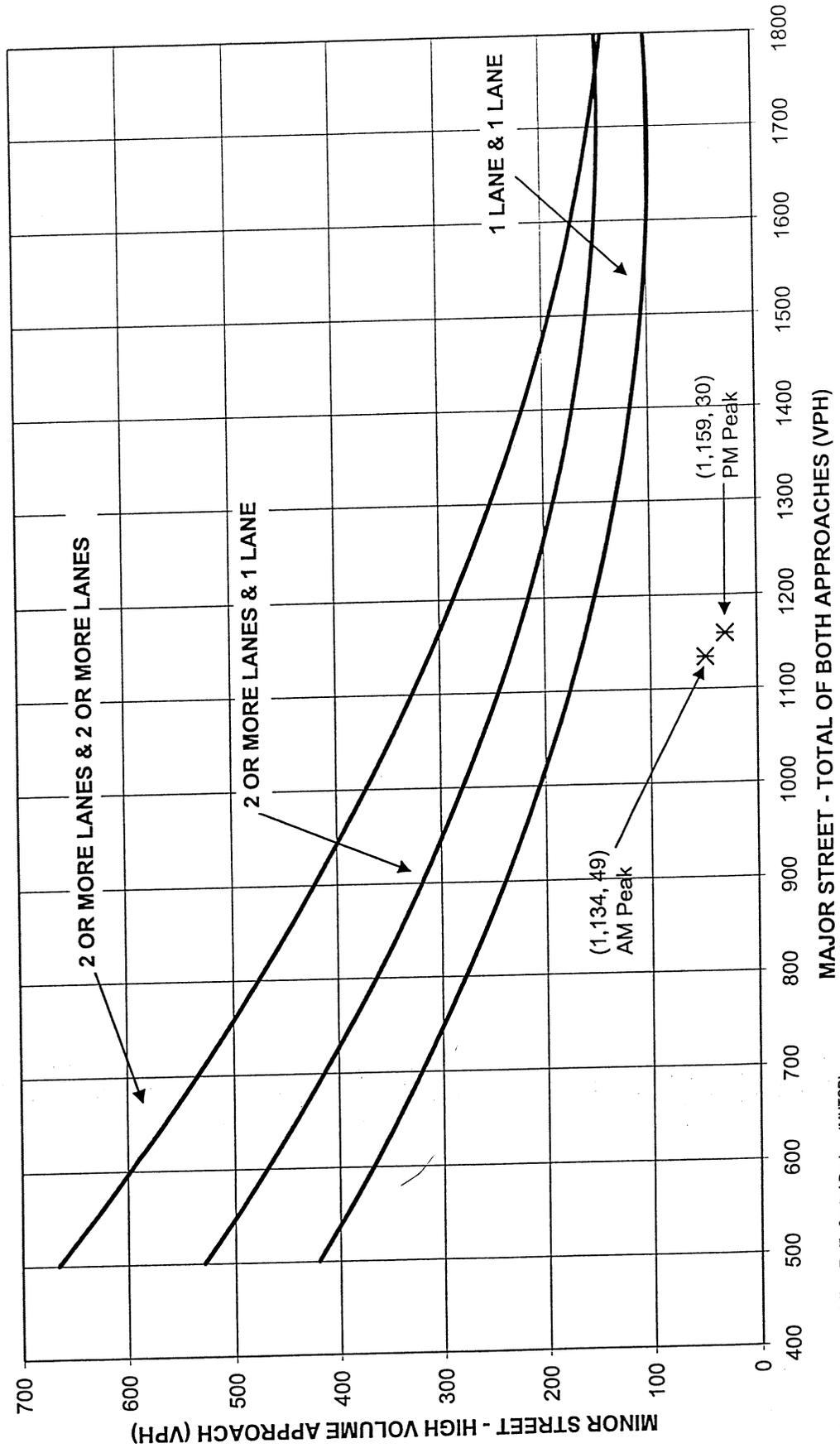
CONCLUSIONS

The conclusions of this addendum are as follows:

1. The critical movements under future conditions at all key intersections in the study area will operate at LOS "D" or better during both peak periods with the exception of the King Street/Chinquapin Drive intersection. The northbound critical movement will operate at capacity during both peak periods. An additional outbound lane will improve the right turn movement to a LOS "B" during both peak periods. Left turn delays will continue to exceed acceptable levels of service, although delays would be reduced by providing a second outbound lane.
2. Based on the revised site plan, exiting parking garage traffic will be forced to turn right and mix with queuing traffic. Congestion is likely to occur in this area given the close proximity of the parking garage entrance, visitor parking, pedestrian activity, and pick-up/drop-off area.
3. Police Control is recommended at the King Street/Radford Street/School Bus Driveway intersection during the school PM Peak Hour. Warrants for a traffic signal are not met.
4. The forecasted queues in front of the school with the school expansion are anticipated to be approximately 300 feet. Queues can be accommodated if the driveway area between the parking garage and King Street is utilized. The forecasted queue on southbound Chinquapin Drive for the student pick-up/drop-off activity is anticipated to also be 300 feet and can be adequately accommodated. Existing parking restrictions along Chinquapin Drive need to be re-evaluated.
5. Additional pavement markings are recommended on Kenwood Avenue at King Street to help guide motorists through the intersection onto the site property.

**WARRANT 3
PEAK HOUR**

King Street
Major Road
Radford Street
Minor Road



Source: Manual on Uniform Traffic Control Devices (MUTCD)

Table 1
T.C Williams High School
Level of Service Summary (1,2)

Intersection	Operating Condition	Critical Movement	Existing 2003 Level of Service		Background 2007 ⁽³⁾ Level of Service		Total Future 2007 ⁽³⁾ Level of Service	
			AM Peak	PM Peak	AM Peak	PM Peak	AM Peak	PM Peak
King Street / Kenwood Avenue	Signalized	EB WB NB SB Overall	B (12.8)	B (12.6)	A (9.6)	B (13.2)	B (13.0)	B (12.9)
			B (14.6)	B (13.2)	B (11.1)	B (13.9)	C (33.5)	B (14.4)
			B (16.4)	B (14.8)	C (21.4)	B (14.8)	B (16.0)	B (15.5)
			B (17.3)	B (16.0)	C (26.9)	B (16.8)	B (18.4)	B (17.6)
			B (14.7)	B (13.5)	B (14.1)	B (14.1)	C (23.0)	B (14.4)
Braddock Road / Kenwood Avenue	Signalized	EB WB NB SB Overall	D (47.2)	B (16.3)	D (47.1)	B (18.4)	D (50.0)	B (18.5)
			C (32.5)	B (19.5)	C (28.1)	C (24.3)	C (23.3)	C (23.0)
			B (19.8)	B (17.0)	D (37.2)	B (19.1)	C (32.8)	B (19.8)
			B (15.5)	B (15.4)	B (19.1)	B (15.8)	B (19.2)	B (15.8)
			C (33.4)	B (17.4)	D (35.8)	C (20.2)	C (34.5)	B (19.8)
King Street / Chinquapin Drive One NB Lane <i>(With Improvements)</i> One NB Left Lane and One NB Right Lane	Unsignalized	WB LT NB LR WB LT NB L ⁽⁵⁾ NBR ⁽⁵⁾	A (9.6)	A (8.8)	B (10.4)	A (9.6)	A (9.6)	A (9.7)
			D (32.5)	C (19.6)	F (81.7)	F (101.1)	E (44.8)	F (53.0)
			NA	NA	NA	NA	A (9.6)	A (9.7)
			NA	NA	NA	NA	F (54.8)	F (55.2)
			NA	NA	NA	NA	B (10.3)	B (11.4)
King Street / West School Entrance / Radford Street	Unsignalized	EB LT WB LT NB LTR ⁽⁴⁾ SB LTR	A (8.0)	A (8.3)	A (8.2)	A (8.6)	A (8.2)	A (8.7)
			A (9.0)	A (8.5)	A (9.3)	A (8.9)	A (9.5)	A (8.9)
			B (14.1)	C (17.3)	C (15.7)	C (21.8)	D (25.5)	C (22.5)
			C (18.8)	C (16.5)	C (22.0)	C (20.1)	D (28.6)	C (20.3)

(1) Capacity analyses based on the updated Highway Capacity Manual software, 2000, Version 4.1c.

(2) Numbers in parenthesis represent average delay per vehicle in seconds.

(3) Signal cycle length and optimized timings for are the same for both Total Future and Background signalized intersection scenarios.

(4) Although the southbound approach is one-way (not clearly marked) into the school, exiting motorists utilized this intersection and therefore, the approach was analyzed.

(5) Overall northbound approach delay is 39.5 seconds during the AM peak and 35.7 seconds during the PM peak for total future conditions.

APPENDIX



WELLS & ASSOCIATES, LLC.
TRAFFIC, TRANSPORTATION, AND PARKING CONSULTANTS

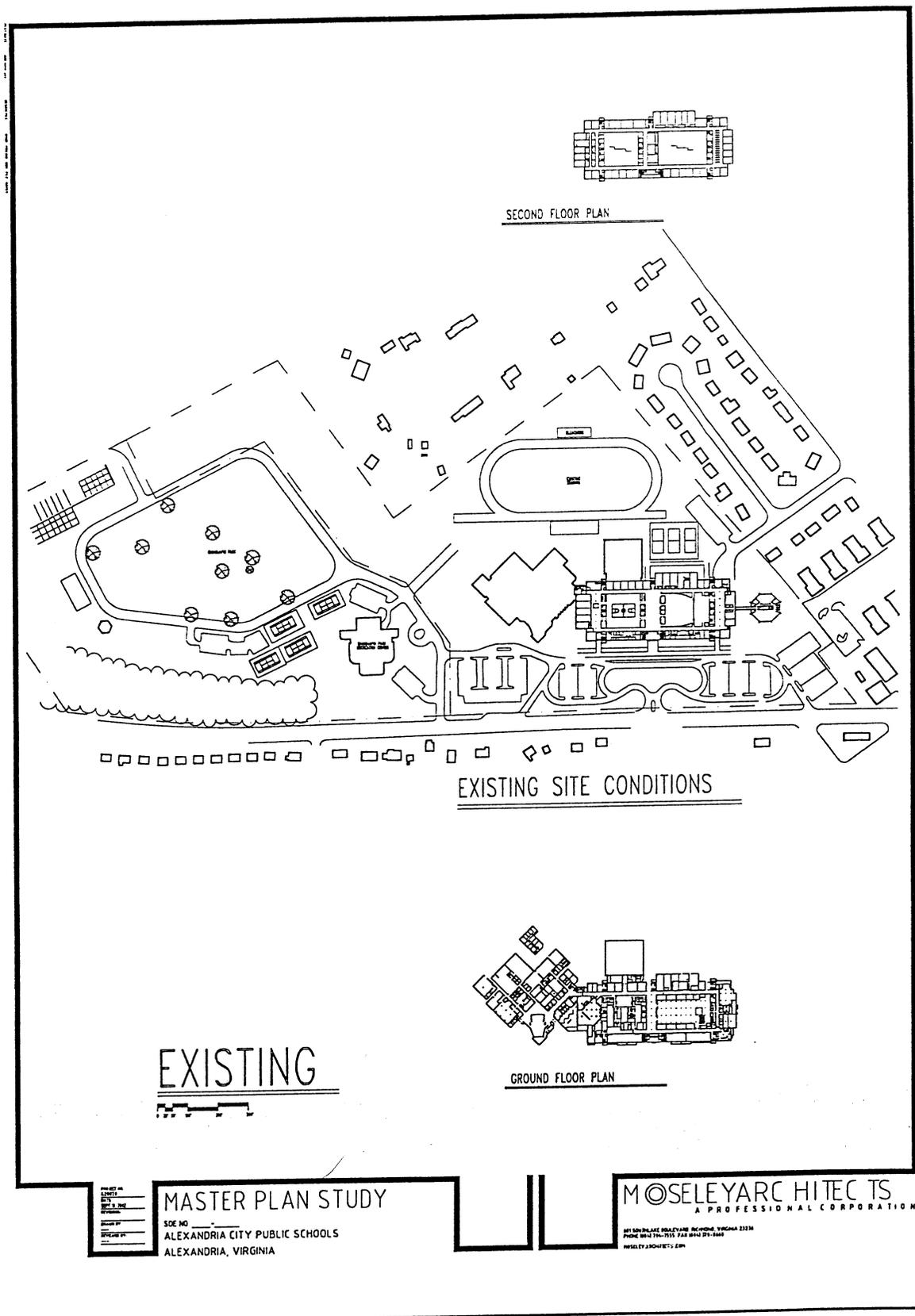


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Figure 1
Site Location Map

T.C. Williams High School
Alexandria, Virginia

O:\Projects\2001-2500\2032 TC Williams High School\Graphics\Fig_3.dwg - MJB



EXISTING

GROUND FLOOR PLAN

SECOND FLOOR PLAN

PROJECT NO. _____
 SHEET NO. _____
 DATE _____
 DRAWN BY _____
 CHECKED BY _____
 APPROVED BY _____

MASTER PLAN STUDY
 SDC NO. _____
 ALEXANDRIA CITY PUBLIC SCHOOLS
 ALEXANDRIA, VIRGINIA

MOSELEYARC HITEC TS
 A PROFESSIONAL CORPORATION

401 SANDHURST ROAD, SUITE 200, ALEXANDRIA, VIRGINIA 22304
 PHONE 703-755-7555 FAX 703-755-7555
 MOSELEYARCHITECTS.COM

Figure 2
Existing Site Plan

North
Schematic

D:\Projects\2001-2500\2039 TC Williams High School\Graphics\Fig_3.dwg M:IP

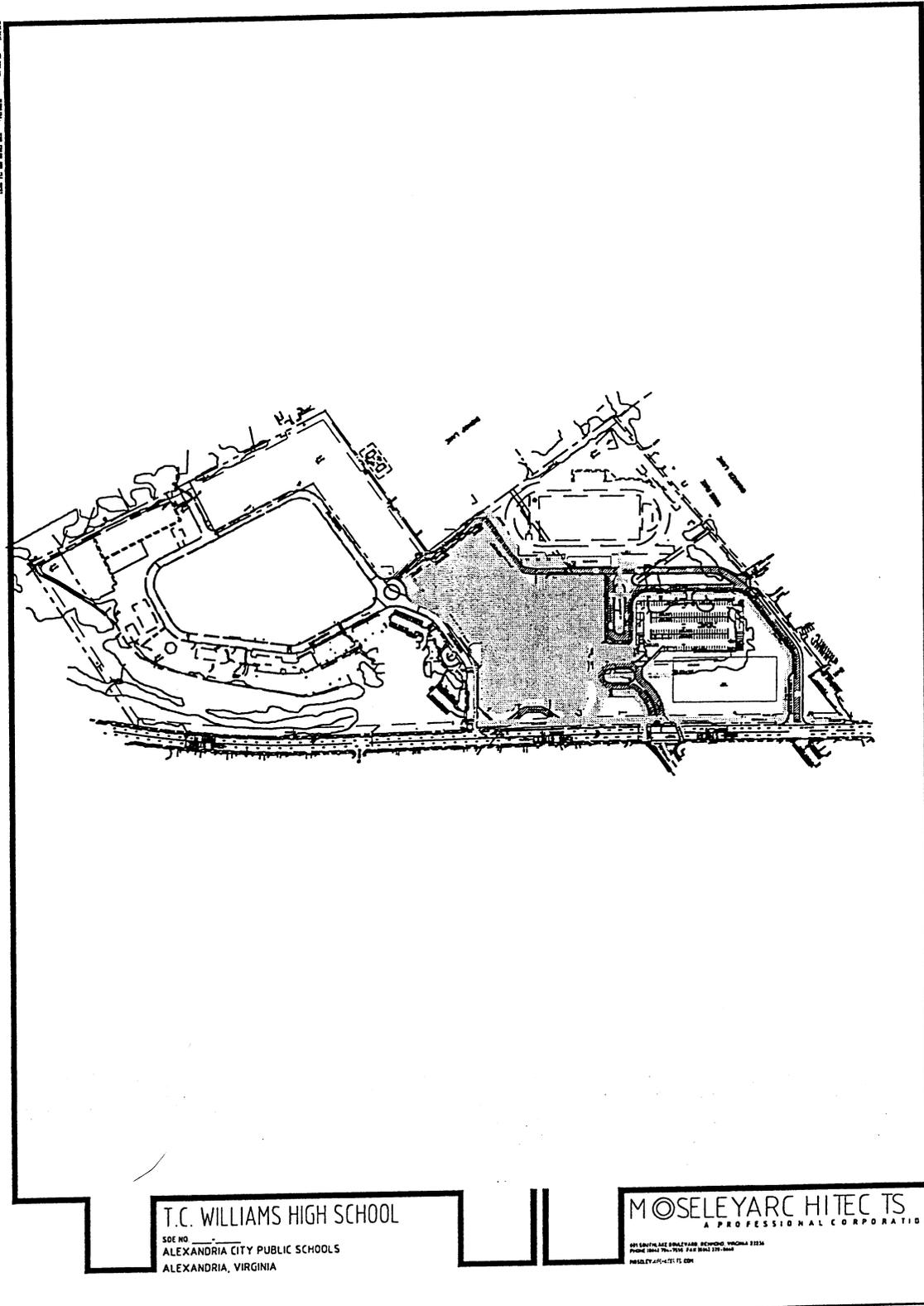
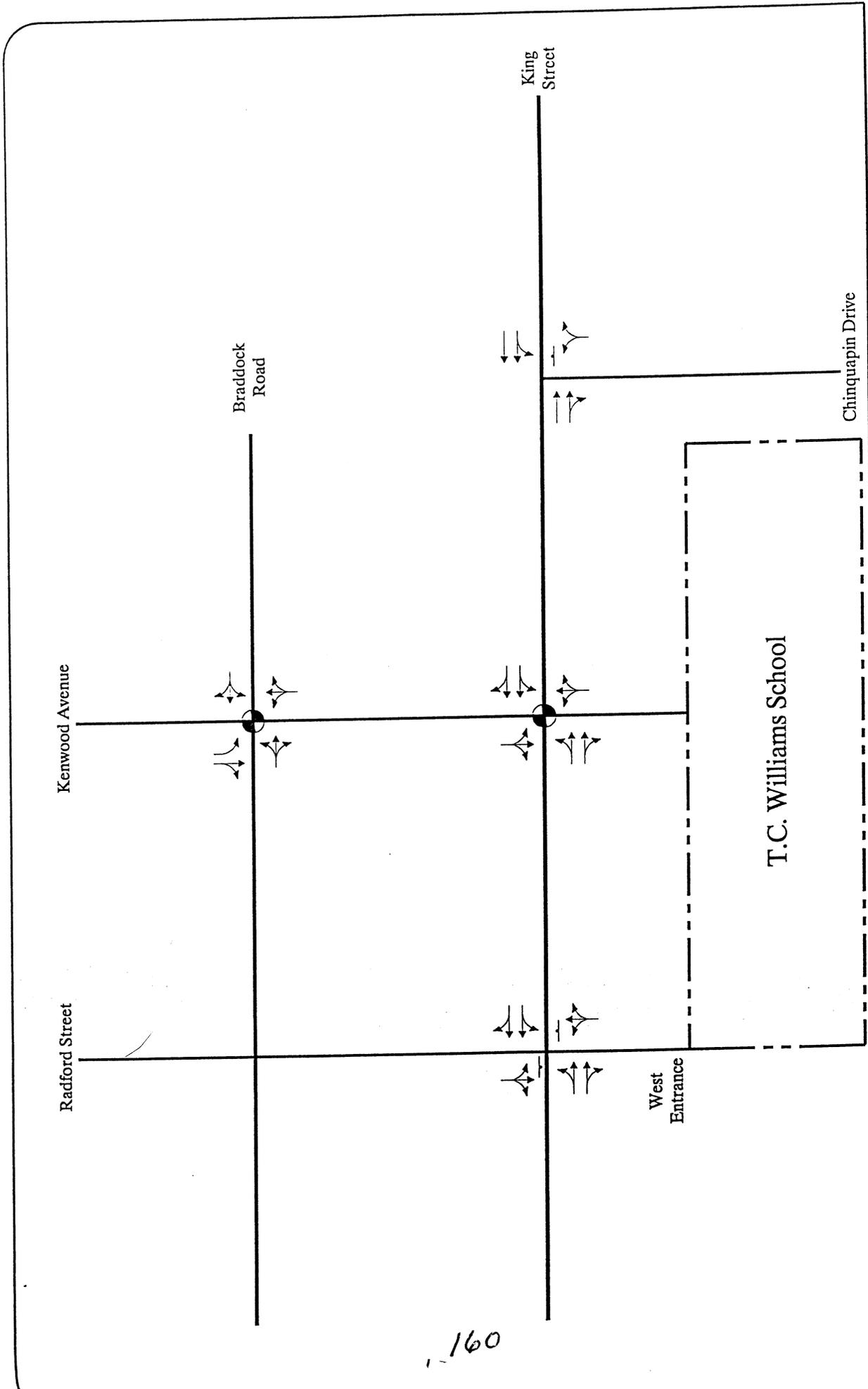


Figure 3 (REVISED)
Proposed Site Plan

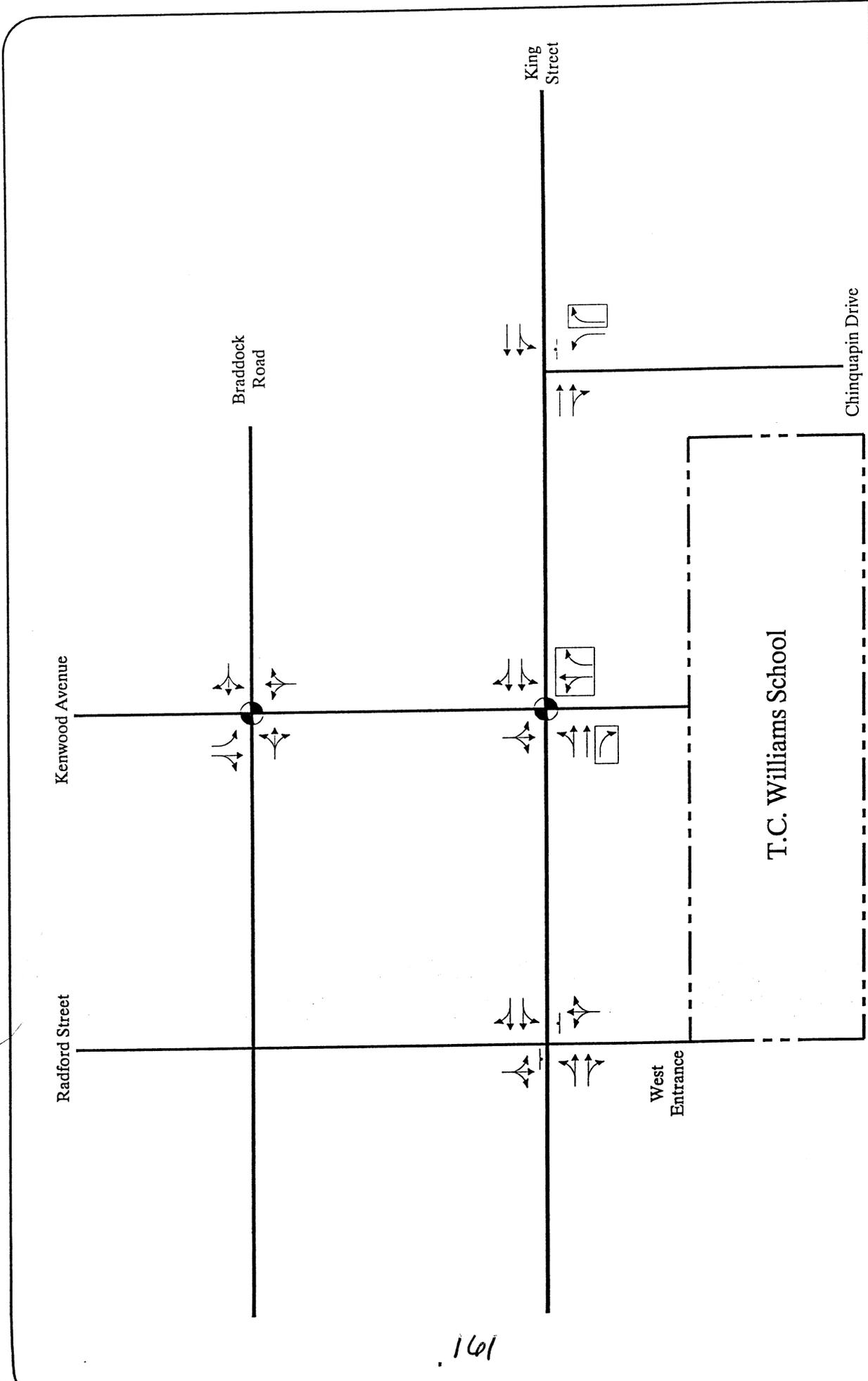
North
Schematic



160

← Represents One Travel Lane
 ● Signalized Intersection
 □ Stop Sign
 ← North Schematic

Figure 4
 Existing Lane Use and Traffic Control



161

- ← Represents One Travel Lane
- ⊗ Signalized Intersection
- Stop Sign

□ Proposed

Figure 5 (REVISED)
Proposed Lane Use and Traffic Control



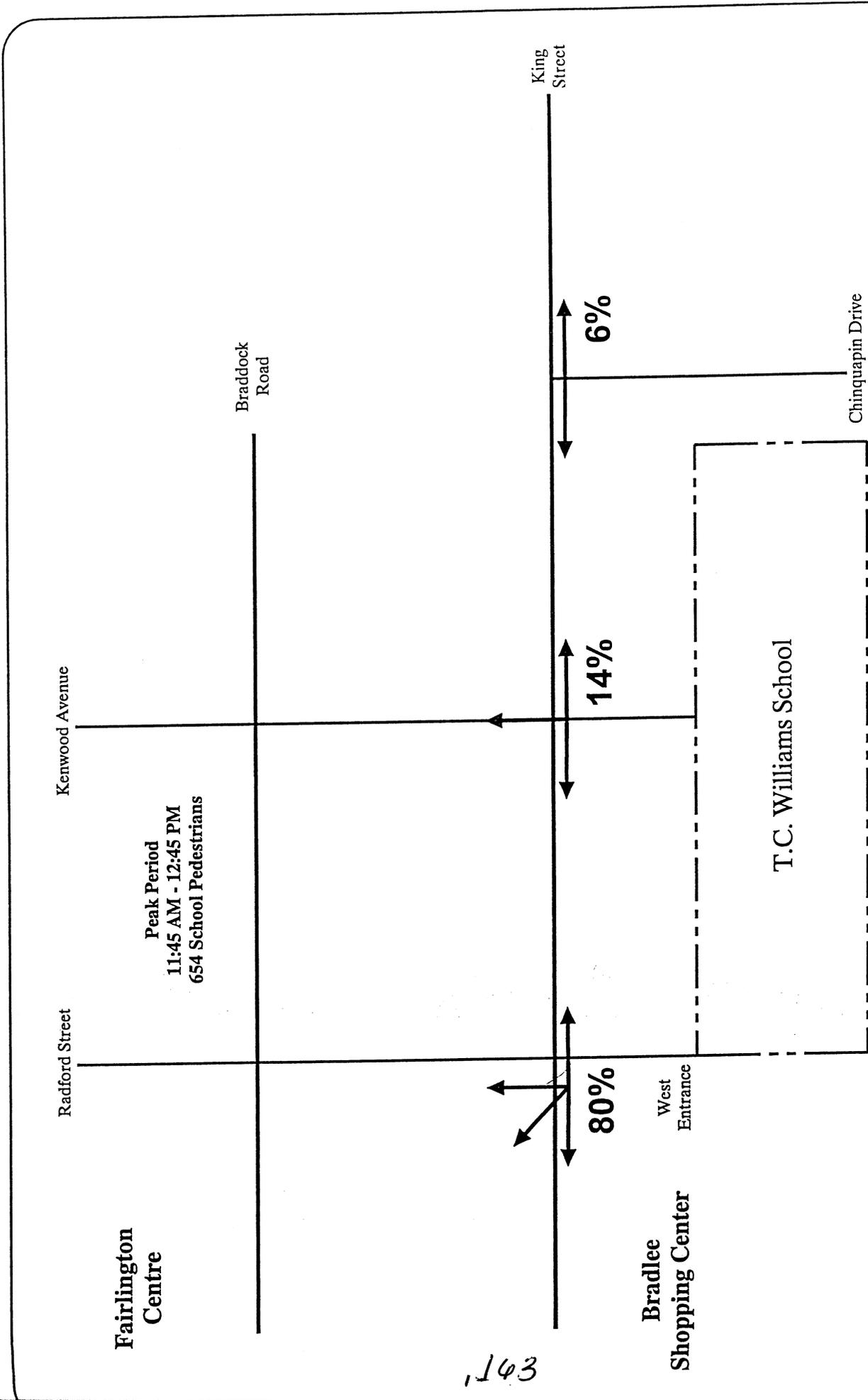
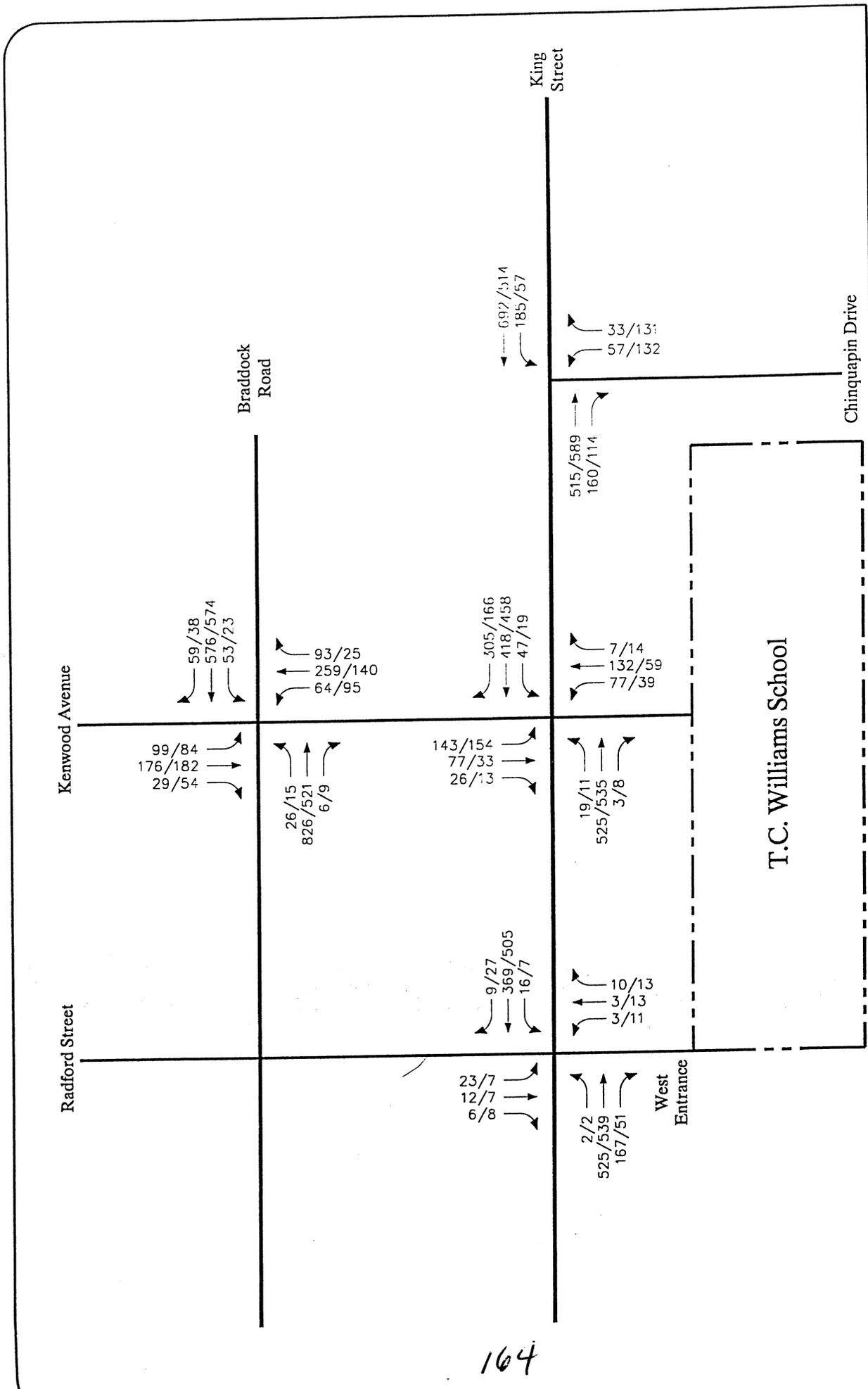


Figure 7
Pedestrian Movements



000/000
 PEAK HOUR
 SCHOOL PM
 AN PEAK HOUR

Figure 8
 Background Volumes in 2007

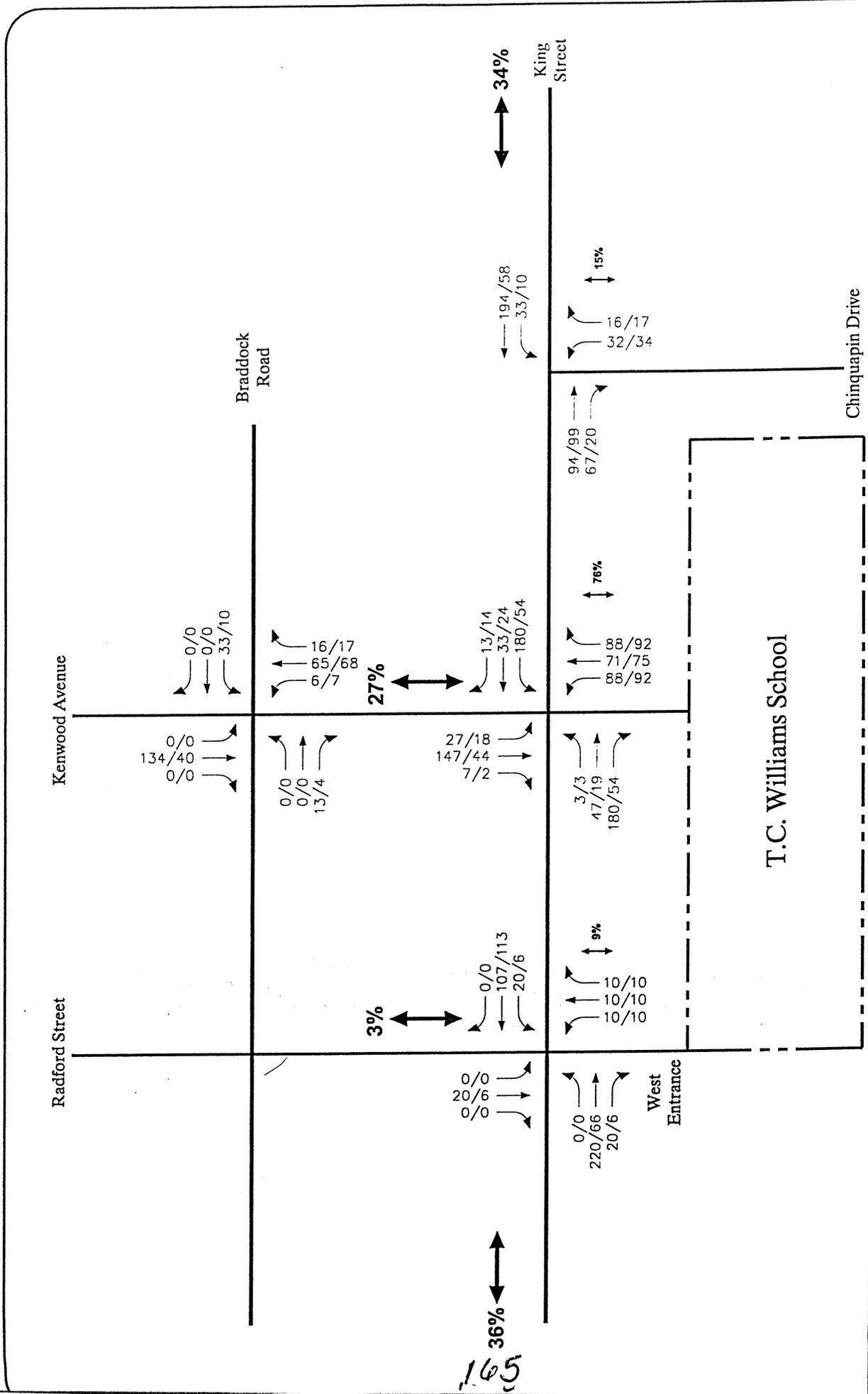
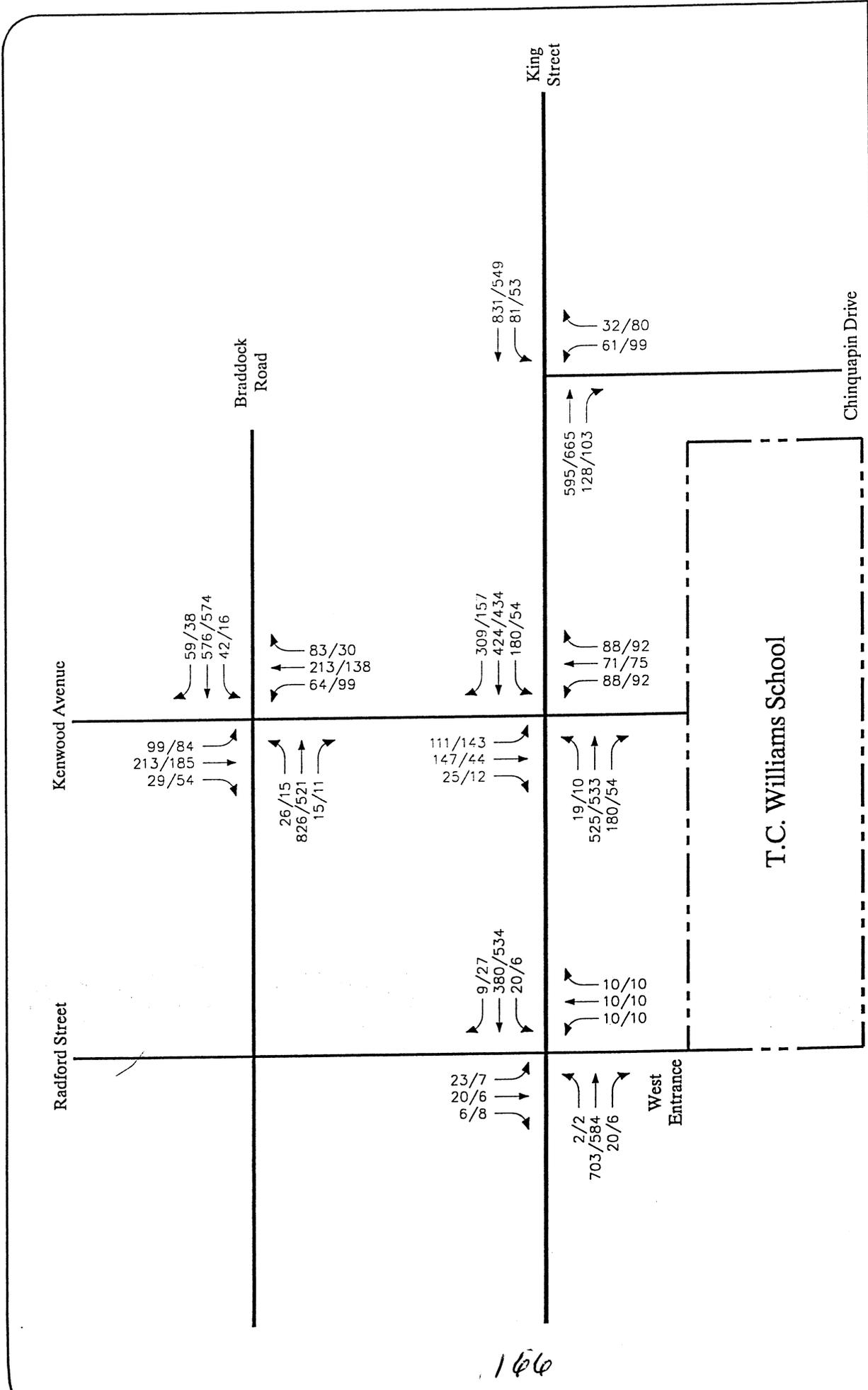


Figure 9 (REVISED)
 Site-Generated Traffic Volumes and
 Directional Distribution (Adjusted for New Site Layout)



AM PEAK HOUR
SCHOOL PM
PEAK HOUR
000/000

Figure 10 (REVISED)
Total Future Traffic Volumes



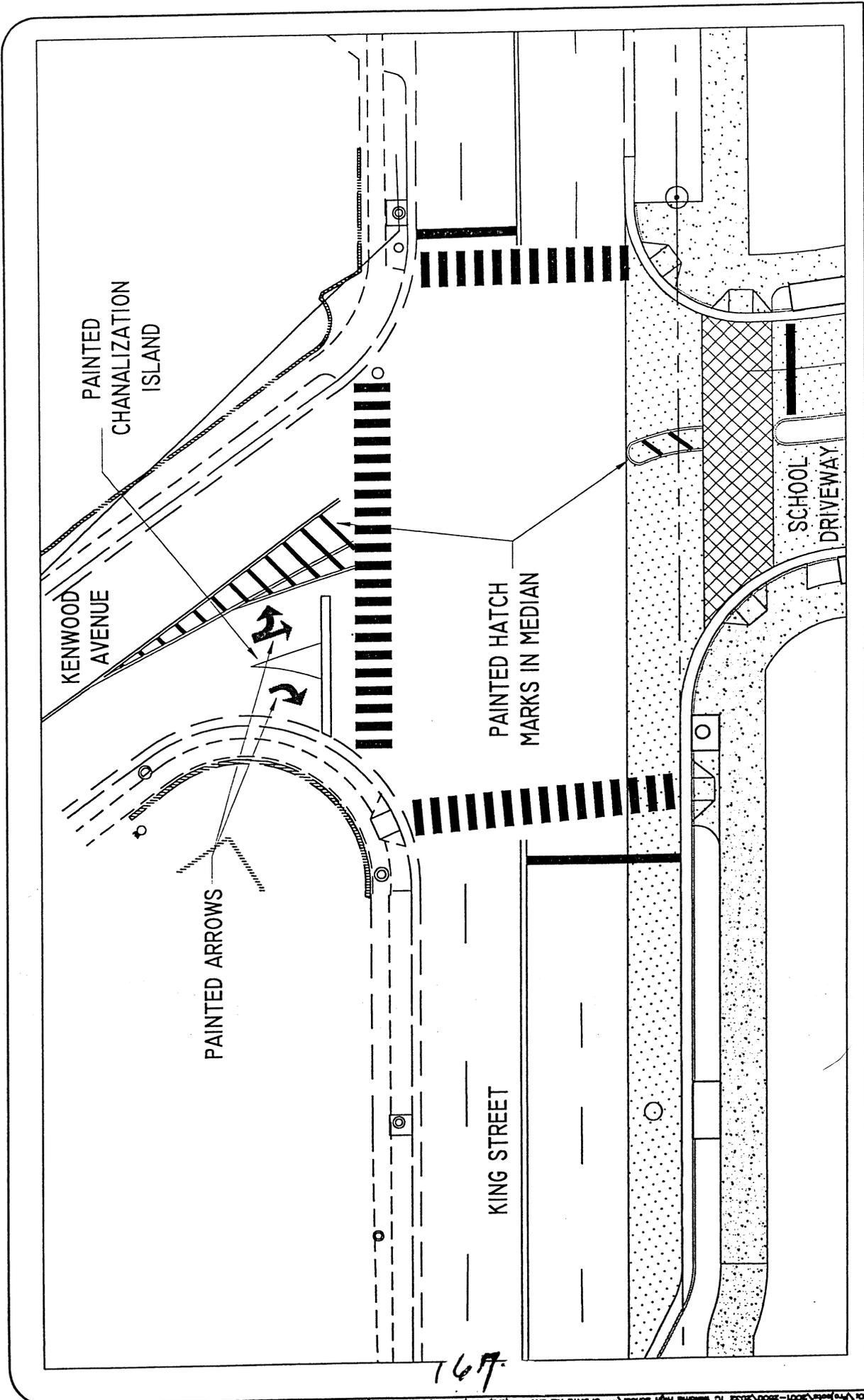


Figure 11
 Intersection Alignment Improvements
 Utilizing Pavement Marking

05/16/2017 10:00 AM - UPDATE REPORT - IMPROVING INTERSECTION TO IMPROVE HIGH SCHOOL - PROJECT NO. 167

HCS2000: Signalized Intersections Release 4.1d

Analyst: VLG
 Agency: Wells and Associates
 Date: 11/20/03
 Period: AM Peak
 Project ID: TC Williams High School
 E/W St: King Street

Inter.: King St / Kenwood
 Area Type: All other areas
 Jurisd: City of Alexandria
 Year : 2007 TF
 N/S St: Kenwood Ave

SIGNALIZED INTERSECTION SUMMARY

	Eastbound			Westbound			Northbound			Southbound		
	L	T	R	L	T	R	L	T	R	L	T	R
No. Lanes	0	2	1	0	2	0	0	1	1	0	1	0
LGConfig		LT	R		LTR			LT	R		LTR	
Volume	19	525	180	180	424	309	88	71	88	111	147	25
Lane Width		12.0	12.0		12.0			12.0	12.0		12.0	
RTOR Vol			0			0			88			0

Duration 0.25 Area Type: All other areas

Signal Operations

Phase Combination	1	2	3	4	5	6	7	8
EB Left	A				NB Left	A		
Thru	A				Thru	A		
Right	A				Right	A		
Peds	X				Peds	X		
WB Left	A				SB Left	A		
Thru	A				Thru	A		
Right	A				Right	A		
Peds	X				Peds	X		
NB Right					EB Right			
SB Right					WB Right			
Green	35.0				30.0			
Yellow	3.0				3.0			
All Red	2.0				2.0			

Cycle Length: 75.0 secs

Intersection Performance Summary

Appr/ Lane Grp	Lane Group Capacity	Adj Sat Flow Rate (s)	Ratios		Lane Group		Approach	
			v/c	g/C	Delay	LOS	Delay	LOS
Eastbound								
LT	1521	3259	0.40	0.47	13.3	B	13.0	B
R	754	1615	0.27	0.47	12.4	B		
Westbound								
LTR	1083	2321	0.94	0.47	33.5	C	33.5	C
Northbound								
LT	530	1325	0.33	0.40	16.0	B	16.0	B
R	644	1610	0.00	0.40	13.5	B		
Southbound								
LTR	573	1432	0.55	0.40	18.4	B	18.4	B

Intersection Delay = 23.0 (sec/veh) Intersection LOS = C

HCS2000: Signalized Intersections Release 4.1d

Analyst: VLG
 Agency: Wells and Associates
 Date: 11/21/03
 Period: PM Peak
 Project ID: TC Williams High School
 E/W St: King Street

Inter.: King St / Kenwood
 Area Type: All other areas
 Jurisd: City of Alexandria
 Year : 2007 TF
 N/S St: Kenwood Ave

SIGNALIZED INTERSECTION SUMMARY

	Eastbound			Westbound			Northbound			Southbound		
	L	T	R	L	T	R	L	T	R	L	T	R
No. Lanes	0	2	1	0	2	0	0	1	1	0	1	0
LGConfig	LT R			LTR			LT R			LTR		
Volume	10	533	54	54	434	157	92	75	92	143	44	12
Lane Width	12.0		12.0	12.0			12.0		12.0	12.0		
RTOR Vol	0			0			54			0		

Duration 0.25 Area Type: All other areas

Signal Operations

Phase Combination	1	2	3	4	5	6	7	8
EB Left	A				NB Left	A		
Thru	A				Thru	A		
Right	A				Right	A		
Peds	X				Peds	X		
WB Left	A				SB Left	A		
Thru	A				Thru	A		
Right	A				Right	A		
Peds	X				Peds	X		
NB Right					EB Right			
SB Right					WB Right			
Green	35.0				30.0			
Yellow	3.0				3.0			
All Red	2.0				2.0			

Cycle Length: 75.0 secs

Intersection Performance Summary

Appr/ Lane Grp	Lane Group Capacity	Adj Sat Flow Rate (s)	Ratios		Lane Group		Approach	
			v/c	g/C	Delay	LOS	Delay	LOS
Eastbound								
LT	1584	3395	0.38	0.47	13.1	B	12.9	B
R	751	1609	0.08	0.47	11.1	B		
Westbound								
LTR	1383	2963	0.52	0.47	14.4	B	14.4	B
Northbound								
LT	563	1408	0.33	0.40	15.9	B	15.5	B
R	636	1591	0.07	0.40	13.9	B		
Southbound								
LTR	452	1130	0.49	0.40	17.6	B	17.6	B

Intersection Delay = 14.4 (sec/veh) Intersection LOS = B

HCS2000: Unsignalized Intersections Release 4.1d

TWO-WAY STOP CONTROL SUMMARY

Analyst: VLG
 Agency/Co.: Wells and Associates
 Date Performed: 11/21/03
 Analysis Time Period: PM Peak
 Intersection: Chinguopin Drive and King St
 Jurisdiction: Alexandria, VA
 Units: U. S. Customary
 Analysis Year: 2007 TF
 Project ID: 2032 - TC Williams HS
 East/West Street: King Street
 North/South Street: Chinguopin Drive
 Intersection Orientation: EW
 Study period (hrs): 0.25

Major Street: Approach Movement	Eastbound			Westbound		
	1 L	2 T	3 R	4 L	5 T	6 R
Volume		655	103	53	549	
Peak-Hour Factor, PHF		0.90	0.90	0.90	0.90	
Hourly Flow Rate, HFR		738	114	58	610	
Percent Heavy Vehicles		--	--	0	--	--
Median Type/Storage		Undivided			/	
RT Channelized?						
Lanes		2	0		0	2
Configuration		T	TR		LT	T
Upstream Signal?		Yes			No	

Minor Street: Approach Movement	Northbound			Southbound		
	7 L	8 T	9 R	10 L	11 T	12 R
Volume	99		80			
Peak Hour Factor, PHF	0.90		0.90			
Hourly Flow Rate, HFR	110		88			
Percent Heavy Vehicles	0		0		0	
Percent Grade (%)		0				
Flared Approach: Exists?/Storage			No	/		/
Lanes	0		0			
Configuration		LR				

Approach Movement	Delay, Queue Length, and Level of Service							
	EB	WB	Northbound			Southbound		
Lane Config	1	4 LT	7	8 LR	9	10	11	12
v (vph)		58		198				
C(m) (vph)		821		259				
v/c		0.07		0.76				
95% queue length		0.23		5.61				
Control Delay		9.7		53.0				
LOS		A		F				
Approach Delay				53.0				
Approach LOS				F				

HCS2000: Unsignalized Intersections Release 4.1d

TWO-WAY STOP CONTROL SUMMARY

Analyst: VLG
 Agency/Co.: Wells and Associates
 Date Performed: 11/21/03
 Analysis Time Period: PM Peak
 Intersection: Chinguapin Drive and King St
 Jurisdiction: Alexandria, VA
 Units: U. S. Customary
 Analysis Year: 2007 TF (Two NB Lanes)
 Project ID: 2032 - TC Williams HS
 East/West Street: King Street
 North/South Street: Chinguapin Drive
 Intersection Orientation: EW
 Study period (hrs): 0.25

Vehicle Volumes and Adjustments							
Major Street:	Approach Movement	Eastbound			Westbound		
		1 L	2 T	3 R	4 L	5 T	6 R
Volume		665	103		53	549	
Peak-Hour Factor, PHF		0.90	0.90		0.90	0.90	
Hourly Flow Rate, HFR		738	114		58	610	
Percent Heavy Vehicles		--	--		0	--	--
Median Type/Storage		Undivided			/		
RT Channelized?							
Lanes		2	0		0	2	
Configuration		T	TR		LT	T	
Upstream Signal?		Yes			No		

Minor Street:	Approach Movement	Northbound			Southbound		
		7 L	8 T	9 R	10 L	11 T	12 R
Volume		99		80			
Peak Hour Factor, PHF		0.90		0.90			
Hourly Flow Rate, HFR		110		88			
Percent Heavy Vehicles		0		0		0	
Percent Grade (%)			0		/		/
Flared Approach: Exists?/Storage		1		1			
Lanes		L		R			
Configuration							

Approach Movement	Delay, Queue Length, and Level of Service								
	EB	Northbound			Southbound				
Lane Config	1	4 LT	7 L	8	9 R	10	11	12	
v (vph)	58	110			88				
C(m) (vph)	821	175			654				
v/c	0.07	0.63			0.13				
95% queue length	0.23	3.54			0.46				
Control Delay	9.7	55.2			11.4				
LOS	A	F			B				
Approach Delay				35.7					
Approach LOS				E					

HCS2000: Unsignalized Intersections Release 4.1d

TWO-WAY STOP CONTROL SUMMARY

Analyst: VLG
 Agency/Co.: Wells and Associates
 Date Performed: 11/21/03
 Analysis Time Period: AM Peak
 Intersection: Chinguapin Drive and King St
 Jurisdiction: Alexandria, VA
 Units: U. S. Customary
 Analysis Year: 2007 TF
 Project ID: 2032 - TC Williams HS
 East/West Street: King Street
 North/South Street: Chinguapin Drive
 Intersection Orientation: EW
 Study period (hrs): 0.25

Vehicle Volumes and Adjustments						
Major Street:	Approach Movement	Eastbound			Westbound	
		1 L	2 T	3 R	4 L	5 T
Volume		595	128		81	831
Peak-Hour Factor, PHF		0.90	0.90		0.90	0.90
Hourly Flow Rate, HFR		661	142		90	923
Percent Heavy Vehicles		--	--		0	--
Median Type/Storage		Undivided			/	
RT Channelized?						
Lanes		2	0		0	2
Configuration		T	TR		LT	T
Upstream Signal?		Yes			No	

Minor Street:	Approach Movement	Northbound			Southbound		
		7 L	8 T	9 R	10 L	11 T	12 R
Volume		61		32			
Peak Hour Factor, PHF		0.90		0.90			
Hourly Flow Rate, HFR		67		35			
Percent Heavy Vehicles		0		0		0	
Percent Grade (%)			0			0	
Flared Approach: Exists?/Storage			0	No	/	/	
Lanes		0		0			
Configuration			LR				

Delay, Queue Length, and Level of Service							
Approach Movement	EB	WB	Northbound			Southbound	
			7	8	9	10	11
Lane Config	1	4	LT	LR			
v (vph)		90		102			
C(m) (vph)		875		188			
v/c		0.10		0.54			
95% queue length		0.34		2.82			
Control Delay		9.6		44.8			
LOS		A		E			
Approach Delay				44.8			
Approach LOS				E			

HCS2000: Unsignalized Intersections Release 4.1d

TWO-WAY STOP CONTROL SUMMARY

Analyst: VLG
 Agency/Co.: Wells and Associates
 Date Performed: 11/21/03
 Analysis Time Period: AM Peak
 Intersection: Chinguopin Drive and King St
 Jurisdiction: Alexandria, VA
 Units: U. S. Customary
 Analysis Year: 2007 TF (Two NB Lanes)
 Project ID: 2032 - TC Williams HS
 East/West Street: King Street
 North/South Street: Chinguopin Drive
 Intersection Orientation: EW Study period (hrs): 0.25

Vehicle Volumes and Adjustments

Major Street: Approach Movement	Eastbound			Westbound		
	1 L	2 T	3 R	4 L	5 T	6 R
Volume		595	128	81	831	
Peak-Hour Factor, PHF		0.90	0.90	0.90	0.90	
Hourly Flow Rate, HFR		661	142	90	923	
Percent Heavy Vehicles		--	--	0	--	--
Median Type/Storage		Undivided		/		
RT Channelized?						
Lanes		2	0	0	2	
Configuration		T	TR		LT T	
Upstream Signal?		Yes			No	

Minor Street: Approach Movement	Northbound			Southbound		
	7 L	8 T	9 R	10 L	11 T	12 R
Volume	61		32			
Peak Hour Factor, PHF	0.90		0.90			
Hourly Flow Rate, HFR	67		35			
Percent Heavy Vehicles	0		0			
Percent Grade (%)		0			0	
Flared Approach: Exists?/Storage				/		/
Lanes	1		1			
Configuration		L	R			

Delay, Queue Length, and Level of Service

Approach Movement	EB	WB	Northbound			Southbound		
	1	4	7	8	9	10	11	12
Lane Config		LT	L		R			
v (vph)	90	67			35			
C(m) (vph)	875	136			718			
v/c	0.10	0.49			0.05			
95% queue length	0.34	2.30			0.15			
Control Delay	9.6	54.8			10.3			
LOS	A	F			B			
Approach Delay				39.5				
Approach LOS				E				

HCS2000: Unsignalized Intersections Release 4.1d

TWO-WAY STOP CONTROL SUMMARY

Analyst: VLG
 Agency/Co.: Wells and Associates
 Date Performed: 11/21/03
 Analysis Time Period: AM Peak
 Intersection: Radford and King St
 Jurisdiction: Alexandria, VA
 Units: U. S. Customary
 Analysis Year: 2007 TF
 Project ID: 2032 - TC Williams HS
 East/West Street: King Street
 North/South Street: Radford / West School Entrance
 Intersection Orientation: EW Study period (hrs): 0.25

Vehicle Volumes and Adjustments						
Major Street: Approach Movement	Eastbound			Westbound		
	1 L	2 T	3 R	4 L	5 T	6 R
Volume	2	703	20	20	380	9
Peak-Hour Factor, PHF	0.90	0.90	0.90	0.90	0.90	0.90
Hourly Flow Rate, HFR	2	781	22	22	422	10
Percent Heavy Vehicles	0	--	--	0	--	--
Median Type/Storage	Undivided			/		
RT Channelized?						
Lanes	0	2	0	0	2	0
Configuration	LT TR			LT TR		
Upstream Signal?	No			Yes		

Minor Street: Approach Movement	Northbound			Southbound		
	7 L	8 T	9 R	10 L	11 T	12 R
Volume	10	10	10	23	20	6
Peak Hour Factor, PHF	0.90	0.90	0.90	0.90	0.90	0.90
Hourly Flow Rate, HFR	11	11	11	25	22	6
Percent Heavy Vehicles	0	0	0	0	0	0
Percent Grade (%)	0			0		
Flared Approach: Exists?/Storage	No			/		
Lanes	0	1	0	0	1	0
Configuration	LTR			LTR		

Delay, Queue Length, and Level of Service							
Approach Movement Lane Config	EB	WB	Northbound			Southbound	
	1 LT	4 LT	7 	8 LTR	9 	10 	11 LTR
v (vph)	2	22		33			53
C(m) (vph)	1132	821		208			205
v/c	0.00	0.03		0.16			0.26
95% queue length	0.01	0.08		0.55			0.99
Control Delay	8.2	9.5		25.5			28.6
LOS	A	A		D			D
Approach Delay				25.5			28.6
Approach LOS				D			D

HCS2000: Unsignalized Intersections Release 4.1d

TWO-WAY STOP CONTROL SUMMARY

Analyst: VLG
 Agency/Co.: Wells and Associates
 Date Performed: 11/21/03
 Analysis Time Period: PM Peak
 Intersection: Radford and King St
 Jurisdiction: Alexandria, VA
 Units: U. S. Customary
 Analysis Year: 2007 TF
 Project ID: 2032 - TC Williams HS
 East/West Street: King Street
 North/South Street: Radford / West School Entrance
 Intersection Orientation: EW Study period (hrs): 0.25

Major Street:	Approach Movement	Vehicle Volumes and Adjustments					
		Eastbound			Westbound		
		1	2	3	4	5	6
		L	T	R	L	T	R
Volume		2	584	6	6	534	27
Peak-Hour Factor, PHF		0.90	0.90	0.90	0.90	0.90	0.90
Hourly Flow Rate, HFR		2	648	6	6	593	30
Percent Heavy Vehicles		0	--	--	0	--	--
Median Type/Storage		Undivided			/		
RT Channelized?							
Lanes		0	2	0	0	2	0
Configuration		LT TR			LT TR		
Upstream Signal?		No			Yes		

Minor Street:	Approach Movement	Southbound					
		Northbound			Southbound		
		7	8	9	10	11	12
		L	T	R	L	T	R
Volume		10	10	10	7	6	8
Peak Hour Factor, PHF		0.90	0.90	0.90	0.90	0.90	0.90
Hourly Flow Rate, HFR		11	11	11	7	6	8
Percent Heavy Vehicles		0	0	0	0	0	0
Percent Grade (%)		0			/		
Flared Approach: Exists?/Storage		No			/		
Lanes		0	1	0	0	1	0
Configuration		LTR			LTR		

Approach Movement	Delay, Queue Length, and Level of Service							
	EB	WB	Northbound			Southbound		
	1	4	7	8	9	10	11	12
Lane Config	LT	LT		LTR			LTR	
v (vph)	2	6		33			21	
C(m) (vph)	978	941		239			257	
v/c	0.00	0.01		0.14			0.08	
95% queue length	0.01	0.02		0.47			0.26	
Control Delay	8.7	8.9		22.5			20.3	
LOS	A	A		C			C	
Approach Delay				22.5			20.3	
Approach LOS				C			C	

Learning to Live • Loving to Learn



Alexandria City Public Schools

December 19, 2003

Seminary Hill Civic Association
Task Force
Alexandria, VA

2000 N. Beauregard Street
Alexandria, Virginia 22311

Re: T.C. Williams High School

Telephone: (703) 824-6600
Facsimile: (703) 824-6699
TDD: (703) 824-6666
www.acps.k12.va.us

Ladies and Gentlemen:

This letter is in response to the memorandum dated November 23, 2003, provided by John Salmon on behalf of the Task Force, and the meeting at City Hall on November 17, 2003, held to discuss the new T.C. Williams High School construction project. Attending the meeting were members of the planning staff, representatives of the Alexandria City Public Schools ("ACPS") and members of the Task Force, consisting of neighbors from the Bishop Lane and the Woods Avenue areas.

Superintendent of Schools
Rebecca L. Perry

At the meeting, it was agreed to divide the issues presented by the neighborhood representatives into two categories: (1) issues appropriate to be addressed in the special use permit and site plan process; and (2) operational principles appropriate for an understanding between ACPS and the adjacent neighborhoods. At the meeting, it was agreed that the planning staff would include the City Department of Parks, Recreation and Cultural Activities ("DPRCA") in the process.

School Board

Chairman
Mark O. Wilkoff

Vice Chairman
Gwendolyn H. Lewis

Members
Sally Ann Baynard

Mary M. Danforth

Mark R. Eaton

Kenneth L. Foran

Melissa W. Luby

Arthur E. Schmalz

Charles H. Wilson

We agree that issues specified as site plan conditions in the November 23 memorandum, including stadium lighting, the advisory committee, buffer zone planting generally, outdoor lighting, sound, and the Woods Avenue entrance, are to be addressed as site plan conditions. As you are aware, these conditions will be drafted by the City planning staff and provided to both the ACPS and to the public in a staff report.

Operational issues are a matter between the ACPS, including the administration of T.C. Williams High School, the City and the neighborhood. It is our hope that the dialogue begun in the planning and construction process will serve as a basis for an on-going relationship between the high school and the neighborhood. Many of the issues that we are now discussing will of necessity be considered on a continuing basis in the context of an advisory committee to be established. At this time, however, we would like to respond to the comments made in the November 23 letter regarding various operational issues. Our response is as follows:

1. Stadium sound system design. ACPS will employ sound engineers and/or acoustic consultants to evaluate the current stadium system and make the necessary adjustments to improve the sound system and minimize the impact of the sound on the adjacent neighborhood. The acoustic consultants' evaluation and the proposed adjustments will be discussed with the advisory committee prior to installation. The system will have controls to limit the volume to reasonable levels. Such controls will have a locking device to prevent unauthorized use.
2. Stadium sound system operation. The stadium sound system will be used only for sanctioned school, school related and DPRCA events. Use of the stadium sound system will not occur prior to 9:00 a.m.; except for Saturdays when the public school track meets are held; and except for the Hershey Track Meet held by the DPRCA. Although ACPS does not anticipate using the stadium on Sundays or holidays, ACPS and DPRCA may wish to use the stadium on Sundays on an occasional basis as a backup site when other fields are not available, but not on a regular schedule.
3. School public address system. The new school building public address system for external use will be designed, installed and used in such a manner that daily announcements, etc., will not be broadcast outside the school building except for emergencies or emergency preparedness.
4. Access to athletic facilities. Use of the stadium, its track and the practice fields shall be limited to school academic and athletic events and those events sponsored by the DPRCA. Outside groups will only be permitted to use these facilities on an occasional basis under special circumstances, and prior to allowing any outside group to use these facilities, ACPS will notify the advisory committee and discuss the arrangements if requested by the committee.
5. Stadium operational hours. Regular use of the stadium for school and DPRCA events will be restricted to the hours of 7:30 a.m. until sunset, Monday through Saturday, and 10:00 a.m. to sunset on Sundays and holidays and will be used on Sundays and holidays only on an occasional basis.
6. Practice fields operational hours. Use of the practice fields for school and DPRCA events will be limited to the hours of 7:30 a.m. until sunset, Monday through Saturday, and 9:00 a.m. until sunset Sunday and holidays. Use of the proposed King Street practice field for any event on Sunday will be restricted to the hours of 1:00 p.m. to sunset.
7. School deliveries. Regular deliveries and pick-ups from commercial trucks, school food service and trash removal vehicles shall generally occur between the hours of 7:00 a.m. and 5:00 p.m., Monday through Friday, and 9:00 a.m. to 5:00 p.m., weekends and holidays. Food deliveries may occur between the hours of 6:00 a.m. and 7:00 a.m. on a limited basis.

8. Bus idling. All buses (or other commercial, diesel-powered vehicles) shall be required to turn off their engines if they are idling for more than 5 minutes before moving or leaving school property.
9. Parking. The school parking garage shall be available during after school hours and Saturdays and Sundays (with bus lanes also available on Sundays only) to accommodate parking for activities at the neighboring church; however, such parking shall not be reserved for the exclusive use of the church nor will ACPS assume any responsibility for monitoring and security. ACPS will also anticipate release of liability for damage to property or personal injury occurring in the parking garage or in the bus lane.

It is our hope that the discussions regarding T.C. Williams High School operations and the statements made in this letter constitute the basis of a continuing relationship between representatives of the neighboring community and school officials. We believe the establishment of an advisory committee of ACPS staff, DPRCA staff and neighbors will be an important aspect of the relationship. There will be changes in circumstances over time which may require new arrangements, and a continuing dialogue within the advisory committee will be an important part of maintaining good relations between the school and the neighborhood. We look forward to continuing to work with you.

Yours truly,

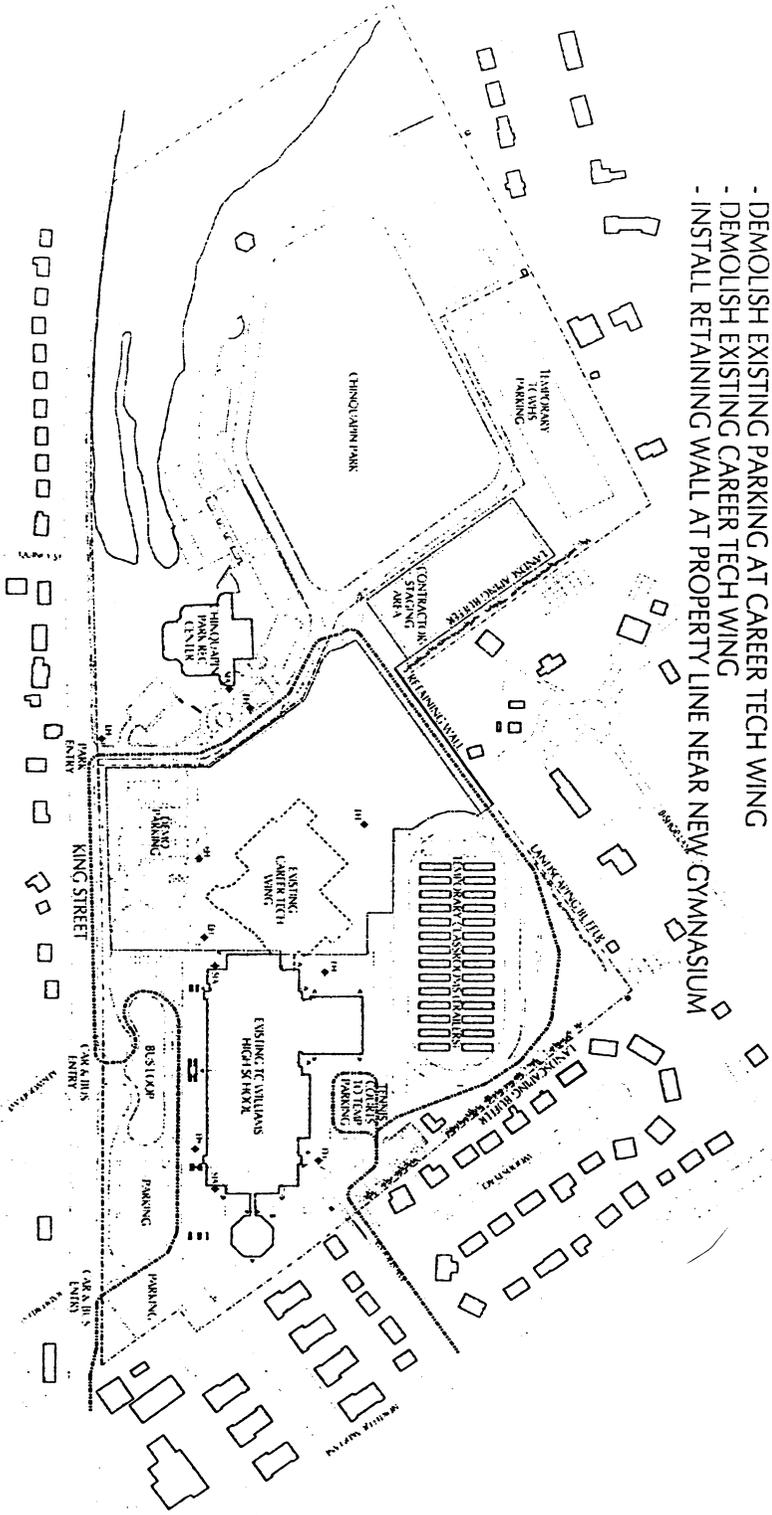


Rebecca L. Perry
Superintendent

cc: Barbara Ross
J. Howard Middleton

CONSTRUCTION ACTIVITIES:

- ESTABLISH CONSTRUCTION STAGING AREA
- BUILD TEMPORARY TC WILLIAMS PARKING LOT
- INSTALL LANDSCAPING BUFFERS AT ADJACENT NEIGHBORHOODS
- INSTALL CONSTRUCTION FENCING
- RELOCATE EXISTING TEMPORARY CLASSROOMS (TRAILERS)
- INSTALL TEMPORARY CLASSROOMS (TRAILERS)
- CONVERT TENNIS COURTS TO TEMPORARY PARKING LOT
- RELOCATE LARGE STORM SEWER AT KING STREET
- DEMOLISH EXISTING PARKING AT CAREER TECH WING
- DEMOLISH EXISTING CAREER TECH WING
- INSTALL RETAINING WALL AT PROPERTY LINE NEAR NEW GYMNASIUM



PHASE 1
JUNE 2004 - AUGUST 2004

--- CONST FENCE
 - - - - - TEMP EVE
 - - - - - BUILDING EXIT

TC WILLIAMS HS
 CONTRACTOR
 SIA ♦ SIAMESE CONNECTION
 EH ♦ EXISTING HYDRANT
 FH ♦ FIRE HYDRANT

PH1

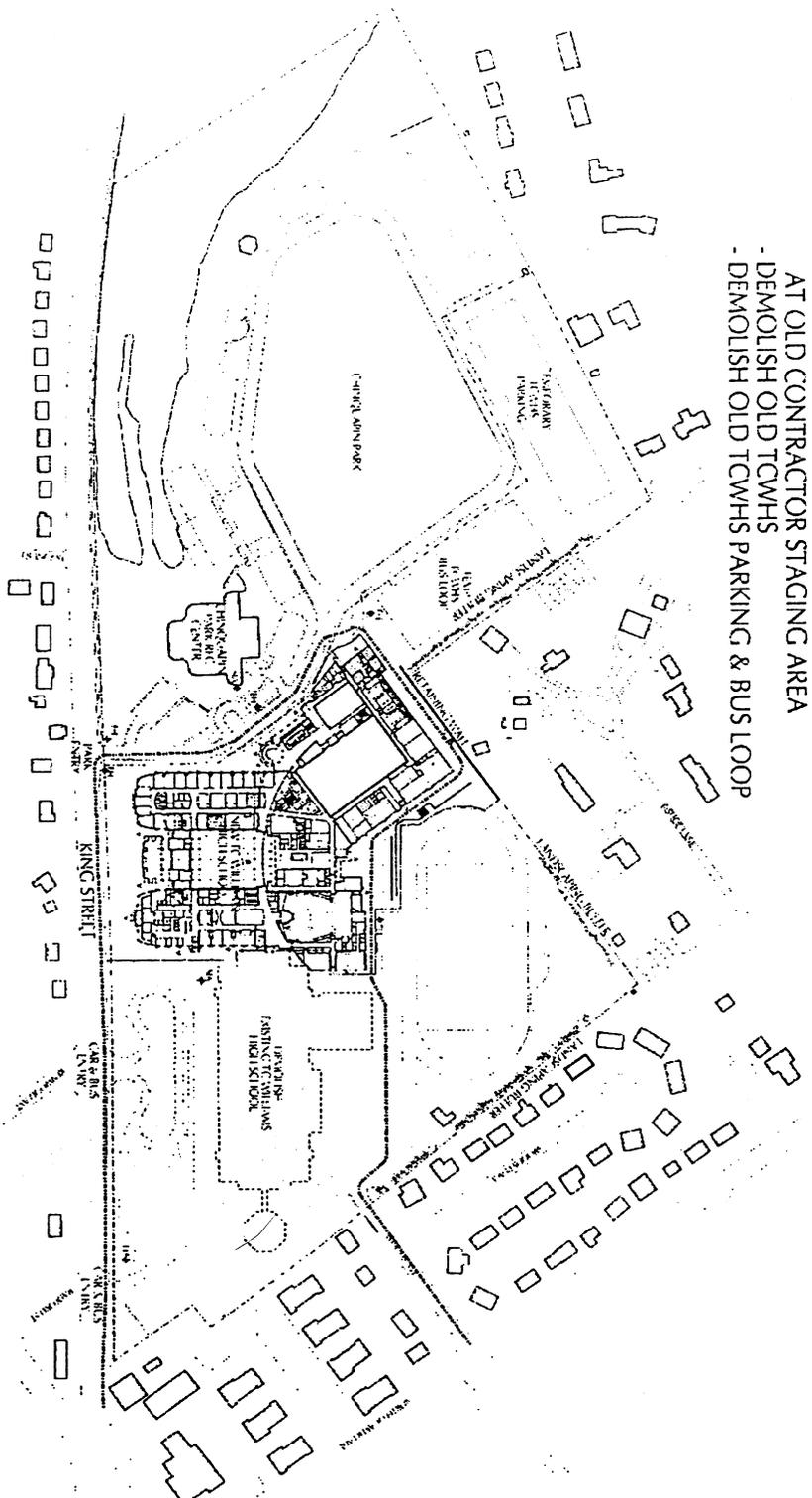
T.C. WILLIAMS HIGH SCHOOL
 SDE NO 101 210
 ALEXANDRIA CITY PUBLIC SCHOOLS
 ALEXANDRIA, VIRGINIA

179

ARCHITECTS
 A PROFESSIONAL CORPORATION

CONSTRUCTION ACTIVITIES:

- MOVE FROM OLD TCWHS TO NEW TCWHS
- REMOVE TRAILERS (TEMPORARY CLASSROOMS)
- REPAIR TRACK & FOOTBALL FIELD
- MOVE CONTRACTOR STAGING AREA
- PROVIDE ADDITIONAL TEMPORARY TCWHS BUS LOOP
- DEMOLISH OLD TCWHS
- DEMOLISH OLD TCWHS PARKING & BUS LOOP



PHASE 3
AUGUST 2006-

- CONST FENCE
- TEMP EVE
- BUILDING EXIT

- TC WILLIAMS HS
- CONTRACTOR
- SIA \blacklozenge SIAMESE CONNECTION
- EH \blacklozenge EXISTING HYDRANT
- FH \blacklozenge FIRE HYDRANT

T.C. WILLIAMS HIGH SCHOOL
SCE NO 10' 21D
ALEXANDRIA CITY PUBLIC SCHOOLS
ALEXANDRIA, VIRGINIA

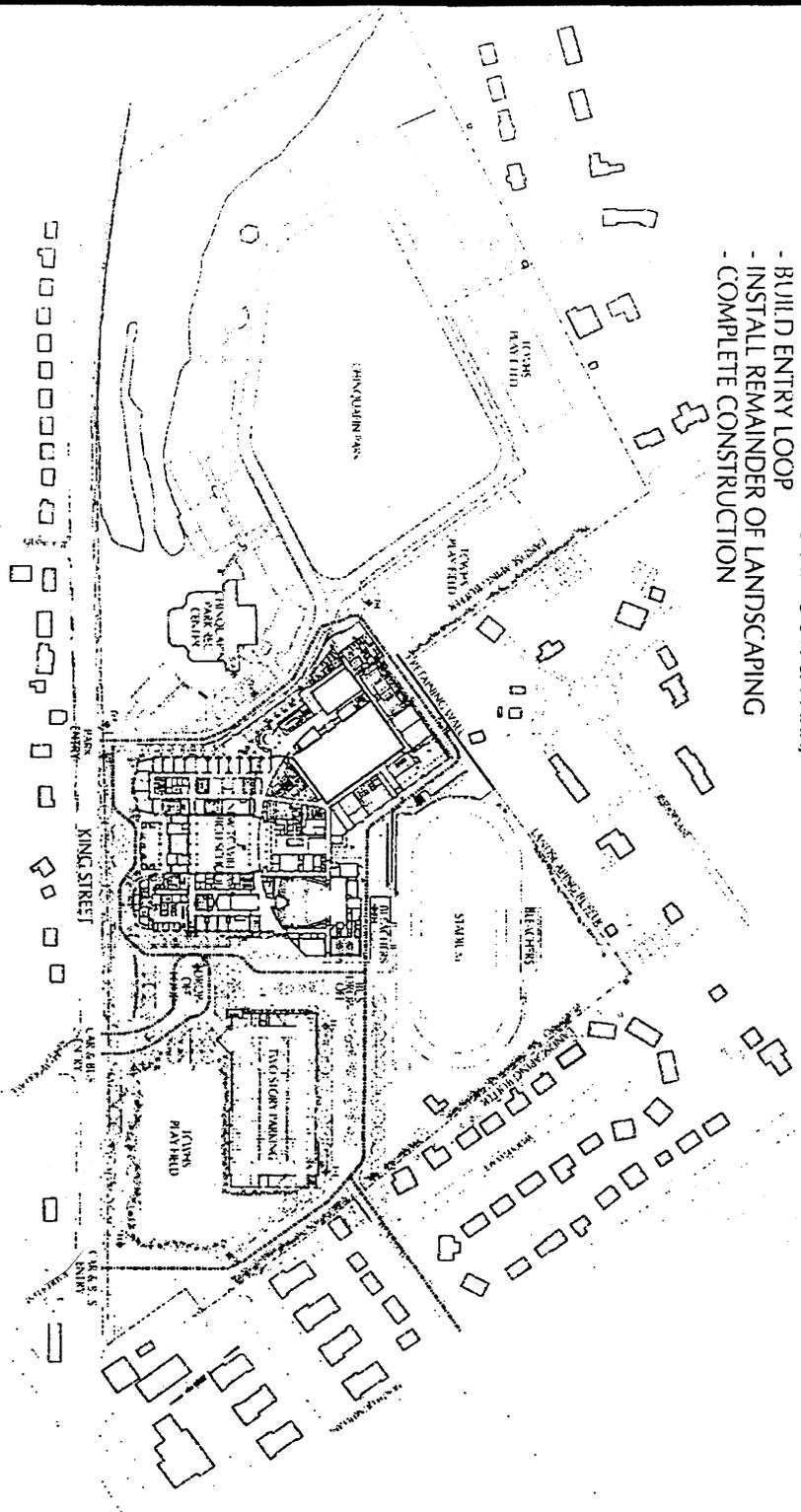
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ARCHITECTS
A PROFESSIONAL CORPORATION
1000 COMMONWEALTH BLVD, SUITE 200
ALEXANDRIA, VA 22304
TEL: 703.836.1100
WWW.ARCADIAARCHITECTS.COM

PH3

CONSTRUCTION ACTIVITIES:

- COMPLETE ORCHESTRA CLASSROOM
- BUILD PARKING STRUCTURE
- BUILD CISTERN
- COMPLETE BUS LOOP & SERVICE ROAD
- GRADE PRACTICE FIELDS (KING ST. & PARK)
- BUILD ENTRY LOOP
- INSTALL REMAINDER OF LANDSCAPING
- COMPLETE CONSTRUCTION



PHASE 4
AUGUST 2006 - FEBRUARY 2007

--- CONST FENCE
 - - - - - TEMP EVE
 BUILDING EXIT

TC WILLIAMS HS
 CONTRACTOR
 SIAMESE CONNECTION
 SIA ♦ EXISTING HYDRANT
 EH ♦ EXISTING HYDRANT
 FH ♦ FIRE HYDRANT

PH4

T.C. WILLIAMS HIGH SCHOOL 182
 SCALE NO. 01-0 0
 ALEXANDRIA CITY PUBLIC SCHOOLS
 ALEXANDRIA, VIRGINIA

ARCHITECTS
 A PROFESSIONAL CORPORATION

The North Ridge Citizens' Association

A Non-Profit Organization

PO BOX 3242

ALEXANDRIA, VA 22302-0242

January 5, 2004

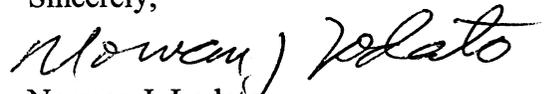
Eric Wagner, Chairman
Alexandria Planning Commission
301 King Street Room 2100
Alexandria, Virginia 22313

Dear Eric Wagner, Chairman:

The North Ridge Citizens' Association had a meeting on November 10, 2003 where we were briefed on the new T.C. Williams construction project. Mr. Howard Middleton gave a very detailed presentation along with the architect who was in charge of the project. At this meeting no one on the North Ridge Board had any objections to the project or expressed concerned about any aspect of this venture. At the end of the presentation several members asked that North Ridge be informed about the impact the construction project may have on traffic flow during this period. We will be interested as the project develops but the presentation was received in a most positive manner by our board members. We were pleased that North Ridge had a chance to weigh in on this issue and that all meetings on this issue going back to last year were open to all interested parties.

Thank you for hearing our perspective on this most important issue.

Sincerely,



Norman J. Lodato

President NRCA

14/153-16
MPA-2003-001
REZ-2002-001
DUP-2002-001

Chapel Hill Homeowners Association
PO Box 16533
Alexandria, VA 22302

URGENT!

December 30, 2003

Dear Ms Ross,

Re: TC Williams High School Rebuilding Program

Thank you for meeting with us on Monday Dec. 22, 2003. We appreciated the opportunity to discuss the concerns of the Chapel Hill Homeowners Association with you, Mr. Culpepper and the attorney for the School Board. Although the meeting provided us with useful information about the new school project we continue to have concerns about what is being proposed by the Planning Department.

As you know from my written comments in September 2002 our concerns are largely about the impact that the new school will have on traffic in this area. We have reviewed the Traffic Impact Study Addendum of November 26, 2003, that Mr. Culpepper provided to us on December 22, and there are a number of issues that we feel need to be clarified before irrevocable decisions are made.

The Addendum itself notes that the new construction will increase traffic on Kenwood by 16%, possibly resulting in 'driving confusion at the main access drive', and 'congestion ...given the close proximity of the parking garage entrance, visitor parking, pickup/drop-off area and pedestrian activity.' Of special concern to us are the findings regarding vehicle queues. The Addendum indicates that its analysis is based on just one day of field observations. Considering the importance of traffic to this community and the success of the new construction it seems as if more analysis would be warranted.

These, and other concerns, were identified at the October 2002 public meeting and City officials assured us that they would be addressed. Unfortunately, despite the recommendation of the Planning Department staff on Dec. 18 to approve these plans, the concerns we raised have not been addressed.

Coming now to our own review of the Study Addendum, we have the following concerns.

Firstly, we should point out that we do not have access to a traffic management plan for the school grounds and we do not know if one exists. The plan that we have seen is for traffic flows in and out of the school. Consequently we are unable to check the calculations made about 'storage capacity' for vehicle queues inside the school grounds.

Having said this, there seem to be a number of factors that materially affect the calculations of traffic impact on Kenwood Avenue and King Street. These add to our expressed concerns about the layout of the building and the consequent need to waive regulations on setback from the road and height with respect to Chinguapin Drive and King Street. This configuration we believe will detract from the architectural merit of the school; will detract from the safety of children entering Chinguapin park; will cause crowding of pedestrians (school students) close to a main road, and will tend to degrade the general amenities of the neighborhood.

1. Garage entry and exit

There appears to be no analysis of vehicle queuing for the garage itself. In the existing school vehicles enter the school through three entrances for about 340 open air spaces. In the new plan there will be a single entrance to 316 covered spaces plus further open air spaces. We expect this to cause two particular problems. First of all, in the morning towards school opening time there would be bunching of arrivals combined with a slowdown of movements within the garage as drivers try to locate the remaining parking places. The resulting parking queue will be alongside another queue for drop-offs on the turning circle. On reasonable assumptions about arrival times and parking times we believe that this will lead to a tail back along the approach road and out into the King Street/Kenwood Avenue entrance.

Secondly, the length of the queue for pickup at the end of the day will be affected. According to the recommended pattern cars exiting from the garage will have to merge with pick-up cars while swapping lanes in the turning circle. This will undoubtedly create considerable congestion (referred to in the consultants report) and lead to a significant tail-back for pickups, most probably out into King Street/Kenwood Avenue.

2. Pick up and drop-off queues.

The report states that 76% of all traffic will enter through the Kenwood Avenue entrance (659 vehicles a day) while traffic at Chinguapin and Radford will be considerably reduced compared to the present situation. Of the 659 vehicles, it seems that about 350 will be for drop-offs and open air parking while the rest enter the garage. The Chinguapin entrance will receive 130 school drop-off vehicles a day. Despite this major concentration of traffic on the Kenwood Avenue entrance the report states that the vehicle queue (estimated maximum 300 feet) will still be containable within the school grounds because 350 feet of queuing space will be available at this entrance.

As stated we have not been able to check this space measurement because we have not got access to an interior traffic plan. Yet even assuming the space measurement is correct, the 300 foot queue assumption appears to be arbitrary, and is inconsistent with the report's own survey findings ¹ (p5). The one-time traffic survey showed a maximum 375 feet measured queue length for pickups at the main entrance at the existing school, which would be 435 feet (16% increase) with the planned school expansion. Thus even with the existing school traffic pattern the claimed storage space would be exceeded by 25%. But this situation would be aggravated by the probable congestion in the afternoon as cars exit from the garage and merge with the pickup traffic. As a result we could expect that significant tailbacks of cars entering to pickup students in King Street and Kenwood Avenue would occur during the afternoon peak.

3. Traffic management at Kenwood

The increase in traffic through the Kenwood Avenue entrance would be such that special arrangements would be made for turn lanes, crossings, and signaling. This is a clear indication of the significantly increased traffic and disturbance in this residential street, and the corresponding hazards it will cause.

¹ It is based on a seemingly unjustified 'estimate' that, despite the major concentration of traffic in the Kenwood entrance, the queue lengths would be equal at the Kenwood and Chinguapin entrances.

4. Solutions

Our ideal solution is a radical one in the present circumstances, but one which is optimal from many perspectives. In particular it would mean the rational use of the whole existing school site. Namely, we would like the City and School Board to revisit the whole configuration and realign the school building along King Street, as it currently is, with frontage slightly nearer the street than the present school building leaving more space at the rear, and to contain all traffic, parking and freight delivery at the rear of the school. The need to maintain the existing building in operation during construction could be handled by a phased building program.

Failing this 'radical solution', however desirable it may be, and given the importance of this new construction, and the need to successfully address traffic congestion and safety concerns, we would like the City and the School Board to further examine moving the school's main entrance to the Western entrance where there is considerable space available, and to relocate the entrance to the parking garage at the side (Western end) of the parking garage, allowing much greater vehicle storage capacity. We would ask for the Kenwood Avenue entrance then to be closed and the 'practice field' located more to the center of the site, leaving additional room for traffic at the West end of the site.

This solution would improve the amenities of the neighborhood and lead to the least disruption of residents (a welcome change of approach considering the encroachments on homes that have been permitted over the last several years). It would also eliminate a traffic hazard at the Kenwood Street/King Street intersection, a hazard which will be worse with significantly increased traffic (a scenario that current police traffic analysis cannot take into account). And it would also probably indirectly relieve the

Chinquapin park entrance, allowing more peaceful and safer use of the park.

A still lesser solution would be to retain the central entrance but reroute all parking garage traffic through the Western entrance - thus reducing at least some of the burden on the Kenwood Avenue entrance which would then be more tolerable to neighboring residents.

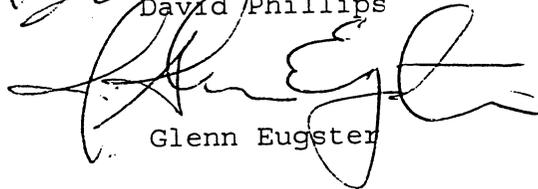
We would like you to consider these proposals in the formal decision-making process. If it is helpful for us to meet with you we are available to do so.

Thanks again for giving us the opportunity to provide input to this process.

Sincerely,



David Phillips



Glenn Eugster

Chapel Hill Homeowners Association

14, 15, & 16

1-24-04

Docket Item # 14 and 15
MASTER PLAN AMENDMENT #2003-0010
REZONING #2003-0006
T.C. WILLIAMS HIGH SCHOOL

Planning Commission Meeting
January 6, 2004

ISSUE: Consideration of a request for an amendment to the Taylor Run/Duke Street Small Area Plan chapter of the City's Master Plan to change the land use designation of the subject property from P/Parks to I/Institutional.

Consideration of a request for a zoning amendment to change the zoning designation of the subject property from POS/Public Open Space to R-20/Single family residential.

APPLICANT: Alexandria City Public Schools, by J. Howard Middleton, attorney

LOCATION: 3330 King Street

PLANNING COMMISSION ACTION, JANUARY 6, 2004: On a motion by Mr. Komoroske, seconded by Ms. Fossum, the Planning Commission voted to **recommend approval** of the proposed master plan amendment and rezoning. The motion carried on a vote of 7 to 0.

Reason: The Planning Commission agreed with the staff analysis, thanking both ACPS and city staff for their tremendous effort, and noting the lengthy and extensive community involvement for the project. The Commission revised condition #69, adding a requirement that use of the sound system at the stadium remain at its current levels unless increases are allowed by the Community Advisory Committee or by the Planning Commission and City Council. In addition the Planning Commission requested that the applicant submit with its report to City Council, a report analyzing why underground parking is not feasible or desirable at the school.

(For speakers, see DSUP 2003-0044).

14, 15 & 16
1-24-04

Docket Item # 16
DEVELOPMENT SUP # 2003-0044
T.C. WILLIAMS HIGH SCHOOL

Planning Commission Meeting
January 6, 2004

ISSUE: Consideration of a request for a development special use permit, with site plan, subdivision, and site plan modifications, for construction of a new high school which exceeds the FAR and height in the R-20 zone.

APPLICANT: Alexandria City Public Schools, by J. Howard Middleton, attorney

LOCATION: 3330 King Street

ZONE: R-20/Residential zone
POS/Public Open Space zone

PLANNING COMMISSION ACTION, JANUARY 6, 2004: On a motion by Mr. Komoroske, seconded by Ms. Fossum, the Planning Commission voted to **recommend approval** of the proposed development special use permit , with subdivision and site plan modifications, subject to compliance with all applicable codes, ordinances and all staff recommendations and to amend Condition #69. The motion carried on a vote of 7 to 0.

Reason: The Planning Commission agreed with the staff analysis, thanking both ACPS and city staff for their tremendous effort, and noting the lengthy and extensive community involvement for the project. The Commission revised condition #69, adding a requirement that use of the sound system at the stadium remain at its current levels unless increases are allowed by the Community Advisory Committee or by the Planning Commission and City Council. In addition the Planning Commission requested that the applicant submit with its report to City Council, a report analyzing why underground parking is not feasible or desirable at the school.

Speakers:

Mr. Howard Middleton, attorney, represented the application.

Mr. David Phillips, Chapel Hill Home Owners Association spoke in general support of the project, but against the lack of involvement of Kenwood Avenue residents, especially related to traffic, congestion safety, and the building mass.

Mr. Joseph Glenn Eugster, president Chapel Hill Home Owners Association spoke about his concerns about: traffic and safety and loss of open space.

Ms. Mary McCarthy, 2714 Franklin Court and T.C. Williams employee, spoke about traffic concerns.

Mr. Peter Coppelman, 405 High Street and T.C. Williams PTSA President, spoke in support of the project.

Ms. Lillian Patterson, Seminary Hill Civic Association board member and co-chair of the Seminary Hill T.C. Redevelopment Task Force spoke in general support of the project and planning process, but noted concerns about pollution, the proximity of the garage to Woods Place residences, use of the new practice field, landscaping and bus traffic.

Ms. Carter Fleming, Seminary Hill T.C. Williams Redevelopment Task Force co-chair spoke in general support of the project, but with concern about DRPCA use of the stadium and sound system and the artificial turf proposed for the field. She requested that the DRPCA not have open-ended use of the stadium sound system.

14,15,16
1-24-04

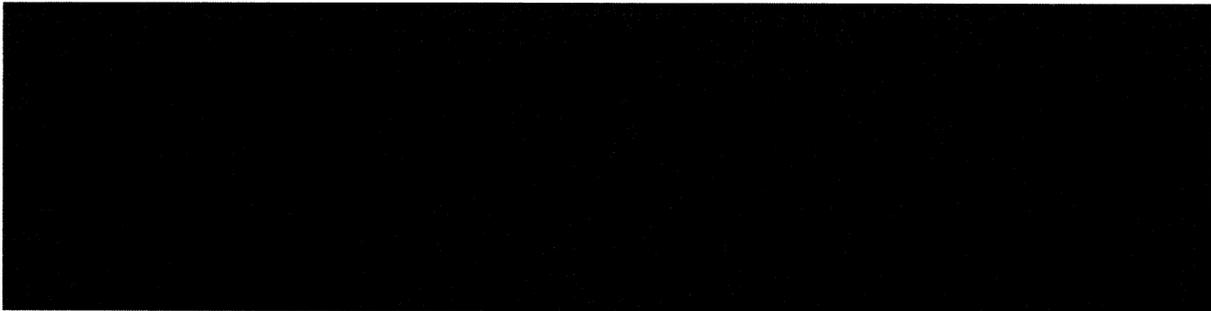


<kirkjm@comcast.net>
01/20/2004 10:28 PM
Please respond to kirkjm

To: <alexvamayor@aol.com>, <delpepper@aol.com>,
<council@joycewoodson.net>, <councilmangaines@aol.com>,
<rob@krupicka.com>, <macdonaldcouncil@msn.com>,
<paulcsmedberg@aol.com>, <rose.boyd@ci.alexandria.va.us>,
<jackie.henderson@ci.alexandria.va.us>

cc:

Subject: City of Alexandria Website Contact Us - EMail for Mayor, Vice-Mayor
and Council Members (alexvamayor@aol.com, delpepper@aol.com,
council@joycewoodson.net, councilmangaines@aol.com,
rob@krupicka.com, macdonaldcouncil@msn.com,
paulcsmedberg@aol.com, rose.boyd@ci.alexandria.va.us,
jackie.henderson@ci.alexandria.va.us)



Time: [Tue Jan 20, 2004 22:28:10] IP Address: [69.140.64.191]

Response requested:

First Name: Chapel Hill Home Owners Asso.

Last Name: Jim Kirkland, Board Secretary

Street Address: 2722 Franklin Court

City: Alexandria

State: VA

Zip: 22302

Phone: 703-824-9374

Email Address: kirkjm@comcast.net

Comments: January 20, 2004

Honorable Mayor and City Council Members:

We, the Board of the Chapel Hill Home Owners Association across from T.C. Williams High School, appeal to you, whom we elect, to consider the issues we are raising regarding traffic and design concerns in the reconstruction of T.C. Williams High School. Following below are three letters drafted to you but pasted below because unable to be attached to this City Hall form of e-mail. They are, as Documents A, B and C: (1) a summary letter of what's before you and our concerns, (2) our detailed letter for you to review

and for the written record, and (3) a letter to you from our President, Glenn Eugster, reviewing the issues he raised before the Planning Commission January 6.

We are delivering hard copies for you to City Hall and plan to address the Council this Saturday's City Council meeting. We thank you for your time and service to the City.

ATTACHMENT A

January 18, 2004

Honorable Mayor and City Council members:

At this Saturday's City Council, you will be considering taking three actions regarding reconstruction of T.C. Williams High Schools: 1) amend the City's Master Plan for the School; 2) Rezone the school property; and 3) issue a special use permit to allow a development that is not currently permitted.

Earlier this month the City Planning Commission unanimously approved the proposal for TC Williams. Three folks from our Association, David Phillips, Mary McCarthy and I spoke at the hearing about our concerns about the development proposal. Unfortunately we were not able to get the Commission to discuss our interests or make any modifications to the proposed plan.

The Board continues to have concerns about the impact of the proposed project on the quality of life of our community. David Phillips is planning to provide comments on behalf of the Association on Saturday. We hope you will have the courage to respond to our concerns, and put off your final decision to make changes as deemed necessary.

We know the city and many groups have worked hard to create and assemble everything necessary to put forward this necessary reconstruction, and much positive has been accomplished. However, listed below are the concerns our research has identified.

Thank you for your time.

Board of Directors, Chapel Hill Home Owners Association:

President Glenn Eugster – 703-845-8947

Treasurer David Phillips – 703-824-9437
Secretary Jim Kirkland – 703-824-9374
Grounds Chair Ann Parham – 703-933-0737

Negative Impacts of some proposed T.C. Williams High School changes on the Chapel Hill townhouse community on Franklin Court off Kenwood Court across King Street,

1. The City proposes that the new school be set back 44 feet from King Street. The current building is set 200 feet back. The new school building will be massive and will be raised on an elevation directly opposite our houses that will take it up to 70 feet in height above the level of the road. This would normally require an equivalent setback of at least 70 feet. This is much higher than any other building in the vicinity. There is likely to be crowding of students at the roadside as well as litter, traffic etc.

The building will come right up to Chinquapin drive. There will therefore be crowding of the drive with both vehicles and students who will spill out from the school on entry or exit. Entrance to the park will likely be impeded whenever the school is open.

2. The new parking garage plus nearby above-ground parking and pick up /drop off area will mean that the Kenwood entrance will apparently take three times the existing traffic. This is because parking will be accessible only from the Kenwood Avenue entrance (compared to three entrances at the moment). So, in addition to the additional traffic from a larger school there will also be a greatly increased concentration of traffic entering at the Kenwood Avenue entrance - making it possible that there will be serious backups into Kenwood and King, and an increased hazard at the Kenwood/King intersection, especially in bad weather.

The City's traffic study predicts that there will be a 23%, or greater, increase of traffic on Kenwood Ave. (including a 75% increase in school traffic) and that traffic congestion and confusion are likely to occur within the entrance drive. Nevertheless, the City traffic engineer told the meeting of the Planning Commission that he did not believe that traffic or the likely congestion and safety problems would arise at the Kenwood-King Street or Kenwood-Braddock intersections.

The residents on the other side of the school (off

Quaker lane) have, apparently, successfully lobbied to get the back entrance closed to avoid inconvenience to them - so the trash and goods vehicles will now also enter from King Street, increasing further the traffic burden.

3. Construction will require that current parking areas be closed and cars and construction equipment are proposed to be moved to other off-site parking areas, such as the Scottish Rite Temple parking lot. Construction is scheduled to go on for four years. This will further strain the traffic on Kenwood, as vehicles access the construction site.

4. The City has formed a Committee of neighborhood organizations to work with the City, School Board and the developer of the property to guide the planning, construction and future management of the property. The Chapel Hill Homeowners Association has not been invited to be a part of the committee.

5. The new school is designed to remove 3.9 acres of existing City public open space that will not be replaced. In addition, plans for Chinquapin Drive include using the road to provide access and parking to the school. The amount of traffic proposed for Chinquapin Drive will make it more difficult for residents to drive or walk to the recreation center, further reducing public convenience.

6. With an ample 28 acre site, it is unnecessary, as well as undesirable, for the school to "spill out" into the local community, causing hazards for both students and traffic.

ATTACHMENT B

2727 Franklin Court
Alexandria, VA 22302

Mayor and City Council
City of Alexandria
Alexandria City Hall
301 King Street
Alexandria, VA
January 16, 2004

Subject: Remarks by Glenn Eugster on the TC Williams High School Rebuilding Project

Dear Mayor and City Council Members,

Thank you for the opportunity to make comments on the three actions before you on Saturday January 24, 2004 related to the reconstruction of the TC Williams High School. I reside at 2727 Franklin Court, across King Street from TC Williams High School and I'm the President of the Chapel Hill Homeowners Association.

First, hats-off to each of you for the role that you play in planning and managing our community. Your commitment to public service is noteworthy and appreciated.

Second, hats-off to the Planning Commission staff, the City agencies and the School Board for the work they have done to develop this proposal. It has been challenging to try to meld environmental, social and economic factors into a plan for the future.

In September 2002 my wife and I attended our first TC Williams High School planning meeting. I submitted 3-4 pages of questions, concerns and suggestions about the six options that were being considered for the new school. In October that year I received a brief reply from Ms. Walsh of the School Board indicating that my concerns would be addressed in the option that would be selected.

On December 18, 2003 I attended the Planning Commission's work session and was able to hear, see and read about the proposal you are considering tonight. Despite Ms. Walsh's assurances and several discussions with City and School Board staff, some of my most important concerns have not been addressed in the plan that staff has recommended you approve tonight. On January 6 I presented comments to the City Planning Commission and our concerns were not responded to. Frankly, I'd like your help in addressing the concerns we shared with the City over a year ago about the following issues:

- Traffic congestion and safety on Kenwood Avenue. The City's own report indicates that there will be congestion and confusion with the new design and we were told that safety at Kenwood and King Street was not considered in the studies leading to this plan.

- The loss of open space. The proposed plan eliminates public open space at a time when the City has made a commitment to protect existing public open space and find ways to increase parks, open space and recreation areas.

I'd like you to respond to our concerns, either before you make a final decision on this plan or as part of the conditions for plan approval. Please help our efforts to maintain this part of Alexandria as a livable community by doing the following:

- 1) Help ensure that the new TC Williams school

will not increase the traffic congestion or safety problems we are experiencing on Kenwood Ave. and King Street. We'd like you to require traffic calming measures and new safety measures be implemented along Kenwood Ave. and at the intersection of Kenwood and King Street. The City's study indicates that traffic will increase by more than 23% on Kenwood and that congestion and confusion are likely at the main entrance to the school. The City's traffic police believe more accidents may occur and traffic-calming measures are desirable. Traffic is a problem everywhere but we need to take action through your process to assure that we aren't designing problems.

2) Require the School Board or the City to replace the public open space you are taking for the school with an equal or greater amount within this part of the City. Alexandria was ranked the 11th densest city in the U.S. in 1990 in terms of people per acre. In 2000 we went from 11.0 people per acre to 12.7. In a dense community such as ours open space is essential for our ecological, physical, spiritual and economic health. We'd like you to require that this project, and hopefully others you act on, will not result in a reduction of public open space in the City. Public open space is part of what makes our community special!

3) Finally, the City and the School Board have created two community committees that include neighborhood groups related to TC Williams. The Chapel Hill Homeowners Association has not been given the opportunity to participate on these committees. Your process for public involvement has been selective in working with neighborhood groups and it has created an uneven playing field for decision-making. We have asked the City for the opportunity to participate on the advisory committees and would like to have representation.

I appreciate the opportunity you have provided us, through your hearing and the public meetings, to provide input into these decisions. Thank you.
Sincerely,

Glenn Eugster

ATTACHMENT C

Chapel Hill Home Owners Association
P.O. Box 16533
Alexandria Va 22302

January 17, 2004

T.C. Williams HS rebuilding project

Dear Council Members,

We are writing to you on behalf of the Chapel Hill Homeowners Association of Franklin Court, a community of some 70 individuals directly across from T.C. Williams High School. A reason why many of us purchased our homes in this area was because of its reasonably open peri-urban environment, and its access to Chinquapin park, the sports and childrens' facilities. Since we are the closest residential community to the school, and the most affected by the rebuilding project we are concerned at a number of proposals that are being made.

We would like to say from the outset that we fully support the rebuilding of the school in its present site and we appreciate that this is a highly complex project and that a great deal of sincere effort and time has gone into the site planning. We also understand that efforts have been made to include the community in the planning process, although the views of our homeowners association have not been directly sought until this November.

We attended a public meeting in September, 2002, at which the options were outlined, and submitted a letter listing three pages of questions and concerns especially regarding traffic congestion and safety. We received a reply from Ms. Margaret Walsh in October, 2002, assuring us that the concerns would be addressed. We have been aware of the alternative plans during this time but we were not aware that the final selection had been made – as far back as the end of 2002. In August 2003 we visited the planning office at City Hall (6th floor) and were told that the 'decision' on which option was to be proposed had not been made. After learning that the planning department had in fact made an internal decision, we attended a Planning Commission working meeting on December 19, and we met further with Ms. Ross, Mr. Culpeper and Mr. Middleton, attorney, to explain again what our concerns were. Following this we sent a five-page letter to Ms. Ross setting out in detail the points that we were trying to make, especially regarding the traffic access to the school and its effects on our neighborhood and the safety issues. We have

neither received a reply, nor an acknowledgement of this letter.

On January 6, five members of our Association attended the public meeting of the Planning Commission at City Hall. Once again we explained (albeit very briefly, given the three minutes allowed) our problems especially with traffic and safety. We asked the Commissioners to reconsider the pattern of traffic access. Unfortunately at the end of the presentations the Commissioners made no attempt to discuss these issues. Instead the discussions that ensued suggested that amplified noise from occasional school sports events were of more concern to them than traffic congestion and potential accidents. We were finally led to believe that our concerns and representations were to be ignored.

The local environment in general

The area around the intersection of Quaker Lane/King Street/ Braddock Road is greatly in need of proper planning. Currently it is a largely residential area but with mixed use and heavy traffic. The area is dissected by several planning zones and by the county border. Its last 'small area plan' dates from 1992 (previously 1974) in which it was recommended that the traffic problems at King, Braddock and Quaker should be addressed and resolved. It has undergone piecemeal changes, which can cause the dysfunctional growth that has characterized many suburban areas. Its mixed-use character makes it especially vulnerable to this. An example of this was the recent approval by the City council of a change of use from residential to commercial for Lindsay cars, resulting in replacement of one of the few blocks that had contained single family homes, on a wooded lot, by (yet another) service garage. There is a very large amount of space in the neighborhood taken up by gas stations (4), car dealerships and service garages, mainly serving people from outside the area. The King Street residents east of Quaker Lane are not well served by shops and retail enterprises and the intersection of Quaker, King and Braddock remains dangerous for pedestrians (even with pedestrian lights, finally installed after several years of requests). There are other issues such as the presence of a crematorium, which would not be permitted under current regulations in a residential area.

We perfectly well understand that garages, gas

stations, dealerships, crematoria, office buildings, and schools are all needed, but the question is whether they should be concentrated in one small area! In sum, despite considerable efforts at site planning solutions it seems that very little attention has been paid by the Planning Commission to this area as a cohesive place for people to live in.

The school site and design

The current school site is an attractive and valuable piece of land running along King Street, with plenty of depth to set back buildings from the road. The obvious configuration for the new school is to build it along the length of the site, allowing a broad and, hopefully impressive, front elevation, as is the case currently. However the design footprint of the buildings has been constrained by the need to keep a school operating during the construction period. As a way of achieving this the building has been 'shoehorned' in to one end of the available site, positioned 'end-on' to King Street, to allow the existing school buildings to remain in use. Ultimately the central part of the site, where the main architectural features would normally be situated, will be a parking garage !!

Along with the 'shoehorned' design come a number of real problems. The small site that the school is squeezed into means it has to come up to 44 feet from Kings Street, with an elevation of 70 feet (including elevated ground) from the street level. Both features breach regulations for setback and height (even taking into account a recent height regulation increase). The very large building will loom over King Street, out of proportion with any other structure in the vicinity, whereas the current school building is set 200 feet back from the road, with trees and landscaping in front. Given the number of students using entrance doors close to the street it is unlikely that planned landscaping will be respected, and it is questionable whether exits used by several hundred students should be permitted so close to a main road. The access to Chinquapin recreation center and park will now be along the side of the school building, on a road crowded with vehicles and students during certain times of the day. It will no longer be a quiet public park access but one that is crowded, probably litter strewn, and where children will have to be closely supervised in the daytime. At night, the building will be an empty hulk!

This plan cannot be described as a triumph of design, unless defined purely in terms of short-term expediency. Alexandria will have to live with this design for its principal high school for say the next 50 years, whereas the design has been largely conditioned by the interim needs of the next three years. Regardless of the great effort that has gone into trying to produce an attractive exterior design and environmental safeguards, a school will always have its attendant problems for the community. In this case there will be a massive building right by the roadside. Put another way, the question is quite simple. Why should a school with a large and totally adequate 28-acre site adjacent to a public park need to 'spill out' into the local streets, creating a hazard and risking accidents for the students themselves as well as the public?

Traffic pattern

A particular concern for us is the traffic flow. The enlarged school will have more traffic and more spaces for parking. A rear entrance will be sealed off at the request of residents on Quaker lane, so that trash and goods delivery will now also have to use access from King Street. Most cars to be parked in the school will no longer be in the open, for which there are currently three entrance roads, five exit roads and a large amount of lining-up space for vehicles, all interconnected, so that traffic flow is unimpeded. Instead, according to the Impact Study, 76% of all traffic (nearly 700 vehicles) are now planned to use the central (Kenwood Avenue) entrance, for pickups, drop-offs, parking in the parking garage and some open parking. Kenwood Avenue is a residential street on which we live. The four-way King/Kenwood intersection will become more dangerous with the greater concentration of traffic in and out of one entrance, and the additional backups in Kenwood Avenue (a scenario that police analyses based on current traffic flows cannot address). To try to reduce the hazard the plan calls for the intersection to have crossings, filters and turn-lanes, further degrading a residential neighborhood.

To make matters worse, despite the considerable increase in traffic through the main entrance, the effective line-up and waiting space within the main entrance will now be greatly reduced, so much so that the space available would not in fact accommodate the maximum observed vehicle queue even at the existing school, i.e. regardless of increases. This observed queue information

has been ignored within the same report in which it is cited, and has been replaced by a lower queue estimate with inadequate justification or explanation. Furthermore, the drive-lane from the entrance into the garage will accommodate only seven to eight vehicles, which clearly risks blockage back to the intersection if there are any garage entry delays at peak times, but the impact study made available to us does not analyze the garage entrance traffic at all, only the pickup/dropoff traffic. In summary, we believe that the calculations made on vehicle queuing at the Kenwood entrance drive are significantly underestimated, and that this has not been seriously addressed despite our repeated representations to the planners.

Compensations and mitigations

Compensations built into the design include an open space at the front of the school site, on King Street, to be used as a practice field. However this space already largely exists in the current school. Furthermore (unlike the space re-zoned from POS) it will not be accessible to the public, and finally it is vulnerable to expansion plans for the school in the future. It is therefore not a compensation. A mitigation is the tree screening on King street. This is acceptable but it will not be able to significantly reduce the visual effects of a massive building on the roadside, especially in the first few years, and it is unlikely to tolerate well the large school student traffic. Noise problems and litter will be mitigated by making the school self-sufficient in catering and preventing loud speakers being used on the outside of the building. This is acceptable but of course will not eliminate all problems, especially traffic noise. Even with a truly beautiful design (which this cannot be) TC Williams remains a school, with all the attendant problems of noise, litter and increasing traffic.

Problems during construction.

We anticipate that the construction period will be seriously problematic for us as residents. Traffic will be disrupted for as much as four years, along with the noise and litter. We are not at all confident that the planners have considered how the school will operate on a near-normal basis during construction, especially given the space constraints, for student and staff cars, buses, goods deliveries, along with closure of entrances and builders equipment, vehicles and cars. If parking areas such as the Scottish Rite Temple

and Churches are used this will considerably increase traffic in Kenwood Avenue which would be the main access from the parking areas.

Remedies

We feel that there is a strong case for reviewing the configuration of the school buildings in the long-term interests of the city of Alexandria, to rationally use the existing site and obviate needs for use changes and exemptions from regulations, as well as to improve the overall amenities and prevent adverse effects on the neighborhood. We believe that it is possible to construct a building on a phased basis that uses the site in a way that is more rational and architecturally more attractive, while keeping the current school operating, by aligning the frontage along King Street, as it is currently. The new building could also be set a little closer to King Street than the existing school building, so as to accommodate vehicle traffic and parking at the rear. Regardless of the time and resources spent on 'finalizing' the currently proposed design, we think that the City should look ahead and design a school that all can be proud of in the long-term, and that the local residents can live with.

Failing this solution, which is the optimum, we propose some ways of alleviating specific problems.

1. Redesign the frontage of the building on Kings Street to allow further setback to at least the minimum regulation distance by e.g. creating a flatter frontage.
2. Close off the Kenwood Avenue entrance and move the practice field further to the center of the site on King Street. Use the large space at the west (Radford) end of the site as main traffic access, widening and creating a turnaround and entrance to the parking garage from the side OR Create two entrances to the garage – from Kenwood and from Radford (side entrance to the garage).
3. Realign the Chinquapin access road to allow space for easy pedestrian access to the park and space for school students to 'spill out' without crowding the road, and also clarify the public right of way to the park.
4. Restrict the use by students of the entrances into the school building at the points closest to King Street.

5. Ensure rigorous adherence to material quality, color, tree screening and landscaping plans.
6. During the construction period use the sports field area on Chinguapin Park for builder's vehicles, and not any outside parking areas, so as to avoid creating further traffic congestion on Kenwood and King Street.

We appreciate the length of time that has gone into the current planning, and the major difficulties and frustrations that result from making changes. But we believe that as a community we have no choice but to look ahead and make hard decisions that are in the long-term interests of the city of Alexandria.

Sincerely

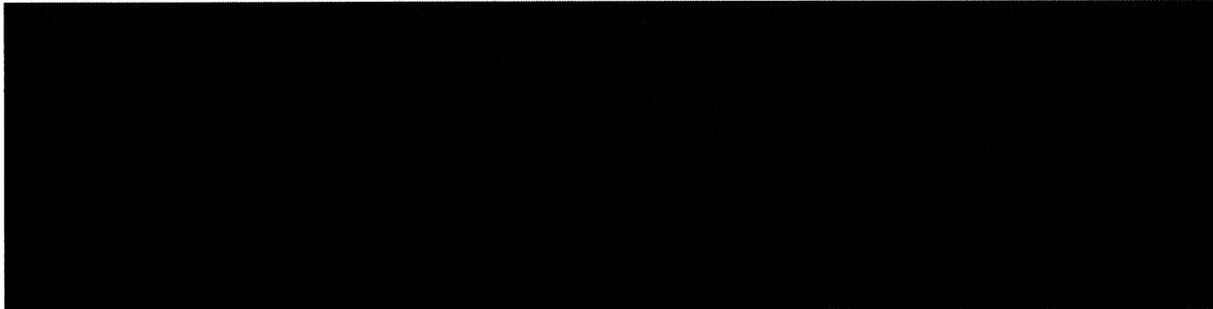
Glenn Eugster, David Phillips, Jim Kirkland, and
Ann Parham
Board of Directors, Chapel Hill Home Owners
Association

14,15,16
1-24-04



<hjaf@loc.gov>
01/21/2004 09:09 AM
Please respond to hjaf

To: <alexvamayor@aol.com>, <delpepper@aol.com>, <council@joycewoodson.net>, <councilmangaines@aol.com>, <rob@krupicka.com>, <macdonaldcouncil@msn.com>, <paulcsmedberg@aol.com>, <rose.boyd@ci.alexandria.va.us>, <jackie.henderson@ci.alexandria.va.us>
cc:
Subject: City of Alexandria Website Contact Us - EMail for Mayor, Vice-Mayor and Council Members (alexvamayor@aol.com, delpepper@aol.com, council@joycewoodson.net, councilmangaines@aol.com, rob@krupicka.com, macdonaldcouncil@msn.com, paulcsmedberg@aol.com, rose.boyd@ci.alexandria.va.us, jackie.henderson@ci.alexandria.va.us)



Time: [Wed Jan 21, 2004 09:09:22] IP Address: [140.147.168.12]

Response requested:

First Name: Howard

Last Name: Jaffe

Street Address: 2713 Franklin Ct.

City: Alexandria

State: Va.

Zip: 22302

Phone: 703-845-1758

Email Address: hjaf@loc.gov

Comments: Negative Impacts of the TC Williams High School Changes on Chapel Hill

1. The City proposes that the new school be set back 44 feet from King Street. The current building is set 200 feet back. The new school building will be massive and will be raised on an elevation directly opposite our houses that will take it up to 70 feet in height above the level of the road. This would normally require an equivalent setback of at least 70 feet. This is much higher than any other building in the vicinity. There is likely to be crowding of students at the roadside as well as litter, traffic etc.

The building will come right up to Chinquapin drive. There will therefore be crowding of the drive with both vehicles and students who will spill out from the school on entry or exit. Entrance to the park will likely be impeded whenever the school is open.

2. The new parking garage plus nearby above-ground parking and pick up /drop off area will mean that the Kenwood entrance will apparently take three times the existing traffic. This is because parking will be accessible only from the Kenwood Avenue entrance (compared to three entrances at the moment). So, in addition to the additional traffic from a larger school there will also be a greatly increased concentration of traffic entering at the Kenwood Avenue entrance - making it possible that there will be serious backups into Kenwood and King, and an increased hazard at the Kenwood/King intersection, especially in bad weather.

The City's traffic study predicts that there will be a 23%, or greater, increase of traffic on Kenwood Ave. (including a 75% increase in school traffic) and that traffic congestion and confusion are likely to occur within the entrance drive. Nevertheless, the City traffic engineer told the meeting of the Planning Commission that he did not believe that traffic or the likely congestion and safety problems would arise at the Kenwood-King Street or Kenwood-Braddock intersections.

The residents on the other side of the school (off Quaker lane) have, apparently, successfully lobbied to get the back entrance closed to avoid inconvenience to them - so the trash and goods vehicles will now also enter from King Street, increasing further the traffic burden.

3. Construction will require that current parking areas be closed and cars and construction equipment are proposed to be moved to other off-site parking areas, such as the Scottish Rite Temple parking lot. Construction is scheduled to go on for four years. This will further strain the traffic on Kenwood, as vehicles access the construction site.

4. The City has formed a Committee of neighborhood organizations to work with the City, School Board and the developer of the property to guide the planning, construction and future management of the property. The Chapel Hill Homeowners Association has not been invited to be a part of the committee.

5. The new school is designed to remove 3.9 acres of existing City public open space that will not be replaced. In addition, plans for Chinguapin Drive include using the road to provide access and parking to the school. The amount of traffic proposed for Chinguapin Drive will make it more difficult for residents to drive or walk to the recreation center, further reducing public convenience.

6. With an ample 28 acre site, it is unnecessary, as well as undesirable, for the school to “spill out” into the local community, causing hazards for both students and traffic.

14, 15, 16

1-24-04

**STATEMENT REGARDING ITEMS 14,15 &16
1-24-04 DOCKETT**

The issue before Council today is the approval of a SUP and rezoning for T.C. Williams H.S. I find no reason to disagree with the recommendations of the staff and the planning commission. The approval of this request is reasonable. If this were a private institution I would expect council to approve it. But this is not for a private institution. It is for the city and there is more a stake here than the simple approval of a SUP.

The approval of this request will also in some way be a ratification of a design process that has been deeply flawed. I believe the overwhelming majority of people who have followed this process closely would agree. I recognize there is a lengthy report associated with this request but in an attempt to be brief I cannot completely address it. So...

Hasn't there been a series of meetings to gather citizen input? Yes BUT ... There have been no substantive responses to serious questions raised and indeed there is a work group now studying whether the stadium should be moved off site. Retaining the stadium on site is a significantly constraining parameter. Questions regarding the site and its elevations have been routinely ignored. Efforts to coordinate with the recreation center have been left to drift. The sports barn, which is a much-needed facility, although initially reinstated, is now again missing.

Haven't the architects made this a green school at citizens request? Yes BUT ... A large number of points needed to classify this as a green building are coming from the recycling of material from the old building. We will have a green demolition but whether we will have a green building is another question. When repeatedly asked, the architects wouldn't even admit the direction true north. The orientation of the building is the prime starting point of a truly green building.

Isn't this a state of the art concept for academic organization of a large school? Yes BUT ... The Career and Tech Ed component has consistently received the short end, losing much needed space to a commissary kitchen serving the whole school system. Only a serving kitchen is needed at the school while the commissary kitchen could be moved almost anywhere and needs not be associated with any school. This reduction in space was done without even giving the remotest look at other alternatives.

Hasn't the planning staff effected necessary changes? Yes BUT...Some of the efforts to obtain a more human scale can be equated to putting make-up on a pig. The building is going to be large. The building is conceived as a monumental structure sitting on a flat site and viewed from a frontal position. Neither of these assumptions are the reality of the site. The site is significantly sloped and is viewed at a passing angle when driving down King Street. The structure is placed on an earthen platform with a 30-degree berm raising 10 ft. above street level at the northeast corner. Fiddling with the architectural details will not lessen the impact of this berm.

A lot of this comes down to discussing if the glass is half full or half empty. There are many things that are right about this school proposal, but I ask why not a FULL glass. Why not a building that responds to, and is integral with this admittedly difficult site? Why not a truly green building? Are we going to push for something that is really good or just settle for something that is better than what we have now? The city will be married to this facility for at least two generations. When selecting a spouse, I don't believe you should just settle for something. No divorce will be possible here.

My time for addressing you is marching on as is the time needed to make decisions concerning this project. So what are you going to do?.. What are WE going to do? I say just say no. If not now, when? Alexandria deserves a better school than this proposal represents.

William Brandon

**TESTIMONY OF PETER COPPELMAN,
PRESIDENT, T.C. WILLIAMS PTSA
BEFORE THE CITY OF ALEXANDRIA
CITY COUNCIL**

January 24, 2004

Mr. Mayor and Members of the City Council:

My name is Peter Coppelman. I am President of the T.C. Williams Parent Teacher Student Association (PTSA). I am here to support strongly the three requests by the ACPS pending before the Council: (1) a Master Plan Amendment (#2003-0006) to change a portion of the land use designation from Parks to Institutional; (2) a Rezoning request (#2003-0006) to rezone a portion of the high school site from Public Open Space to Residential (R-20); and (3) a request for a Development Special Use Permit (#2002-0044) with increase in the floor area ratio, increase in height, and placement of public school trailers. I am not here to discuss the specifics of the requests in detail. Attorney Howard Middleton has done that.

My purpose in coming before the Council is to put these requests in a broader context, and discuss why granting these requests represents the crucial next step in building a new T.C. Williams High School.

T.C. Williams was originally constructed in 1965. The Career Center and the Sports Barn were added in 1976 and 1982, respectively. Five trailers have been added in recent years. The building includes a little over 360,000 square feet. The current building has served the students and community of Alexandria well over the years, but its days, clearly are numbered. The ceilings drip. Students either swelter or freeze in windowless classrooms. Modern computer technology has overtaken the minimalist electrical system. Teachers lack planning space. The whole unified high school concept conflicts with the latest research showing that students fare far better in smaller learning communities.

Beginning in 2001 the school system and the community began to consider what to do about T.C. Williams. Should it be renovated or replaced? If it was going to be replaced, should it be moved to another site? If it remained at the present site, should it expand into Chinquapin Park? Should the stadium at the new school be built in front of the school or remain in the back?

In December 2001 architects were hired to assist the deliberations. The architects developed a rolling series of options for consideration of the school board and the community. Open meetings were held with the community and City Council, in addition to many smaller meetings with neighbors and other interested parties. In December 2002 the School Board decided to build the new school solely on the existing site (i.e. not to expand into Chinquapin Park), to connect the new school with the Chinquapin Recreation Center, and to continue discussions with the Parks Department concerning jointly utilized elements.

At the same time, a grassroots campaign developed in the community to have the new building be environmentally sensitive, and even to have it be the first new high school in the nation to comply with the Leadership in Energy and Environmental Design (LEED) certification program. In February 2003 the architects presented a design for the new TC that would earn a silver LEED rating. Sustainable design features include underground water storage tanks to capture and recycle rainwater for toilet flushing, irrigating, and cooling; use of ceramic tiles that require no painting; recycling building materials from the old building; increased use of natural daylight; erosion and sediment control, and water efficient landscaping.

In accordance with research showing that students learn better in smaller learning communities, the new high school will be organized into five smaller learning groups for core courses. Then students will take elective and specialty courses with students from across the entire school. The new structure will allow a decentralized administrative and guidance support program, and will foster flexibility in how classes will be offered. ACPS and TC staff have worked endless hours planning an educational program that will best enable students to meet the challenges of the 21st century.

Considering the magnitude, complexity, importance, and cost of the task of building one of the most important buildings the City of Alexandria will construct in our lifetimes, the process has gone remarkably smoothly. This fall a joint public hearing was held with the City's Planning and Zoning Department. Meetings between the City and the school system are ongoing. All of the players so far have done their jobs well. Now it is the City Council's turn to step up to the plate. Because the new TC does not comport with current constraints on the property, movement forward on the new TC requires the Master Plan Amendment, Rezoning, and Development Special Use Permit at issue in this hearing.

On behalf of the students, parents, and all citizens of Alexandria who will benefit from a new and glorious TC Williams High School, I ask you to act favorably on the pending requests.

REMEMBER THE TITANS!

Thank you.

SPEAKER'S FORM

DOCKET ITEM NO. 14, 15, 16

**PLEASE COMPLETE THIS FORM AND GIVE IT TO THE CITY CLERK
BEFORE YOU SPEAK ON A DOCKET ITEM.**

PLEASE ANNOUNCE THE INFORMATION SPECIFIED BELOW PRIOR TO SPEAKING.

1. NAME: HOWARD MIDDLETON
2. ADDRESS: 3110 FAIRVIEW PARK DR., FAZLS CHURCH, LA
TELEPHONE NO. (703) 641-4225 E-MAIL ADDRESS: hmiddleton@redanthe.com
3. WHOM DO YOU REPRESENT, IF OTHER THAN YOURSELF? applicant
4. WHAT IS YOUR POSITION ON THE ITEM?
FOR: AGAINST: _____ OTHER: _____
5. NATURE OF YOUR INTEREST IN ITEM (PROPERTY OWNER, ATTORNEY, LOBBYIST, CIVIC INTEREST, ETC.):
attorney
6. ARE YOU RECEIVING COMPENSATION FOR THIS APPEARANCE BEFORE COUNCIL?
YES NO _____

This form shall be kept as a part of the permanent record in those instances where financial interest or compensation is indicated by the speaker.

A maximum of three minutes will be allowed for your presentation. If you have a prepared statement, please leave a copy with the Clerk.

Additional time not to exceed 15 minutes may be obtained with the consent of the majority of the council present; provided notice requesting additional time with reasons stated is filed with the City Clerk in writing before 5:00 p.m. of the day preceding the meeting.

The public normally may speak on docket items only at public hearing meetings, and not at regular legislative meetings. Public hearing meetings are usually held on the Saturday following the second Tuesday in each month; regular legislative meetings on the second and fourth Tuesdays in each month. The rule with respect to when a person may speak to a docket item at a legislative meeting can be waived by a majority vote of council members present but such a waiver is not normal practice. When a speaker is recognized, the rules of procedures for speakers at public hearing meetings shall apply. If an item is docketed for public hearing at a regular legislative meeting, the public may speak to that item, and the rules of procedures for speakers at public hearing meetings shall apply.

In addition, the public may speak on matters which are not on the docket during the Public Discussion Period at public hearing meetings. The Mayor may grant permission to a person, who is unable to participate in public discussion at a public hearing meeting for medical, religious, family emergency or other similarly substantial reasons, to speak at a regular legislative meeting. When such permission is granted, the rules of procedures for public discussion at public hearing meetings shall apply.

Guidelines for the Public Discussion Period

- (a) All speaker request forms for the public discussion period must be submitted by the time the item is called by the City Clerk.
- (b) No speaker will be allowed more than three minutes.
- (c) If more speakers are signed up than would be allotted for in 30 minutes, the Mayor will organize speaker requests by subject or position, and allocate appropriate times, trying to ensure that speakers on unrelated subjects will also be allowed to speak during the 30 minute public discussion period.
- (d) If speakers seeking to address council on the same subject cannot agree on a particular order or method that they would like the speakers to be called on, the speakers shall be called in the chronological order of their request forms' submission.
- (e) Any speakers not called during the public discussion period will have the option to speak at the conclusion of the meeting, after all docketed items have been heard.

SPEAKER'S FORM

DOCKET ITEM NO. 14, 15, 16

**PLEASE COMPLETE THIS FORM AND GIVE IT TO THE CITY CLERK
BEFORE YOU SPEAK ON A DOCKET ITEM.**

PLEASE ANNOUNCE THE INFORMATION SPECIFIED BELOW PRIOR TO SPEAKING.

- 1. NAME: David Phillips
- 2. ADDRESS: 2726 Franklin Court, Alexandria Va
- TELEPHONE NO. 703 824 9437 E-MAIL ADDRESS: davidgphillips1@gmail.com
- 3. WHOM DO YOU REPRESENT, IF OTHER THAN YOURSELF? myself
- 4. WHAT IS YOUR POSITION ON THE ITEM?
FOR: _____ AGAINST: _____ OTHER:
- 5. NATURE OF YOUR INTEREST IN ITEM (PROPERTY OWNER, ATTORNEY, LOBBYIST, CIVIC INTEREST, ETC.):
property owner
- 6. ARE YOU RECEIVING COMPENSATION FOR THIS APPEARANCE BEFORE COUNCIL?
YES _____ NO

This form shall be kept as a part of the permanent record in those instances where financial interest or compensation is indicated by the speaker.

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