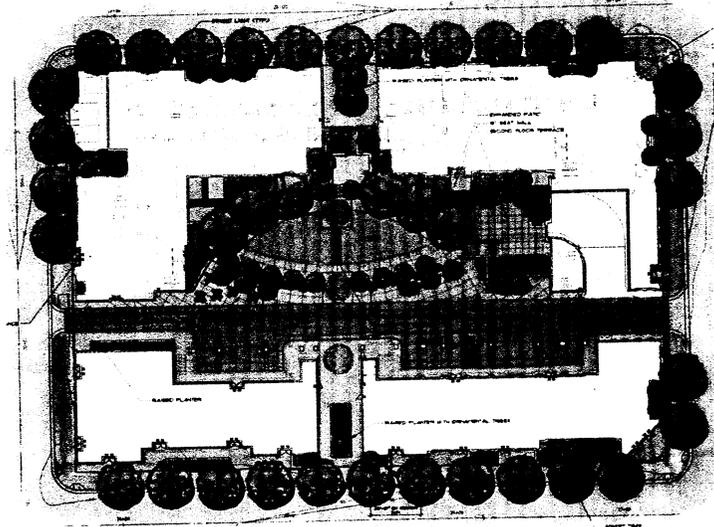


DEVELOPMENT SPECIAL USE PERMIT #2003-0019 THE MONARCH (HENNAGE PRINTING)



CITY OF ALEXANDRIA
PLANNING COMMISSION
NOVEMBER 4, 2004

Charlotte Landis, 433 N. Patrick St. and representing the Inner City Civic Association, spoke in support of the project with the staff recommended changes, noting the improved livability along Route 1.

Amy Harris-White, 621 North West St. and Inner City Civic Association, spoke in support of the project with the staff recommended changes, noting improvements such as hiding the substation, replacing an abandoned building and adding good new businesses to the area.

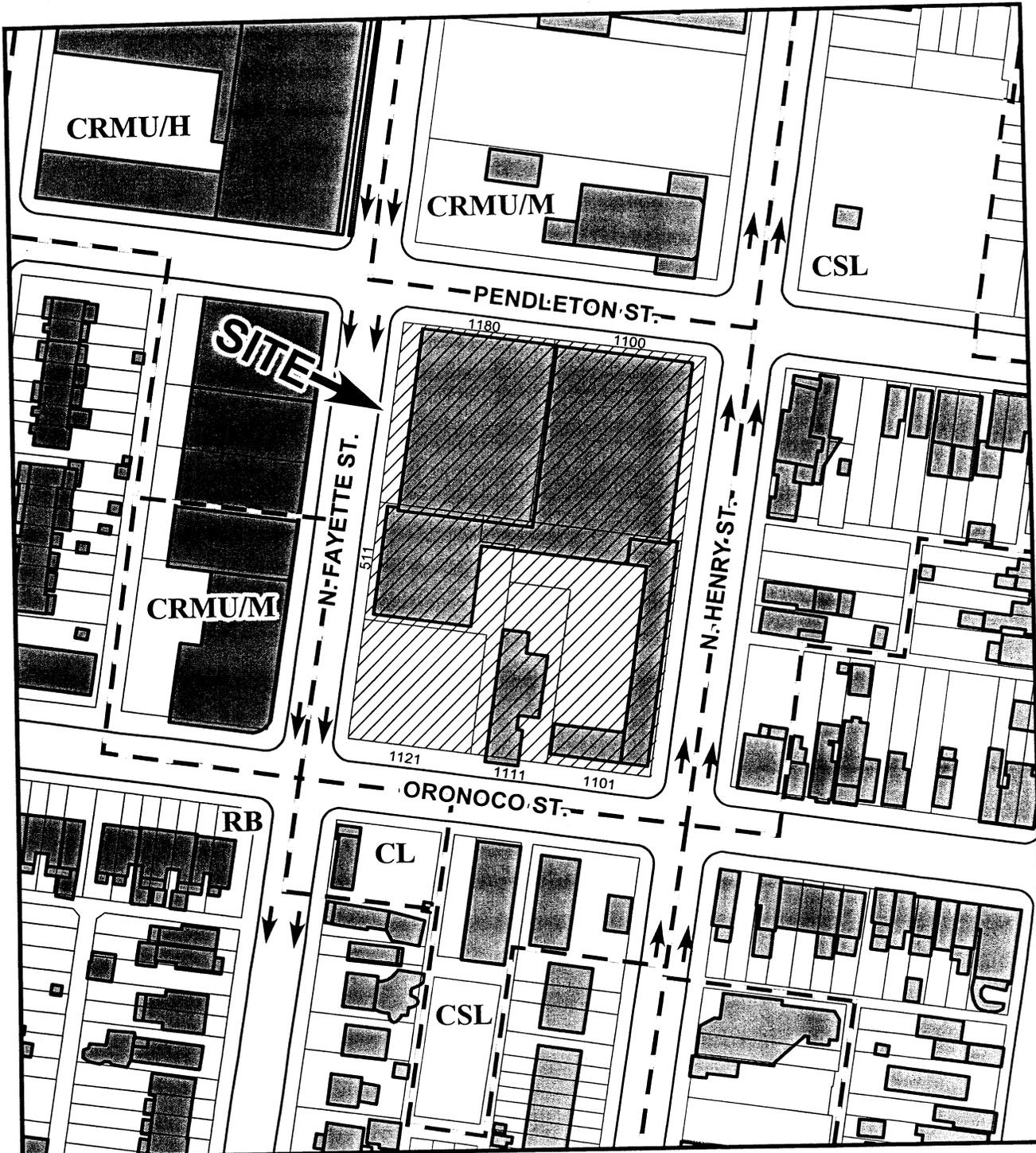
Richard Hobbs, 419 Cameron St. and Old Town Civic Association, spoke in support of the project with the staff recommended changes and modifications and with five affordable housing units. He noted the improvement to the block with the building breaks presented, as well as the public open space, neighborhood retail and replacement of the Hennage building.

Buzz Retting, 719 N. Fayette St. and Braddock Lofts, spoke in support of the building and redevelopment, and also noted increased job opportunities that would be provided by the project.

J. Glenn Hopkins, 1224 Princess St. and Hopkins House, spoke in support of the project with five affordable housing units, ground floor businesses that provide local employment opportunities, and the accessible park/courtyard that children can use as a safe, enclosed space.

Noel Shepherd, 4711 Eisenhower Ave. and business owner, spoke in support of the project with the provision of office spaces, local business opportunities and affordable housing.

Sarah Becker, 1200 Princess St., spoke in general support of the project, emphasizing that the project be subject to future conditions mandated by the Braddock Road Metro Area study. She also noted appreciation of the traffic calming, which was clarified by Tom Culpepper as bulb outs on Henry and Princess Streets, not Henry and Queen Streets.



DSUP #2003-0019

11/04/04



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I. EXECUTIVE SUMMARY

A. Overview:

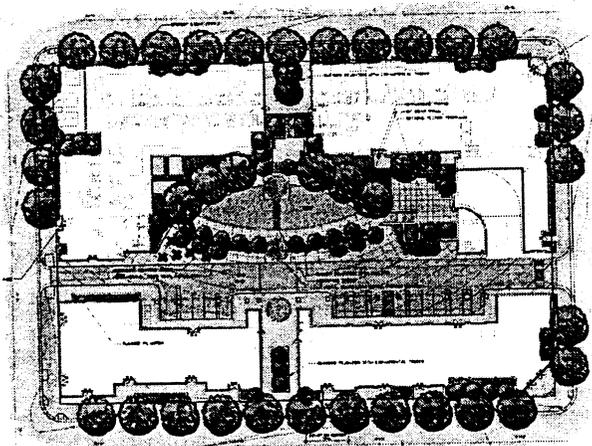
The applicant is requesting approval of a development special use permit to allow an increase in floor area from 1.5 to 2.33 to construct a mixed-use development with 16,000 sq.ft. ground floor retail and 168 residential condominiums. The proposed three to six-story building will be located on an entire city block surrounded by North Henry, Oronoco, North Fayette, and Pendleton streets. Parking is located within a two level below-grade parking garage accessed from Pendleton Street. The site currently contains the vacant Hennage printing building, a carwash facility, and a 10,000 sq. ft. utility substation. The site is approximately three blocks (1,000 ft.) from the adjacent Braddock metrorail station, which provides an opportunity to create a pedestrian-oriented mixed use development near a transit stop.

The adjoining area is characterized by primarily commercial and light industrial uses on the western and northern portion of the site and smaller scale residential townhouses on the eastern and southern portion of the site. The Master Plan anticipates a mixed-use development for the site, and provision of a transition in scale to the established low-scale residential neighborhoods on the southern and eastern portion of the site.

A challenge to redeveloping this block is integrating the proposed development into the existing fabric



Site Area Context



Prospective of Proposed Henry St. Elevation

and character of the surrounding neighborhoods, including the lower-scale townhouses on the southern and eastern portion of the site. The redevelopment of the site presents several challenges and opportunities that include:

Challenges

- Providing a mass and scale that are compatible with the adjoining neighborhoods;
- Ensuring that the building is well designed as one of the first significant recent redevelopment sites in the area;
- Maintaining a level of ground level open space and “openness” for the development; and
- Minimizing traffic and parking impacts.

Opportunities

- Redevelopment of a site that has been identified by the Master Plan to be redeveloped;
- Pedestrian and streetscape improvements;
- Affordable housing;
- Mixed-use development within close proximity (1,000 ft.) of a transit stop;
- Ground level open space, openness and publically accessible open space;
- Neighborhood serving retail uses; and
- Underground parking.

Staff supports the redevelopment of the site and the provision of a mixed-use development with underground parking in close proximity to the Braddock Road metro station. As part of the June 2004 Planning Commission worksession, the Commission provided clear direction to the applicant to reduce the building’s overall mass, provide building breaks, revise the building design to be more compatible with the City, provide affordable units, and provide ground-level open space. The applicant has addressed many of these issues such as providing building breaks, revising the building design and providing ground-level open space. However, additional refinement to the mass, building design and open space, and provision of affordable housing are needed as discussed below.

Therefore, the staff recommendation of approval is based upon additional changes in the building mass, building and open space design, and provision of additional affordable units.

B. Issues Addressed with the Staff Recommendations:

Building Design/Mass and Scale:

The goal of the Master Plan is to encourage redevelopment in a way that is compatible with the modest scale and character of the Parker Gray District and adjoining neighborhoods. The applicant has worked with staff and the community to provide full building breaks on Henry and Fayette streets. The applicant has also worked with staff on the overall design of the building so they appear as a collection of buildings that have been constructed organically over time, typical of many streets within Alexandria.



Oronoco Street south of the site

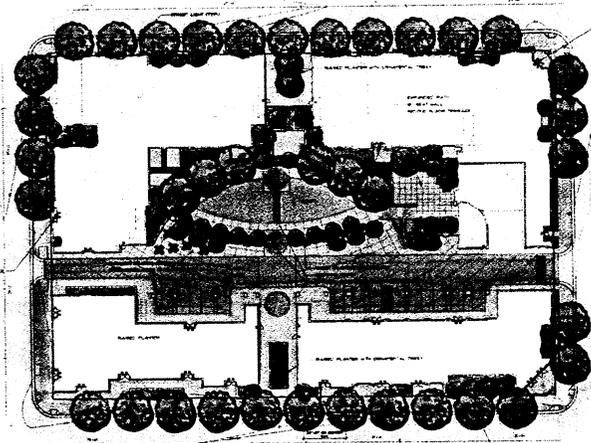
However, staff remains concerned with the massing on North Henry Street and the massing at the corner of Oronoco Street and North Fayette Street, the two locations where the proposal abuts the more traditional scale of Parker Gray and the primarily 2- to 3-story buildings on North Henry Street. To address this issue, staff is recommending that approximately 5 units be eliminated to create a better transition in scale to the adjoining neighborhood and primarily 20 ft. to 25 ft. tall buildings that are typical of the buildings on North Henry Street and the buildings within Parker Gray. By reducing the overall mass and revising the design, the proposal will be more compatible with the existing character and development pattern of the neighborhood and building styles more typical of Alexandria. Staff is also recommending the building design be revised to be more similar to building types and forms typically found in Alexandria.



Townhouses across N. Henry Street

Open Space and Streetscape:

The applicant is providing a significant amount of ground-level open space, most of which is consolidated ground-level open space within a central courtyard that is accessible and visible from each of the four adjoining streets. At the request of the City, the applicant has agreed to provide a public access easement for the internal pedestrian connections and the internal courtyard park to enable this space to function as a neighborhood pocket park for the adjoining residents and the City. Because of the adjoining retail uses, the courtyard space could also serve as an area for outdoor dining and other uses that could help to activate the space. More specifically, conditions have been included to:



- Provide a public access easement for the internal courtyard;
- Public art or sculpture with the public courtyard open space;
- Relocate the existing above-grade utilities below grade;
- Minimize and screen the existing electrical substation;
- New 6 ft. to 10 ft. wide unobstructed sidewalks;
- Stamped asphalt pedestrian crosswalks;
- A bus shelter on Pendleton Street;
- Pedestrian scale lighting for each street frontage and the internal courtyard;
- Public benches and trash receptacle for the street frontages and internal courtyard;
- Bicycle racks; and
- Additional landscaping and amenities within the central courtyard.

With these conditions, the proposed development will provide a significant amount of pedestrian improvements and a useable consolidated area of open space pocket park for the community, residents and proposed retail uses.

Affordable Housing

The applicant initially proposed to provide three on-site affordable units valued at \$475,000 or \$2.02 per gross square foot. Staff recommended that the applicant provide six affordable units. At its October 7, 2004 meeting, the City's Affordable Housing Advisory Committee considered the applicant's proposal and an alternate staff proposal for six on-site units. The committee voted

to recommend that the Planning Commission and City Council seek to increase the number of units offered (three), and directed staff to work with the applicant. The applicant has since offered four affordable units. Staff recommends that the applicant provide five affordable units, 3 two-bedroom units and 2 one-bedroom units.

C. Community

The applicant and staff conducted numerous community meetings for this project with the Inner City Civic Association, Upper King Street Neighborhood Association, and the Old Town Civic Association. In addition, staff held a work session with the Planning Commission in June 2004 to discuss issues related to the proposed redevelopment of the site. The redevelopment of the site has been positively received, although issues identified by the community include:

- Ensuring publically visible and accessible open space;
- Minimizing the traffic impacts and making the street more pedestrian friendly;
- Making the development compatible with the neighborhood;
- Providing locally owned and operated, neighborhood-serving retail;
- Affordable housing; and
- Setting an appropriate precedent for future redevelopment.

To address these concerns, staff has included conditions requiring a public easement ground-level open space, 6 ft. to 10 ft. wide unobstructed sidewalks with street trees and landscaping, reduction in mass on the eastern and southwest portion of the site, limiting personal service and office use in the retail space, and provision of five affordable dwelling units.

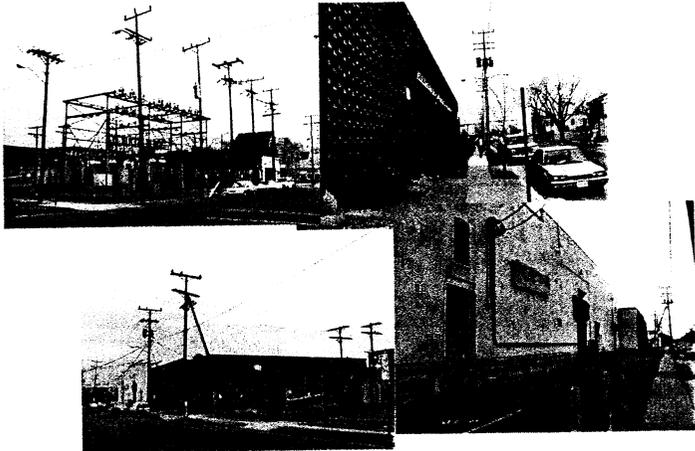
D. Conclusion

Staff recommends approval of the proposal contingent upon conditions to make the mass more compatible with the scale of the neighborhood, to refine the design of the building and open space, and to increase the contribution of affordable dwelling units within the project.

II. BACKGROUND:

Site Description:

The site is surrounded by one and two-story buildings that consist of a mix of residential and commercial uses, as well as some light industrial uses. The blocks to the north and west of the site consist of commercial/office development, warehouse and light industrial uses that will likely redevelop in the future. The blocks to the south and to the east consist of well established primarily residential neighborhoods.

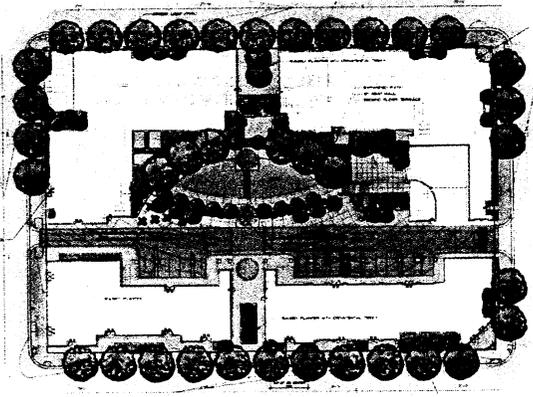


Existing Site Conditions

The site consists of six parcels totaling 87,174 sq. ft and contains a vacant printing service building, a Dominion Virginia Power substation and a former car wash. The site is essentially flat and almost entirely covered with impervious surfaces. The property is zoned CRMU-H/Commercial Residential Mixed Use (High).

Proposal:

The 168-unit residential condominium building will consist of 76 one-bedroom units and 92 two-bedroom units. The building will be three to six-stories in height. The proposal will demolish the existing printing building, the carwash facility and the electrical substation on the site. The proposal reduces the footprint of the Dominion Virginia Power substation at the southwest corner of the block from 10,000 sq. ft. to a 1,000 sq. ft., as well as provides screening for the remaining smaller substation.



While internally the proposed building will function as one multi-family building, externally the building will appear as a series of seven buildings. The building faces are articulated in a series of vertical bays, with portions of the buildings set back providing visual interest and providing additional areas for landscaping, open space and courtyards between the buildings and the

adjoining public streets. The primary entrances to residential buildings are located on Oronoco Street in the middle of the block and at the corner of Pendleton and North Fayette Streets. The proposed development will be constructed over a two-level underground parking garage. A total of 374 parking spaces will be provided and will consist of the required residential parking (260 spaces), plus 39 (15%) visitor parking spaces. The retail parking spaces will be located on the first floor of the garage and residential parking spaces will be located on the bottom floor of the garage, with a gate to separate the retail/visitor spaces from the residential spaces. Controlled access will be provided for the underground garage.

The applicant is proposing a north-south central "alley" that is accessed from Oronoco and Pendleton streets. The proposed central alley is typical of many blocks within the City. The "alley" will provide access to the underground parking and the 15 surface spaces for parking and loading adjacent to the proposed retail use on North Henry Street. Vehicle access on the central and southern portion of the alley will be limited to the retail patrons and delivery vehicles. Otherwise, this portion of the alley will primarily serve as pedestrian access. The applicant has provided an on-site loading space accessed from the internal private alley. Areas for trash are located within the building at the north and south end of the new internal alley.

Evolution of the Plan:

The project has evolved significantly since the initial proposal by the applicant. Overall the height of the building decreased from over 70 feet to a range of 40 to 60 feet proposed. Open space has moved from being on upper-story rooftop decks that are entirely private and not visible to the public, to the current project which provides the majority of open space at-grade both visually and physically accessible to the public. Two building breaks are now proposed on Henry and Fayette Streets. Each of the buildings on Oronoco and Pendleton Streets have been sectioned further by a 30' deep setback in the structure above an at-grade alley that opens up views and connections to the central courtyard that functions as an architectural hyphen between the buildings. The design of the building has grown from one large monolithic structure of the same architectural design around the block, to the appearance of a series of separate buildings, grouped together in a manner that begins to emulate more closely the style and building widths of traditional development in the historic parts of the City.

III. ZONING:

The property is zoned CRMU/H which provides for an urban mix of commercial retail space and residential dwelling units. The applicant is requesting a special use permit to increase the floor area from 1.5 to 2.33.

HENNAGE-MONARCH		
Property Address:	1180 & 1100 Pendleton St., 1121, 1111 & 1101 Oronoco St. and 511 N. Fayette St.	
Total Site Area:	87,174 square feet (2.0 acres)	
Zone:	CRMU-H/Commercial Residential Mixed Use (High)	
Current Use:	Printing facility (vacant), car wash, and electrical power substation	
Proposed Use:	Retail/Residential condominium	
	<u>Permitted/Required</u>	<u>Proposed</u>
Floor Area	217,935 s.f.	203,355 s.f.
FAR	1.5; 2.5 w/SUP	2.33
Yards	n/a with mixed use	0 -14'
Zone Transition	20' (RB Zone)	0' (modification requested)
Height	77'	40 to 62.5'
Open Space	40%	30% at grade 12.8% rooftop 42.8% total
Parking		
Residential:	260	260
Residential Visitor (15%):	39	39
Retail:	73	75
Total:	372 spaces	374 spaces

IV. STAFF ANALYSIS:

The proposed redevelopment of this vacant and former light industrial site presents an opportunity to redevelop an entire City block, which is located only three blocks from the Braddock Metro station. The former light industrial uses on the site are remnants of the industrial uses that were once typical of this area. Staff anticipates that this is one of the many industrial or warehouse uses that will be redeveloped within this area in the next several years.

The proposed mix of land-uses (residential and retail) will enable neighborhood serving retail uses and residential uses within a reasonable walking distance to the Braddock Road Metro station, both of which will be able to utilize this important transportation amenity within the City. The proposed mix of uses, combined with a useable and consolidated central courtyard will create what staff believes will be a vibrant urban development consistent with the City's vision. The central open space with landscaping, amenities and elements such as public art or a water fountain will create a pocket park or "outdoor room" which will provide a useable area to bring people together through casual encounters, increasing daily exchanges and a sense of community for this portion of the City.

Staff supports redevelopment of the site and a mixed-use development for the site; however, staff does not support redevelopment at the current mass and scale, which is too large for the scale of the existing neighborhood. The challenge for this site has been to integrate redevelopment of this site with the existing low-scale uses that surround the site and are anticipated to remain. The applicant has worked with staff to provide many of the necessary elements such as building breaks and designing the buildings to appear as separate buildings. However, staff is recommending the reduction in the mass of the building on Henry, Oronoco and Fayette streets to address to ensure that the proposed development becomes more compatible with the existing development pattern on the southern and eastern well established residential neighborhoods as discussed below.



*General Comparison of Building on N. Henry Street
(w/ staff changes) to Braddock Lofts Townhouses*

With the proposed reductions in mass and refinement to the architecture of the buildings, the scale of this proposal will be dramatically different than the 1-2 level warehouses that exist on the site today. Staff has added conditions to continue to refine each of these elements. Even with the reductions in mass recommended by staff, the buildings will be larger than the adjoining two

level townhomes, although the transition in scale becomes more compatible. This proposal will also be considerably larger than the 1-2 level warehouse buildings on the northern and western portion of the site. Redevelopment of this site requires a delicate balance between mass, scale, height and building design. Staff has added conditions to address each of these areas to provide a better balance between the existing conditions, existing townhouses and the mix of uses that is envisioned by the Master Plan and permitted by the current zoning with a special use permit. In addition to mass and scale, staff has added conditions to ensure that the overall design and materials of the building, open space and streetscape improvements are of the highest quality because of the scale, adjoining uses, adjoining Parker Gray district and the visually prominent location on Henry Street.

The other remaining area of staff concern is the provision of an adequate amount of affordable housing. As part of the special use permit, the applicant is requesting an increased floor area ratio (FAR) from 1.5 to 2.33. Therefore, staff has recommended that the applicant provide five affordable units, which equates to approximately \$1,050,000 or \$4.28 per sq. ft. The applicant is proposing four units as discussed in more detail below.

Staff believes that with recommended changes to the building design, massing, affordable housing and open space, the proposed development will be more compatible with the Braddock Road Metro and Parker Gray neighborhoods.

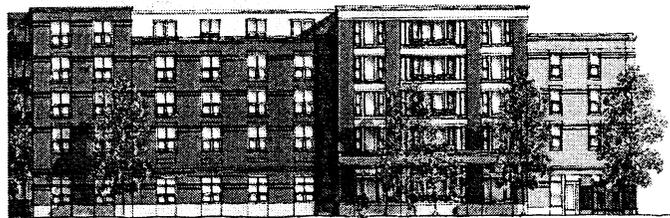
A. Building Design - Scale.

A goal of the Master Plan is to provide development that ensures height, mass and density transitions between established low scale residential areas and areas of more intense commercial redevelopment. This is of critical importance on the southern and eastern portions of the site due to the scale of the adjoining residential townhouses and established residential neighborhoods.

In addition, one of the character defining elements of this neighborhood and the City is the architectural quality, variety and texture of the building styles, the variation in massing and heights that occurs within each block. Because the buildings within each block developed over time, the result is that each block contains a collection of



Henry Street Facade without Staff Recommended Changes



Fayette Street Facade without Staff Recommended Changes

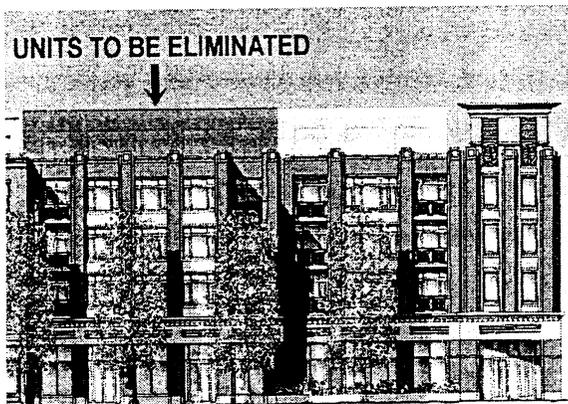
varied buildings, architectural styles and roof forms and this variety is a character-defining element of this neighborhood.

In this case, the applicant is redeveloping an entire City block at one time as a single project. To ensure that the proposal is more compatible with the character of the neighborhood, staff has worked with the applicant to design the project as a series of varying architectural styles with the insertion of two full building breaks on North Fayette and Henry Streets, and large gaps on Oronoco and Pendleton Streets where the discontinuity between building elements can be emphasized.

The remaining issue of concern for staff is reducing the mass of the building in two key areas where the height of the building continues to be too great given the sensitivity of the context: one is a portion of building fronting Henry Street near Pendleton Street; the other is at the corner of Oronoco and Fayette Streets. Staff continues to recommend the elimination of the top portion of elements of the buildings on each street frontage (from five-stories to four in both cases) to create a building height that is more compatible with the existing buildings, the scale on North Henry Street and to provide a better transition to the Parker Gray neighborhood at the corner of Oronoco and Fayette Streets.

Henry Street

The Henry Street elevation is the longest and is intended to create a transition from the scale of the Parker-Gray neighborhood to the south and east to the larger buildings approaching Braddock Metro. The facade has a considerable amount of building articulation that ranges from



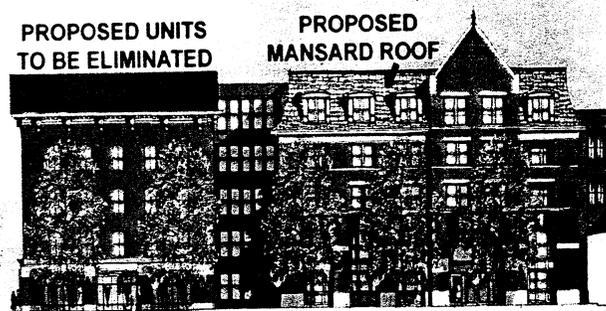
Proposed Henry Street Elevation with Staff Recommendation

6 ft. to 14 ft. from the property line. The building elements on the southern portion of the site are intended to emulate traditional mixed-use buildings on King Street or other areas within the City. The building on the northern portion of the facade is an Art Deco facade, recalling a more industrial scale reminiscent of the early 20th century. Staff believes that a sheer height of six-stories along North Henry Street across from the lower scale residential is too imposing, and therefore recommends the elimination of the upper level on the northern warehouse-type building element to reduce the overall mass. In addition, staff recommends the continued refinement of the corner tower as a defining element of the project, particularly as seen from the approach from the north on Henry Street.

THE MONARCH (HENNAGE PROPERTY)

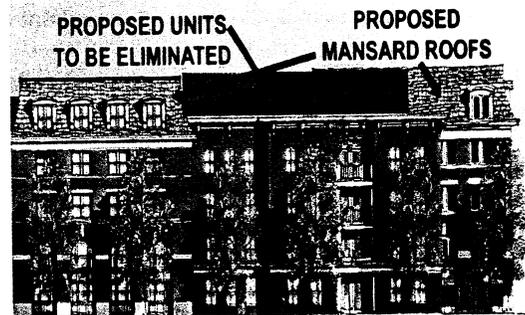
Oronoco and Fayette Streets:

The Oronoco Street Facade is one of the shortest elevations and contains the principal entrance for the condominium building on the south half of the site. The proposed building consists of three sections facing Oronoco Street: the three-story, mixed-use element from the corner at Henry Street; the central portion which contains the entrance in a Queen Anne or late Victorian mixed-use building; and a 5-story level commercial type building at the corner of Fayette Street. The three buildings are connected by two architectural hyphens that serve as internal walkways for the units. The hyphens are setback approximately 30 feet from the face of the adjacent buildings and reinforce the impression of separate architectural pieces. The overall composition of mixed-use multi-family buildings is a reasonable approach but the scale of the proposed 5-story element at the corner of Oronoco and Fayette Streets is too imposing for the adjacent neighborhood, both in its height approximately 50 ft. feet and in the monumental language of three-story columns, large-scale windows, heavy entablature, etc.



Proposed Oronoco Street Elevation with Staff Recommendations

Therefore, staff recommends that the corner element be reduced in its visual impact, both by modifying the style to a less grandiose expression and by eliminating a portion of the fifth floor so that the building will appear as a four-story building from the adjoining streets. This modification will result in the elimination of approximately three units. The elimination of the units also occurs on the accompanying Fayette Street facade and is essential to enable a better transition to the adjoining two-story buildings.



Proposed N. Fayette Street Elevation with Staff Recommendations

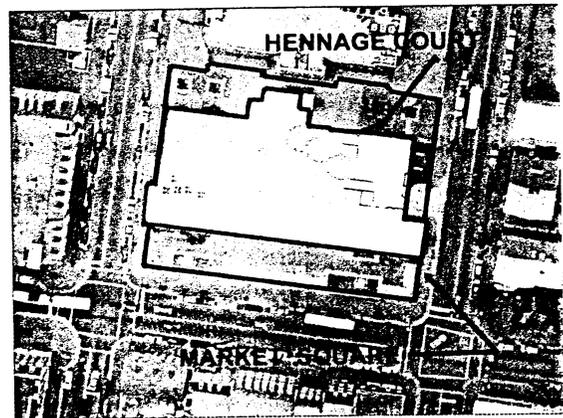
B. Green Building

The applicant has committed to environmentally-sensitive methods of building and operation. There is not any one single technique for designing and building a green building to these specifications, but certified buildings often preserve natural vegetation, contain non-toxic or recycled-content building materials, maintain good indoor air quality, use water and energy efficiently, conserve natural resources, feature natural lighting, and recycled construction materials.

C. Open Space - Pedestrian Improvements

The proposal includes approximately 30% ground-level open space and a total of 42.8% open space including the roof-top open space. The ground level open space and “openness” displays a vast improvement compared to the limited open space initially proposed by the applicant. The largest portion of the open space is provided in the interior courtyard which is approximately 40% of the size of Market Square plaza in front of City Hall or 140 % of the Hunter/Miller Park at the intersection of Queen and N. Fayette Streets in Parker Gray.

The ground-level open space is accessible and visible from the public streets and consists of a mixture of plaza/courtyard, outside seating, benches, and landscaping. The space will be visible from all four streets surrounding the block. From North Henry Street and North Fayette Street, the space will be visible through 26 feet to 32 feet wide building breaks that will lead into the courtyard. From Pendleton and Oronoco streets, the courtyard will be visible through the ground level alley entrances that will also support sidewalks leading to the space. At the request of the City, the applicant has agreed to provide an



open space easement for the ground level courtyard and pedestrian connection to enable the ground level open space to function as a public pocket park. This will provide an additional public open space pocket park for the community. In addition, staff has included recommendations to enhance the quality of the open space, streetscape and pedestrian realm which include:

- A public access easement for the 23,000 sq. ft. open space courtyard and pedestrian connections;
- Public art or sculpture with the public open space area;
- Existing utilities on N. Fayette, N. Henry, and Oronoco Streets be located below grade; 6 ft., wide brick sidewalks, landscape strip and street trees for Oronoco, Fayette and Henry Streets and 10 ft. wide sidewalks on Henry Street;
- Bus Shelter on Oronoco Street;
- Pedestrian scale lighting;
- Stamped and colored asphalt crosswalks at the intersections of Fayette and Oronoco streets and Fayette and Pendleton streets;
- Decorative benches and trash receptacles for each street frontage;

- Bicycle parking for the retail patrons and residents of the condominium building;
- Irrigation for the open space;
- Decorative paving for the internal “alley” and courtyard;
- Continual row of street trees on the perimeter of the site; and
- Features for the roof-top open space such as seating, trash receptacles, and pedestrian-scale lighting, high quality paving, additional landscaping and irrigation.

One of the issues that the applicant has discussed with staff is the possibility of eliminating the internal “alley” to nearly all of the required open space at the ground level. However, the “alley” is typical of most blocks in Alexandria, provide some limited short-term retail parking. More importantly for any public outdoor space to be successful, there must be a level of activity that enlivens and activates the space through the activity of the retail patrons and pedestrians.

While this ground-level open space is considerable, the overall effectiveness of this space as a pocket park for the community and retail uses will depend on the quality of the courtyard design, materials, maintenance, and amenities within the space. To address these issues, staff is recommending the interior courtyard be redesigned in a more traditional or formal manner with a central focal feature such as a fountain or public art sculpture, additional trees and amenities, decorative paving and less hardscape in consultation with the adjoining residents. While the open space is publicly accessible, the space will be privately maintained by the condominium association and retail tenants of the development.

D. Braddock Road Metro Study

The site is one of the largest redevelopment sites within the Braddock Road Metro area. One of the questions that has been raised is whether the project should be permitted to proceed ahead of the upcoming Braddock Road Metro study. Staff acknowledges that with large development projects, the City has generally encouraged them to process concurrently with the long-range planning processes such as the East Eisenhower plan. However, in this case, the applicant has been working with the City and the community for approximately 12 months and has provided many of the elements such as underground parking, mixed-use, pedestrian and streetscape improvements etc., that staff believes will be part of the upcoming Braddock Road Metro plan. In addition, the current issue of the transition of the mass and scale to the neighborhood are something that would be part of any subsequent application. Therefore, staff believes it is reasonable to process this application ahead of the completion of the upcoming Braddock Road Metro study. In addition, staff has included a condition requiring the proposed development to participate within any special districts or programs that are a result of the Braddock Road Metro study.

E. Parking

The project will provide all of the required retail and residential parking, including 15% visitor parking spaces within the below grade parking garage and on the private alley behind the retail space. Parking along the perimeter of the block consists of 2 or 3 hour parking. The block lies outside Residential Parking District 3 which begins to the west and south of the site on the 1200 block of Oronoco Street and the 400 block of N. Fayette Street. To encourage use of the underground parking garage and discourage new residents from parking on the street, staff is recommending that residents of the new condominiums not be permitted to obtain a residential parking permit.

The applicant has discussed the possibility of reducing the amount of parking spaces because of the proximity to the adjoining metro station. Staff acknowledges that in the past, the City has supported a parking ratio as low as 1 space for each unit for residential projects that are in close proximity to a metro station such as the Whole Foods and Mill Race developments. In comparison, the applicant is providing the required number of parking spaces for each units which equates to 1.8 spaces for each unit. The applicant is also providing an additional 15% visitor parking within the underground parking garage. However, staff is concerned that a lower parking ratio will encourage spillover parking into the primarily residential neighborhoods that adjoin the site. In addition, while many of the residents will utilize Metro, many will continue to own cars and therefore require parking within the underground garage. For all of these reasons, staff does not support a reduction in the required parking at this location.

F. Traffic

Based upon the applicant's traffic assessment report, this development will generate approximately 1,273 trips per day (637 entering and 636 exiting). During the AM peak hour, an estimated 68 trips will be generated, while during the PM peak hour 109 trips will be generated. At build out in 2005, site-generated traffic is expected to constitute one to one and one-half percent of the traffic volumes on Henry Street. The development is also expected to increase the traffic on the adjoining streets of Oronoco, Fayette Streets and Pendleton Streets by less than 100 vehicles. During both peak hours, these volumes are well below the operating capacity of these streets.

At build out, site generated traffic will increase traffic demand at the four adjacent intersections by no more than four percent during either peak hour's. The intersections of Henry and Oronoco and Henry and Pendleton currently operate at Level of Service B during both the AM and PM peak hours. At build-out, these levels of service are expected to continue.

Staff concludes that, alone, the Monarch development will not appreciably impact traffic in the surrounding neighborhood. However, there are a number of other properties in the area that will also redevelop in the future. The cumulative affect of all redevelopment in the area may have a much larger impact on traffic. As this redevelopment occurs, ongoing attention should be given to maintaining a good urban street grid for this local traffic.

The applicant's analysis indicated that 63 vehicles per hour typically violate the Fayette Street "DO NOT ENTER" signs prohibiting traffic from traveling southbound between the hours of 4:30 and 6:30 PM. As the Planning Commission and City Council are aware, a condition of the approval for the Hopkins development was the installation of bulb-outs, landscaping and street trees at the intersection of Fayette and Oronoco Streets, which staff believes will reduce this violation level and existing cut-through traffic on Fayette Street. In addition, staff recommends that the applicant provide funding for the construction of bulb-outs on Queen Street at its intersection with Henry Street to help further reduce cut-through traffic in the area.

G. Affordable Housing

In lieu of a contribution to the City's Housing Trust Fund, the applicant originally proposed to provide 3 on-site units for sale to purchasers eligible for the City's housing assistance programs. While staff appreciates the offer of units in lieu of a contribution, staff did not believe that the 3 units offered were sufficient for the size of the project. Staff proposed 6 units on-site. At its October 7 meeting, the City's Affordable Housing Advisory Committee considered the applicant's proposal and the staff proposal. After considerable discussion, the Committee did not wish to support either proposal. Rather, the members voted to recommend that the Planning Commission and City Council seek to increase the number of units offered and directed staff to continue to negotiate with the applicant to reach a compromise solution.

In subsequent negotiation, the applicant proposed to provide 4 units: 2 one-bedroom units; 1 one-bedroom plus den unit; and 1 two-bedroom unit. Staff proposed that the applicant provide 5 units: 2 one-bedroom units; and 3 two-bedroom units.

The applicant values its contribution at \$675,000 to \$700,000, based on its estimated market sales prices of \$300,000 per unit for the one-bedroom units versus the discounted sales price of \$175,000, for a total discount of \$250,000; \$375,000 for the one-bedroom with den unit for a discount of \$200,000 (the applicant assumed the unit would also sell for \$175,000); and \$450,000 to \$475,000 for the two-bedroom unit, for a discount of \$225,000 to \$250,000. The applicant says this equates to a contribution value of \$2.80 to \$2.90 per gross square foot.

Staff still believes that five units are more appropriate for this site for the following reasons:

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- The applicant proposed to increase the gross square footage from approximately 147,118 to 245,197 with a Special Use Permit. This would allow the addition of between 68 and 72 units to the site, a 66.6% increase in floor area. Even though staff recommends a somewhat smaller building than that proposed, the increase through the SUP is substantial.
- The staff alternative of 3 two-bedroom units and 2 one-bedroom units would have a total discount value of \$975,000 to \$1,050,000, which would equate to a contribution value of \$3.98 to \$4.28 per gross square foot, a figure more in line with other recent projects. At a Planning Commission Work Session on this project in June, Planning Commissioners directed the applicant to provide a significant on-site affordable housing component in its plan. Although both the applicant's proposed density and the density to be recommended by Department of Planning and Zoning staff are within the maximum allowed within an SUP in the project's CRMU-H zone, the project nevertheless stands to receive a substantial increase over the base density. Therefore, City staff feel that 5 units (3% of the total 164 units) is a reasonable level for this site.

Comparison of Affordable Housing

Project	Type	Units	Aff. D.U.s	% of Units	Value of Contribution	Contrib. per s.f.	% of Standard Contribution
Mill Race	Sales	326	13	4.0%	\$1,229,600	\$2.01	201%
Mill Race	Rental	369	15	4.1%	*	*	*
Preston	Sales	63	6	9.5%	\$100,435	\$1.04	104 %
Cameron Station Condos	Sales	148	7	4.7%	\$975,000	\$2.07	207 %
Northampton	Rental	572	25	4.4%	\$1,605,680	\$2.73	273 %
Park Center	Sales	173	9	5.2%	\$720,000	\$3.48	348 %
Prescott	Sales	64	4	6.3%	\$451,800	\$5.87	587 %
Monarch -Hennage	Sales	168	5	3.0%	\$1,050,000	\$4.28	428 %

* Note – figures not available, but the \$2.01 per gross square foot contribution value applies to the entire Mill Race project, including the office building.

H. Community

Staff and the applicant conducted community meetings for this project with the Inner City Civic Association, Upper King Street Neighborhood Association and the Old Town Civic Association. In addition, staff held a work session with the Planning Commission in June 2004 to discuss issues related to the proposed redevelopment of the site. The primary issues identified by the community include:

- Minimizing the traffic impacts and making the street more pedestrian friendly;
- Reducing the mass and scale of the building;
- Ensuring public visible and accessible open space;
- Providing locally-owned and operated, neighborhood-serving retail;
- Affordable housing; and
- Setting an appropriate precedent for future redevelopment.

The development process has been one of community input, and the community has had an opportunity to get involved and shape their neighborhood. There are numerous changes and revisions to the proposal that have made the plan better through active citizen involvement.

V. Compliance With City Vision:

The proposed development provides a mix of uses, pedestrian and streetscape improvements to the existing block and the neighborhood that are consistent with the Goals of the Master Plan and the recent City Vision adopted by City Council as discussed below.

Creating a Vibrant City - Urban Village

The proposal provides an opportunity to provide a mix of retail and residential uses adjacent to the metro station which will add activity and vibrancy for the adjoining neighborhoods. The residents and a portion of the retail patrons will be able to utilize the adjoining Braddock Metrorail Station which has one of the lowest ridership rates of any station in the system. Staff is also recommending streetscape improvements and amenities such as bus shelters to encourage pedestrian activity. The proposal incorporates many of the fundamental elements of an urban village that include:

- Mixed land use with commercial offices and shops on main spines, surrounded by residential development;
- Public and private ground level open space;
- Underground car parks with access from rear areas;
- Public spaces with strong design features (water, street furniture, sculptures); and
- Pedestrians and use of mass transit are encouraged through the design and mix of uses.

While the proposal incorporates many of the desirable elements such as retail and residential uses, ground-level open space and underground parking, the site will continue to be surrounded by industrial use on the northern and western portion of the site. Therefore, the integration of this site with the community and the adjoining metro will require coordination with these adjoining sites as these redevelop in the future which may be several years or more. Until these adjoining industrial uses are redeveloped the connectivity and vibrancy for the site cannot be fully realized.

With the recommended refinements of the building's mass and scale and the incorporation of high quality architectural materials, the project will be more compatible with the neighborhood; the redevelopment provides affordable housing units; the proximity to the Braddock Road Metro station ensures a highly transit oriented proposal. The project reflects the goals for redevelopment in the City's Master Plan; and both staff and the applicant have met with the community throughout the process and a Planning Commission work session to encourage community participation.

VI. Conclusion:

Staff recommends approval with conditions to address building mass, open space, affordable housing, and the other conditions outlined within the report.

STAFF: Eileen Fogarty, Director, Planning and Zoning
Jeffrey Farner, Chief, Development;
Steve Milone, Urban Planner;
Laura Durham, Urban Planner.

VII. STAFF RECOMMENDATION:

Staff recommends **approval** subject to compliance with all applicable codes and ordinances and the following conditions:

BUILDING DESIGN:

1. **CONDITION AMENDED BY THE PLANNING COMMISSION:** The final architectural elevations shall be consistent with the level of quality and detail provided in the preliminary architectural elevations dated October 4, 2004. In addition, the applicant shall provide additional refinements to the satisfaction of the Director of P&Z that shall at a minimum include:
 - a. The facade materials for the front, side and rear of each building shall be entirely masonry (brick, precast, stone).
 - b. Through-wall HVAC vent grills and any other vents (approximately 22) located on facades adjacent to and facing public streets shall be redesigned or relocated to the satisfaction of the Director of P&Z and shall be shown on the final site plan. Details on the screening methods shall be included in the final site plan submission.
 - c. The retail bays shall continue to be revised to emulate the scale and materials of traditional retail bays as reflected in the historic retail buildings of Alexandria.
 - d. The retail base shall provide low-level pedestrian-scale lighting as an integral part of the facade design to add nighttime visual interest to the buildings.
 - e. The buildings shall be designed to incorporate a variety of traditional fenestration as represented in the submitted plans and refined to match the architectural style emulated in each building.
 - f. Color architectural elevations (front, side and rear) shall be submitted with the final site plan.
 - g. The interior elevations of the buildings shall emulate the design, and be of comparable quality and materials as the front facades. The interior recessed corridor hyphen shall be clad in dark green metal with traditional two-over-two windows.
 - h. All refinements to the design and materials shall be revised prior to the release of the final site plan.
 - i. The applicant shall provide larger scale drawings to evaluate the retail bays, projections, balcony rails, cornice brackets, entrance canopies and sign bands and that the final detailing, finish and color of these elements is critical and must be studied in context with the overall buildings. These detail elements shall be submitted prior to review and approved prior to the release of the final site plan.
 - j. A color on-site mock-up shall be provided prior to the final selection of the brick and building materials.

- k. The balconies shall be constructed of high quality materials comparable to the materials for the primary building facades. The balconies shall not project beyond the plane of the building. Ceilings for the balconies shall be constructed of durable cementitious material or similar high quality substitute.

Henry Street:

- l. The four proposed balconies within building #2 shall be eliminated and replaced with traditional two over two windows as provided within the remainder of the building.
- m. Through-wall HVAC vent grills and any other vents (approximately 22) located on facades adjacent to and facing public streets shall be redesigned or relocated to the satisfaction of the Director of P&Z.
- n. The railing for the roof-top open space for building #2 shall be setback a minimum of 10 ft. from the building facade to ensure that railing is not visible from the adjoining streets or incorporated as part of the building's parapet.
- o. Each bay for building #2 shall provide a variation in brick colors to provide the appearance of separate buildings.
- p. ~~For building #1, two units shall be eliminated on the Henry Street facade as generally depicted in Attachment #1.~~ The Henry Street facade shall be revised as generally depicted in Attachment #1a. ~~shall be revised to provide the appearance of a more traditional style and a three-story element rather than the five-story element as currently depicted. The roof for the upper floor of this element shall be slate or synthetic slate.~~
- q. The recessed upper level shall be a lighter color brick. The fenestration for this level shall be similar to the facades for each "building."
- r. Details of the corner tower element shall be refined to the satisfaction of the Director of P&Z and shall be included in the elevations submitted for review with the final site plan.

Oronoco Street:

- s. The two recessed architectural hyphens shall be a dark green metal cladding with traditional two-over-two windows.
- t. The central and western portion of the facade shall be revised ~~by eliminating two units~~ as generally depicted in *Attachment # 2*. The roof for the upper floor shall be slate or synthetic slate.
- u. The recessed upper level shall be a lighter color brick. The fenestration for this level shall be similar to the facades for each "building."
- v. The screening for the mechanical equipment on the corner shall be constructed of same masonry material as the primary facade.

Fayette Street:

- w. The southeastern portion of the building shall be revised as generally depicted in *Attachment # 3*. The roof for the upper floor shall be slate or synthetic slate.
- x. The base for building # 4 shall incorporate the use of windows proportioned to the size of the base of the building.
- y. A different material or banding shall be provided at the upper level to simulate the appearance of a cornice.
- z. Details of the tower element shall be refined to the satisfaction of the Director of P&Z and shall be included in the elevations submitted for review with the final site plan.

Pendleton Street:

- aa. The two recessed architectural hyphens shall be a dark green metal cladding with traditional two-over-two windows.
2. The applicant shall demonstrate the use of green building and sustainable techniques for building systems design for the project such as low emission paints and high efficiency mechanical equipment to the satisfaction of the Director of P&Z. (P&Z)
 3. All stairs shall extend thru the roof so that door access to the roof is provided for firefighting access. (Code Enforcement)
 4. In lieu of strict compliance with ladder truck access requirements, the proposed massing, articulation of the facade and placement on the lot will include the following fire protection and fire fighting features built into the buildings:
 - a. A corridor smoke ventilation system.
 - b. Enclose all elevator lobbies in smoke tight construction.
 - c. Stair capacity to be designed without taking the sprinklered building exception.
 - d. Fire phones installed on every level in the elevator lobby and the stairs.
 - e. Knock out panels that have an area of 20 feet for every 50 lineal feet of building perimeter.
 - f. Floor drains to allow for quick removal of sprinkler and fire hose water.
 - g. A full fire suppression system designed to NFPA 13 standards regardless of occupancy type. (Code Enforcement)

RETAIL USES AND SIGNAGE:

5. The retail spaces shall be solely utilized by retail uses to include: a store engaged in the sale of goods for personal use that shall include, bakeries, barber shop/beauty salon, banks, credit unions, bookstores, clothing, clothing accessories, copier/reproductions, department stores, drugstores, dry cleaners(not dry cleaning plant), florists, cigar shops, travel agencies, health and sport clubs, groceries, jewelry, restaurants and any similar uses deemed by the Director of Planning and Zoning to meet the intent of providing active pedestrian-oriented neighborhood-serving retail uses. Professional offices may be permitted but shall occupy no more than 30% of the gross floor area. Restaurants shall require approval of a special use permit.
6. The colors and materials of the retail tenant signs shall be designed of high quality materials and shall be designed as an integral part of the building that shall relate in materials, color and scale to the remainder of the building and to the retail bay on which it is displayed to the satisfaction of the Director of P&Z and shall comply with applicable codes and ordinances as well as the following guidelines:
 - a. Sign messages shall be limited to logos, names and street address information.
 - b. Illuminated or non-illuminated parapet signs or wall signs above the first level for retail and/or residential uses are prohibited.
 - c. Signs applied to storefront windows shall cover no more than twenty percent of the glass.
 - d. Box signs shall be prohibited.
 - e. Any exterior decorative exterior banners/flags shall be deducted from the overall permitted sign area. Permanent or temporary advertising banners shall be prohibited.
 - f. Display cases, storage, carts or other obstructions shall not be designed to be temporarily or permanently located adjacent to the retail windows. Tables and other active uses adjacent to the window are encouraged.
 - g. Freestanding signs are prohibited. (P&Z)
7. The retail entrances on Henry Street shall be required to be operable entrances for all tenants and this shall be a requirement as part of the lease for each tenant. (P&Z)
8. Retail entrances shall be provided along the interior courtyard and alley. (P&Z)
9. Kitchen equipment shall not be cleaned outside, nor shall any cooking residue be washed into the streets, alleys, or storm sewers. (T&ES)
10. Provision shall be made for constructing exhaust vent shafts and grease traps within the residential buildings to accommodate future ground floor restaurant uses. (P&Z)

PEDESTRIAN AND STREETScape:

11. **CONDITION AMENDED BY THE PLANNING COMMISSION:** The applicant shall install or reimburse the City for the entire cost of the design and installation of traffic calming, landscape and pedestrian improvements ~~on Fayette Street~~ to the satisfaction of the Director of P&Z and T&ES that at a minimum shall include the following:
- a. Stamped and colored asphalt crosswalks shall be provided for each of the crosswalks at the intersections of Fayette and Pendleton Streets.
 - b. Stamped and colored asphalt crosswalks shall be provided for each of the crosswalks at the intersection of Fayette and Oronoco Streets.
 - c. Street bulb outs shall be provided on Princess Street at N. Henry Street.
 - d. Each bulb-out shall provide signage deemed necessary by the City.
 - e. Each bulb-out shall provide landscaping, ground cover and street trees.
 - f. The bulb-outs, landscaping, pedestrian improvements and all traffic calming measures shall be installed prior to the issuance of the first certificate of occupancy. (P&Z)(T&ES)(PC)
12. The applicant shall provide a bus shelter at the corner of Pendleton and Henry Street. The shelter shall be located with the building recess area and shall be designed as an integrated part of the building. An easement shall be provided for the bus stop and the applicant shall be responsible for the maintenance of the facility. The bus shelter shall be designed to satisfaction of the Director of T&ES. (P&Z)(T&ES)
13. The applicant shall provide pedestrian streetscape improvements that at a minimum shall provide the level of improvements depicted on the preliminary site plan and shall also at a minimum provide the following to the satisfaction of the Director of P&Z:
- a. Revise the planters on Henry Street to provide a minimum 10 ft. wide unobstructed sidewalk. A public access easement shall be provided for the portion of the sidewalk not located within the right-of-way. The area between the retail stores and the public sidewalk shall include decorative patterns, materials and textures to reduce the perceived area of hardscape.
 - b. All street frontages other than Henry Street shall provide a minimum 6 ft. unobstructed sidewalk width with a minimum 4-5 ft. planting area adjacent to the buildings. The 4-5 ft. area shall include evergreen and pedestrian scale plantings and landscaping.
 - c. The garage shall be revised to provide at grade planting and landscaping rather than the raised planters as currently depicted. Alternatively the planters shall be revised to be a maximum height of 18" to serve as seating walls. The materials for the planters shall be brick.
 - d. Decorative pedestrian scale light poles shall be provided for each public street frontage, the internal "alley" and internal courtyard.

- e. At each of the curb cuts on Pendleton Street and Oronoco Street, stamped bomanite pedestrian crosswalks shall be provided.
 - f. All sidewalks within the public right-of-way shall be City standard concrete.
 - g. The applicant shall provide, install and maintain a minimum of two (2) decorative public benches for the Henry Street and one decorative public bench for the Oronoco, Fayette and Pendleton street frontages, exclusive of seating provided at the new bus shelter on Pendleton Street.
 - h. The character, location and type of such street furnishings on the final site plan including but not limited to: benches, lights, trash receptacles, bike racks, signs or sign systems and other site furnishings shall be subject to further review and approval with the final site plan.
 - i. All streetscape improvements shall be completed prior to the issuance of a certificate of occupancy permit. (P&Z)
14. Applicant shall provide \$850/ea to the Director of T&ES for the purchase and installation of eight (8) City standard street cans along the public streets, 2 per block face. (T&ES)
15. The applicant shall provide a bicycle rack to accommodate a minimum of eight (8) surface bicycle parking spaces for use by retail patrons as depicted on the preliminary site plan to the satisfaction of the Directors of P&Z and T&ES. (P&Z)
16. Provide bike racks and show location in the garage:
- 1. For the retail use the applicant shall provide three (3) employee bicycle parking spaces and one (1) visitor space to the satisfaction of the Director of T&ES.
 - 2. For the residential use the applicant shall provide seventeen (17)spaces for residents and four (4) visitor spaces to the satisfaction of the Director of T&ES. (T&ES)

OPEN SPACE - LANDSCAPING:

17. The internal central courtyard area shall provide the level of detail and amenities depicted on the preliminary plan, and shall also provide amenities such as special paving surfaces, materials, benches, trash receptacles, landscaping, etc. to encourage its use. In addition the central courtyard shall be revised to provide the following to the satisfaction of the Director of P&Z:
- a. A perpetual public access easement shall be granted for the internal courtyard and pedestrian connections on Fayette Street and Henry Street and all sidewalks located outside the public right-of-way. All easements and reservations shall be depicted on the subdivision plat and shall be approved by the City Attorney prior to the release of the final site plan.
 - b. The internal courtyard and landscaping shall be privately maintained.

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- c. The central courtyard shall be revised to maximize its use as a neighborhood pocket park. The plaza shall be redesigned to open more fully to the east-west connecting open space and to the building entrance at the south end of the courtyard including a more direct connection and the elimination of gates and walls.
 - d. Where walls or planters are necessary they shall be brick or stone.
 - e. The internal landscaped and open space areas shall be irrigated.
 - f. All lawn areas shall be sodded.
 - g. No shrubs higher than three feet shall be planted within six feet of walkways.
 - h. All landscaping shall be maintained in good condition and replaced as needed.
 - i. The central courtyard shall be fully open to the public following the hours and guidelines established by the Department of Parks, Recreation and Cultural Activities during hours normally associated with residential uses.
 - j. A public art statue or sculpture or water feature that is an appropriate scale for the space shall be provided and permanently mounted within the public pocket park, located in consultation with the Director of P&Z.
 - k. Provide street trees along the west side of the alley.
 - l. Shrub plantings shall provide seasonal color and be arranged to provide visual interest and harmony within the public space as well as be complimentary to the design of the public space and its proposed use.
 - m. Low scale pathway or bollard lighting.
 - n. The surface for north-south "alley" visible from the public right-of-way from the Pendelton and Oronoco Street entrances to the western property line shall be stamped and colored bomanite concrete to appear as cobblestone/brick or similar decorative surface to the satisfaction of the Director of P&Z to reduce the perceived expanse of the alley pavement.
 - o. The final design of the open spaces shall be revised to the satisfaction of the Directors of RP&CA and P&Z in consultation with the neighborhood.
 - p. A public access easement shall be granted over all ground level open space and sidewalks in the project. A public ingress/egress easement shall be granted for the alley and adjoining sidewalks to provide public vehicular and pedestrian access through the site and to the central open space area. All easements and reservations shall be approved by the City Attorney prior to the release of the final site plan. (P&Z)
18. **CONDITION AMENDED BY THE PLANNING COMMISSION:** A final landscape plan shall be provided with the final site plan to the satisfaction of the Director of P&Z and RP&CA. The plan shall include the level of landscaping depicted on the preliminary landscape plan and shall at a minimum also provide:
- a. The size of the tree wells shall be increased to 4 ft. x 12 ft. and groundcover shall be provided within each of the tree wells.

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- b. The proposed 24" underground storm line on Oronoco Street shall be relocated or located at a minimum depth of 4 ft. to enable a continual row of street trees.
- c. Provide three additional street trees on Oronoco Street between the proposed "alley" and Henry Street.
- d. A different trees species shall be provided for each street frontage that shall include Willow Oak on Henry Street, Red Maple on Pendleton and Oronoco Street and London Plane trees on Fayette Street.
- e. All street trees shall be a minimum 3.5" to 4" caliper at the time of planting.
- f. Provide an additional street tree on the west side of the curb cut on Pendleton Street.
- g. Irrigation shall be provided for landscaped and open space areas, including above and at grade areas or the Condominium Association shall require the landscape contractor to provide watering of plants during minimal periods of rain, in accordance with landscape maintenance standards.
- h. Provide landscaping beds and foundation plantings along the edge of the public right-of-way to the building facade around the perimeter of the property as depicted in the preliminary plans to include deciduous and evergreen shade and ornamental trees, shrubs and groundcover. against the western facade.
- i. The planting depth on top of the parking deck shall be a minimum of 2' for the shrubs and groundcover and a minimum of 4' of soil depth for trees with adequate drainage to support the trees. Raised planters shall not be provided on the external portion of th building.
- j. All trees to be limbed up a minimum of 6 feet as they mature to allow for natural surveillance.
- k. Trees are not to be planted under or near light poles.
- l. The proposed shrubbery is to have a maximum height of 36 inches when mature. No shrubs higher than 3 feet to be planted within 6 feet of walkways.
- m. All landscaping shall be maintained in good condition and replaced as needed. All plant specifications shall be in accordance with the current and most up to date edition of the American Standard For Nursery Stock (ANSI Z60.1) as produced by the American Association for Nurserymen; Washington, D.C.
- n. All work shall be performed in accordance with Landscape Specifications Guidelines 4th Edition as produced by the Landscape Contractors Association (LCA) of Maryland, District of Columbia and Virginia; Gaithersburg, Maryland.
- o. Depict all utility structures, including transformers, on the final development plan. All utility structures (except fire hydrants) shall be clustered where possible and located so as not to be visible from a public right-of-way or property. When such a location is not feasible, such structures shall be located behind the front building line and screened.
- p. The landscape plan shall be prepared and sealed by a certified landscape architect. (P&Z)(RP&CA)(Police)(PC)

19. The rooftop open space shall be designed to function as high-quality usable open space for the residents. At a minimum, the revised plans for the plaza decks shall include the following to the satisfaction of the Directors of P&Z and RP&CA:
 - a. Features and elements such as seating, trash receptacles, and pedestrian-scale lighting.
 - b. Varied and high quality paving material.
 - c. A significant amount of additional landscaping and irrigation systems.
 - d. The railing for the roof-top deck shall be set back a minimum of 10 feet so that the railing is not visible from the adjoining streets.
 - e. The lighting for the roof-top open space shall be pedestrian-scale pathway lighting and shall not be visible from the adjoining streets.
 - f. Noise generated in conjunction with the roof top open space shall be limited to the type and hours normally associated with a residential use. (P&Z)

PARKING:

20. Residents of the building shall be ineligible to apply for or receive any residential parking permits pursuant to City Code Sec. 5-8, Article F. (P&Z) (T&ES)
21. In order to promote teleworking, each of the units will be wired for high-speed internet access. A minimum of one space for each unit shall be provided within the garage as part of the purchase price for each unit shall be provided within the garage as part of the purchase price for each unit. Of the 73 required retail parking spaces, not more than 30 % shall be compact. In addition to the parking required by the zoning ordinance for residents, 15 % additional parking shall be provided for visitors. (P&Z)(T&ES)
22. All visitor parking spaces shall require all applicable signage as required by the zoning ordinance and shall be installed by the applicant. All appropriate on-street parking signage and any other signage for control of pedestrians and vehicles shall be installed by the developer to the satisfaction of the Director of T&ES. (P&Z)(T&ES)
23. The applicant shall provide off-street parking for all construction workers without charge. For the construction workers who use Metro, DASH, or another form of mass transit to the site, the applicant shall subsidize a minimum of 50% of the fees for mass transit. Compliance with this condition shall be based on a plan, which shall be submitted to the Department of P&Z and T&ES prior to the issuance of the Excavation/Sheeting, and Shoring Permit. This plan shall set forth the location of the parking to be provided at various stages of construction, how many spaces will be provided, how many construction workers will be assigned to the work site, and mechanisms which will be used to encourage the use of mass transit. The plan shall also provide for the location on the construction site at which information will be posted regarding Metro schedules and routes, bus schedules and routes. If the plan is found to be violated during the course of

construction, a correction notice will be issued to the developer. If the violation is not corrected within ten (10) days, a "stop work order" will be issued, with construction halted until the violation has been corrected. (P&Z)(T&ES)

24. The retail delivery schedule shall be reviewed to the satisfaction of the Directors of P&Z and T&ES prior to the release of the final site plan. Deliveries shall occur in the designated loading area in the private alley as depicted on the site plan. (P&Z)(T&ES)
25. The maximum allowable slope for the garage ramp is 10%. Provide dimensioning of parking spaces, aisle widths, etc. in the parking garage as shown on the preliminary site plan. The dimensions shall not include column widths. (T&ES)
26. The applicant shall provide controlled access into the underground garage. The controlled access to the garage shall be designed to allow convenient access to the underground parking for residents and visitors. The applicant shall provide wheel stops for the ninety-degree parking spaces within the central courtyard. (P&Z)
27. Provide turning movements for the loading area. (T&ES)
28. Wheel stops shall be provided for all do degree surface retail parking spaces. (P&Z)

SITE PLAN:

- 28A. The applicant shall participate and be subject to any special districts or programs created by the Braddock Road metro planning study. (P&Z)
29. All transformers shall be located adjacent to the internal street alley to the satisfaction of the Director of P&Z. (P&Z)
30. The portion of the drive aisle that is located on top of the parking garage shall be designed to AASHTO HS-20 loading requirements of delivery trucks and fire apparatus to the satisfaction of the Directors of P&Z, T&ES and Code Enforcement. (P&Z)
31. Show AASHTO site distances lines at each entrance. (T&ES)
32. The six existing lots shall be consolidated. The plat of consolidation shall be submitted with the submission of the first final site plan. The plat of consolidation shall be approved and recorded prior to the release of the final site plan. (P&Z)
33. A freestanding subdivision or development sign shall be prohibited. (P&Z)

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34. The applicant shall prepare and submit a plan that delineates a detailed phasing plan and construction management plan for the entire project for review and approval by the Directors of P&Z, T&ES and Code Enforcement prior to the release the final site plan for the project. Before commencing any clearing or grading of the site, the applicant shall hold a meeting with notice to all adjoining property owners to review the location of construction worker parking, plan for temporary pedestrian and vehicular circulation, and hours and overall schedule for construction. During the intervals that the alley is not available, temporary loading areas will be provided on the street subject to the review and approval of the Director of T&ES. (P&Z) (T&ES)
35. Show all utility structures, including transformers, on the final development plan. All utility structures (except fire hydrants) shall be clustered where possible and located so as not to be visible from a public right-of-way or property. When such a location is not feasible, such structures shall be located behind the front building line and screened to the satisfaction of the Director of P&Z. (P&Z)
36. The limits of excavation relative to the property lines are of concern. Any structural elements that extend into the right-of-way, including footings, foundations, etc., must be approved by the Director of T&ES. (T&ES)
37. The applicant shall be allowed to make minor adjustments to the building location if the changes do not result in off-street parking or open space below that required by the Zoning Ordinance and do not result in an increase in building height or floor area ratio. (P&Z)
38. Temporary construction and sales trailer(s) shall be permitted and be subject to the approval of the Director of P&Z. The trailer(s) shall be removed prior to the issuance of a certificate of occupancy permit for the building. (P&Z)
39. Any inconsistencies between the various drawings submitted by the applicant shall be reconciled to the satisfaction of the Directors of P&Z and T&ES. (P&Z)
40. The applicant shall identify a person who will serve as liaison to the community throughout the duration of construction. The name and telephone number of this individual shall be provided in writing to residents, property managers and business owners whose property abuts the site, and to the Directors of P&Z and T&ES. (P&Z)
41. A temporary informational sign shall be installed on the site prior to approval of the first final site plan for the project and shall be displayed until construction is complete or replaced with a marketing sign incorporating the required information: the sign shall notify the public of the nature of the upcoming project and shall provide a phone number for public questions regarding the project. (P&Z)

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42. Provide a site lighting plan to the satisfaction of the Director of T&ES in consultation with the Chief of Police. The plan shall show the existing and proposed street lights and site lights. Indicate the type of fixture, and show mounting height, and strength of fixture in Lumens or Watts. Provide manufacturer's specifications for the fixtures. Provide lighting calculations to verify that lighting meets city standards and are located to prevent excessive spillover lighting and glare to adjacent properties. (T&ES) (P&Z)
43. No major construction staging will be allowed on North Henry Street (Route 1). Applicant to meet with T&ES to discuss construction staging activities prior to the release of any permits for ground disturbing activities. (T&ES)
44. Before commencing any clearing or grading of the site, the applicant shall hold a meeting with all adjoining property owners to review the hauling routes, location of construction worker parking, plan for temporary pedestrian and vehicular circulation, and hours and overall schedule for construction. The Departments of P&Z and T&ES shall be notified of the date of the meeting before the permit is issued. Copies of plans showing the hauling route, construction worker parking and temporary pedestrian and vehicular circulation shall be posted in the construction trailer and given to each subcontractor before they commence work on the project. (P&Z)
45. The applicant shall submit a final location survey for the buildings and interior of the parking garage prior to issuance of a certificate of occupancy permit. (P&Z)
46. All condominium association covenants shall be reviewed by the Director of P&Z and the City Attorney to ensure inclusion of all the conditions of this DSUP prior to applying for the first certificate of occupancy permit for the project. The association covenants shall include the conditions listed below, which shall be clearly expressed in a separate section of the covenants.
 - a. The principal use of the underground garage and parking spaces shall be for passenger vehicle parking only; storage which interferes with the use of a parking space for a motor vehicle is not permitted.
 - b. The designated visitor parking spaces shall be reserved for the use of the condominium guests.
 - c. No more than two parking spaces shall be assigned to a specific condominium unit until all settlement on the units are complete; all unassigned spaces in the garage shall be made generally available to residents and/or visitors.
 - d. A public access easement is provided within the central courtyard and is for the use of the general public. The responsibility for the maintenance of the courtyard is the responsibility of the condominium association. The hours for use by the public will be consistent with the Department of Parks, Recreation and Cultural Activities hours for public parks during hours normally associated with residential use.

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- e. All landscaping and open space areas within the development, shall be maintained by the homeowners and condominium owners.
 - f. Exterior building improvements or changes by future residents shall require the approval of the City Council, as determined by the Director of P&Z.
 - g. The developer shall notify prospective buyers, in its marketing materials, that the mid-block alley is private and that all on-site storm sewers are private and will be maintained by the Condominium Owner's Association.
 - h. That the site is located adjacent to N. Henry Street, within the heart of an urban area and proximate to the Metrorail track and other railway operations located within the immediate vicinity of the project, will continue indefinitely, and will generate heavy truck and vehicular traffic surrounding the project. (P&Z)
- 47. Provide all pedestrian and traffic signage to the satisfaction of the Director of T&ES. (T&ES)
 - 48. All driveway entrances and sidewalks in public ROW or abutting public ROW shall meet City standards. (T&ES)
 - 49. Show all existing and proposed easements, both public and private. (T&ES)
 - 50. Replace existing curb and gutter, sidewalks, and handicap ramps that are in disrepair or broken. (T&ES)
 - 51. Provide City standard pavement for emergency vehicle easements. (T&ES)
 - 52. Prior to the release of the final site plan, provide a Traffic Control Plan for construction detailing proposed controls to traffic movement, lane closures, construction entrances, haul routes, and storage and staging. (T&ES)
 - 53. All Traffic Control Device design plans, Work Zone Traffic Control plans, and Traffic Studies shall be sealed by a professional engineer, registered in the Commonwealth of Virginia. (T&ES)
 - 54. Provide proposed elevations (contours and spot shots) in sufficient details on grading plan to clearly show the drainage patterns. (T&ES)
 - 55. The sidewalk along North Henry Street (Route 1) must remain open during construction unless previously approved by the Director of T&ES. (T&ES)
 - 56. Label all existing and proposed utilities on geometry plan.(T&ES)
 - 57. Provide standard City approval block on all plan sheets and orient correctly.(T&ES)

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58. Applicant shall provide and install two 4" rigid galvanized steel conduits around perimeter of site with junction boxes at each corner for future traffic signal. Conduits shall be located behind the curb line, buried 2' deep and terminate in the junction boxes. (T&ES)
59. If fireplaces are to be included in the development, the applicant is required to install gas fireplaces to reduce air pollution and odors. Animal screens must be installed on chimneys. (T&ES)
60. In the event that Section 5-1-2(12b) of the City Code is amended to designate multi-family dwellings in general, or multi-family dwellings when so provided by SUP, as required user property, then refuse collection shall be provided by the City. (T&ES)
61. The developer agrees to deliver all solid waste, as defined by the Code of the City of Alexandria, to a refuse disposal facility designated by the Director of T&ES. The developer further agrees to stipulate in any future lease or property sales agreement that all tenants and/or property owners shall also comply with this requirement. (T&ES)
62. Since eight-inch fire services are shown, the proposed water mains must be a minimum of eight-inch. All of the existing water mains in Oronoco, N. Fayette, Pendleton and N. Henry Streets will need to be replaced. Hydraulic calculations will verify if proposed eight-inch water mains will be sufficient. (VAWC)
63. The proposed four-inch domestic and eight-inch fire services shown on N. Henry St. cannot be "split" as currently shown. They must be individual services to each building. (VAWC)
64. On the northeast corner of Pendleton and N. Henry it states "replace 2" with 6"". Clarify if a proposed fire hydrant is shown there. (VAWC)
65. The walls and ceilings in the garages are to be painted white unless sufficient lighting is demonstrated to the Police Department. (Police)

STORMWATER:

66. Developer to comply with the peak flow requirements of Article XIII of AZO. (T&ES)
67. The project is located in the Braddock West Watershed. Provide an additional 10% detention of the ten year pre-development discharge. (T&ES)
68. Relocate the proposed BMP that is shown at the south end of the alley out of the public right-of-way. (T&ES)

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69. Relocate the proposed 24" storm sewer out of the planting strip into the street. (T&ES)
70. The project lies within the Combined Sewer District thus stormwater management and compliance with the City's Chesapeake Bay program must be coordinated with the City's policy for management of the Combined Sewer District. All proposed sanitary laterals and proposed sanitary sewer shall be connected to the Potomac Yard Trunk Sewer located in N. Fayette Street. The plan does not show the sanitary connections connecting to the Potomac Yard Trunk Sewer. The developer shall construct a new sanitary sewer main on Oronoco Street consistent with the City's CSO Area Reduction Plan. The final size, inverts and alignment shall be approved by the Director of Transportation and Environmental Services. Preliminary analysis estimates the size of the sewer to be 12-inch. (T&ES)
71. Revise the size of the Potomac Yard Trunk Sewer to show a 30" sanitary sewer. The plan shows 24".(T&ES)
72. The proposed 15" storm sewer on Oronoco St. and N. Henry St. is discharging into a 10" combined sewer system. Plan must demonstrate to the satisfaction of the Director of T&ES that adequate stormwater outfall is available to the site or else developer is to design and build any on or off site improvements to discharge to an adequate outfall. (T&ES)
73. The City Attorney has determined that the City lacks the authority to approve the gravity fed sanitary sewer systems which serve over 400 persons. Accordingly, the overall sanitary sewer system for the proposed development must be submitted for approval by the Virginia Department of Health (VDH). Both City and VDH approval are required, though City approval may be given conditioned upon the subsequent issuance of VDH approval. Should state agencies require changes in the sewer design, these must be accomplished by the developer prior to the release of a certificate of occupancy for the units served by this system. Prior to the acceptance of dedications of the sewers by the city or release of any construction bonds, the developer must demonstrate that all necessary state agency permits have been obtained and as-built drawings submitted to the City that reflect all changes required by the state. (T&ES)
74. The applicant is advised that all stormwater designs that require analysis of pressure hydraulic systems and/or inclusion and design of flow control structures must be sealed by a professional engineer, registered in the Commonwealth of Virginia. If applicable, the Director of T&ES may require resubmission of all plans that do not meet this standard. (T&ES)
75. Provide a narrative describing how the project will comply with the stormwater quantity and quality requirements of Article XIII of the Zoning Ordinance.(T&ES)

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76. Provide pre- and post-development, two- and ten-year storm water computations for the entire site.(T&ES)
77. Plan must demonstrate to the satisfaction of the Director of T&ES that adequate stormwater outfall is available to the site or else developer is to design and build any on or off site improvements to discharge to an adequate outfall. (T&ES)
78. The applicant shall reconstruct all public storm sewer structures on the perimeter of the project that are substandard to meet current City of Alexandria construction standards.(T&ES)
79. Provide rim elevations and invert elevations of all existing piping at structures.(T&ES)
80. The stormwater collection system is part of the Potomac River watershed. All on-site stormwater curb inlets and public curb inlets within 50 feet of the property line shall be duly marked to the satisfaction of the Director of T&ES.(T&ES)
81. Provide a drainage map for the area flowing to the chosen BMP, including topographic information and storm drains.(T&ES)
82. The stormwater Best Management Practices (BMPs) required for this project shall be constructed and installed under the direct supervision of the design engineer or his designated representative. The design engineer shall make a written certification to the City that the BMP(s) are constructed and installed as designed and in accordance with the approved Final Site Plan.(T&ES)
83. The surface appurtenances associated with the on-site structural BMP's shall be marked to the satisfaction of the Director of T&ES to identify them as part of the structural BMP system.(T&ES)
84. If the units will be sold as individual units and a home owner's association established the following two conditions shall apply:
 1. The Developer shall furnish the Homeowner Association with an Owner's Operation and Maintenance Manual for all the Best Management Practices (BMP's) used on site. The manual shall include at a minimum: an explanation of the functions and operations of the BMP(s), drawings and diagrams of the BMP(s) and any supporting utilities, catalog cuts on maintenance requirements and a copy of the maintenance agreement with the City.
 2. The Developer shall furnish each home purchaser with a brochure describing the stormwater BMP(s) installed on the site, outlining the responsibilities of the homeowners and the Homeowner's Association (HOA) with respect to maintenance requirements. Upon activation of the HOA, the Developer shall

furnish five copies of the brochure per unit to the HOA for distribution to subsequent homeowners.

Otherwise the following condition applies:

The developer shall furnish the owners with an Operation and Maintenance Manual for all Best Management Practices (BMP's) on the project. The manual shall include an explanation of the functions and operations of each BMP and any supporting utilities, catalog cuts, on any mechanical or electrical equipment, a schedule of routine maintenance for the BMP(s) and supporting equipment, and a copy of the maintenance agreement with the City. (T&ES)

HOUSING:

85. The developer shall provide three affordable two-bedroom set-aside units at a price not to exceed \$225,000 and two affordable one-bedroom units at a price not to exceed \$175,000 (prices include one garage parking space for each unit) for sale to households with at least one member who lives or works in the City of Alexandria, and whose incomes do not exceed the City's income guidelines for the Moderate Income Housing Program (MIHP), which are currently \$68,700 for a one or two person household and \$79,500 for a three or more person household. (Housing)
86. The set-aside units shall be of the same size, type and with the same amenities as other similar units in the development. The units shall be scattered throughout the building. (Housing)
87. Whatever incentives are offered to any potential homebuyers shall also be offered to purchasers of the set-aside units. (Housing)
88. Fifteen year affordability of the set-aside units shall be provided through deed restrictions recorded as covenants at the time of sale of each of the set-aside units, in accordance with the City's set-aside resale policy. Language for the covenants shall be provided by the City in advance of the final sale of any unit. (Housing)
89. The City shall have exclusive right to market the set-aside units for 90 days, after which the developer shall also have the right to market the units, at the affordable price, to buyers qualified for the set-aside program. The developer is encouraged to refer potentially qualified buyers to the Office of Housing's Implementation Division for participation in the set-aside and MIHP programs. In the event a qualified buyer cannot be found for a set-aside unit by the time the unit is completed and ready for occupancy, the developer shall have the right to sell the unit at full market price, and shall contribute to the Housing Trust Fund the equivalent of \$4.28 per gross square foot of each unit so sold. (Housing)

90. If the developer sells the market rate units for less than expected, the applicant shall index the price on the affordable units proportionately. (Housing)
91. Real estate commissions shall be paid (or not paid) for set-aside units in the same manner and on the same basis as for market rate units. (Housing)

ARCHAEOLOGY:

92. An archaeological consultant for the applicant has completed a Documentary Study on this property. Recommendations in the report indicate that the following actions are needed: archaeological monitoring of building demolition and debris removal; manual and mechanical clearing of the entire block to expose potential features, and mapping of all exposed features; archaeological testing of selected features, as established through on-site consultation with the staff of Alexandria Archaeology; laboratory processing and analysis of recovered cultural materials; and preparation of a full technical report on the results of the investigation. The applicant must hire an archaeological consulting firm to fulfill these requirements. (Archaeology)
93. This property has the potential to contain human burials. If human burials that will be disturbed by the proposed development are discovered on this property, excavation in the area of the burial(s) must cease, and appropriate court orders and state permits must be obtained by the applicant before any burial can be removed. (Archaeology)
94. All archaeological preservation measures must be completed prior to ground-disturbing activities (such as coring, grading, filling, vegetation removal, undergrounding utilities, pile driving, landscaping and other excavations as defined in Section 2-151 of the Zoning Ordinance). To confirm, call Alexandria Archaeology at (703) 838-4399. (Archaeology)
95. Call Alexandria Archaeology immediately (703-838-4399) if any buried structural remains (wall foundations, wells, privies, cisterns, etc.) or concentrations of artifacts are discovered during development. Work must cease in the area of the discovery until a City archaeologist comes to the site and records the finds. (Archaeology)
96. The General Notes of the Preliminary and Final Site Plans must include the statements in preceding Conditions # 93, 94, and 95 above so that on-site contractors are aware of the requirements. Additional statements to be included on the Final Site Plan will be determined in consultation with Alexandria Archaeology. (Archaeology)
97. Certificates of Occupancy will not be issued for this property until the final archaeological report has been received and approved by the City Archaeologist. (Archaeology)

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98. The developer will erect a historic marker on the property according to specifications provided by Alexandria Archaeology, and agreed to in concurrence with the Directors of the Office of Historic Alexandria and Recreation, Parks and Cultural Activities . The marker will highlight the historical and archaeological significance of the property. (Archaeology)
99. If warranted by the City Archaeologist, the developer will produce a booklet for the public on the history and archaeology of the property, according to specifications provided by Alexandria Archaeology. (Archaeology)
100. All archaeological work will be carried out in accordance with the *City of Alexandria Archaeological Standards* and is subject to the approval of the City Archaeologist. (Archaeology)

Special use permits and modifications requested by the applicant and recommended by staff:

1. Special Use Permit to increase in FAR from 1.5 to 2.33.
2. Modification to the minimum 75' vision clearance.
3. Modification to the minimum zone transition setback from adjacent residential zone.

Staff Note: In accordance with section 11-506(c) of the zoning ordinance, construction or operation shall be commenced and diligently and substantially pursued within 18 months of the date of granting of a special use permit by City Council or the special use permit shall become void.

CITY DEPARTMENT CODE COMMENTS

Legend: C - code requirement R - recommendation S - suggestion F - finding

Transportation and Environmental Service:

- C-1 Bond for the public improvements must be posted prior to release of the plan.
- C-2 All downspouts must be connected to a storm sewer by continuous underground pipe.
- C-3 The sewer tap fee must be paid prior to release of the plan.
- C-4 All easements and/or dedications must be recorded prior to release of the plan.
- C-5 Plans and profiles of utilities and roads in public easements and/or public right-of-way must be approved prior to release of the plan.
- C-6 All drainage facilities must be designed to the satisfaction of T&ES. Drainage divide maps and computations must be provided for approval.
- C-7 All utilities serving this site to be underground.
- C-8 Provide site lighting plan to meet minimum city standards.
- C-9 The applicant shall comply with the City of Alexandria's Noise Control Code, Title 11, Chapter 5, which sets the maximum permissible noise level as measured at the property line.
- C-10 The applicant must comply with the Chesapeake Bay Preservation Act in accordance with Article XIII of the City's zoning ordinance for storm water quality control which includes requirements for pollutant load reductions and treatment of the Water Quality Volume Default (WQV).
- C-11 The applicant must comply with the City of Alexandria, Erosion and Sediment Control Code, Section 5, Chapter 4. This includes naming a "Certified Land Disturber" on the Erosion and Sediment Control sheets prior to release of the final Site Plan in accordance with Virginia Erosion and Sediment Control Law VAC §: 10.1-563.B.
- C-12 All required permits from Virginia Department of Environmental Quality, Environmental Protection Agency, Army Corps of Engineers, Virginia Marine Resources must be in place for all project construction and mitigation work prior to release of the final site plan. This includes the new state requirement for a VPDES permit for all construction activities greater than 1 acre.

Virginia-American Water Company:

1. Please contact Al Walukonis at VAWC (703-706-2863) with any questions regarding water main requirements. (VAWC)

Code Enforcement:

Updated comments of October 26, 2004 are in **BOLD**.

- F-1 The applicant shall indicate if ownership of the building is single ownership or if property lines will be drawn within the property; horizontally or vertically and separate ownership is assigned to each parcel. Applicant states building will be under single ownership.
- F-2 Fire flow data submitted on Sheet 1 is insufficient for required fire flow submission. Prior to submission of the Final Site Plan, the developer shall provide a fire flow analysis by a certified licensed fire protection engineer to assure adequate water supply for the structure being considered. **Condition not met. Applicant states fire flow will be prepared by a licensed FPE and submitted with final plan. NOTE: submit fire flow analysis on separate 8.5 x 11 sheets. Do not include as a plan sheet.**
- F-3 Two siamese connections are required for each fire service (Retail, Residential, Garage). Each Siamese shall be located remotely from the other. Siamese for each system may be clustered together. Two clusters of siamese are required for this project. Clusters shall be located on Pendleton and Oronoco Streets. Hydrants shall be located within 100 feet of each Siamese connection. Only one siamese is provided on plans for retail use which is located on Henry Street. Additional siamese connections are required based on information provided to date. Relocate siamese from Henry Street. See cluster location above. **Condition not met. The revised drawings break the structure up while maintaining two large structures. The number of siamese connections are confusing and shall be simplified. The fire protection system shall be looped. Place a siamese connection on each major corner (ie. Henry & Oronoco Streets, Henry & Pendleton Streets, etc) of the overall structure. Each siamese shall serve both the garage, retail and residential components of all buildings. Locate hydrants within 100 feet of each FDC.**
- F-4 The landscaping shall not obstruction Siamese connections. Future landscape and site plans shall be coordinated to eliminate possible conflicts. **See F-3 above and revise accordingly.**
- F-5 The submitted plans do not provide a site plan exhibit. The plans provide a grading plan which details most of the site plan details but is not sufficient for a review of the project.
- F-6 Provide overhead clearances for interior drive aisle. Show clearances on Sheets A1.3 & A1.4 in addition to Sheets A2.1 and A2.2. **Condition met.**

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- C-1 A full fire sprinkler system is required. A Fire suppression system designed to NFPA 13 standards shall apply to the retail and garage areas. A fire suppression system designed to NFPA 13R shall apply to the residential portion of the structure. Acknowledged by applicant.
- C-2 A separate tap is required for the building fire service connection. Clarify which tap will provide fire service for the garage. If neither tap shown provides service for garage fire suppression system, a third tap will be required. **Condition not met. See F-3 above and revise accordingly.**
- C-3 This structure contains mixed use groups [M, Mercantile; R, Residential; S-2, Low-Hazard Storage (public garage, group 2) and is subject to the mixed use and occupancy requirements of USBC. Acknowledged by applicant.
- C-4 Fire Department ladder truck access is required for two longest sides (100% access to Henry Street & Fayette Street sides) of all buildings over 50 feet in height. This requires a truck to be able to position itself between 15 and 30 feet from the face of the building. For a building face to be considered accessible by a ladder truck the curb line shall be at least 15 feet and no more than 30 feet from the face of the building. The face of the building may not articulate back into the mass of the building more than 7 feet horizontally in the first 75 feet of vertical dimension of the building. The current massing and articulation does not meet the requirements for ladder truck access. Applicant has agreed to ladder truck alternatives.
- C-5 All elevated structures used for the purpose of ladder truck access shall be designed to AASHTO HS-20 loadings. Acknowledged by applicant.
- C-6 Provide an Emergency Vehicle Easement for the interior roadway. The easement shall be utilized for ambulance and police access only in order to minimize traffic disruption on Henry Street. This easement shall not be utilized for fire apparatus. The easement shall be designed to AASHTO HS-20 loadings. A minimum of 14 foot clearance from overhead obstructions is required. Condition met.
- C-7 Hydrants shall be located within 100 feet and on the same side of the street of the siamese connection served. **Condition not met. See F-3 above and revise.**
- C-8 New construction must comply with the current edition of the Uniform Statewide Building Code (USBC). Shown as Note 2 on Sheet 2.
- C-9 The developer shall provide a building code analysis with the following building code data on the plan: a) use group; b) number of stories; c) type of construction; d) floor area per floor ; e) fire protection plan. Acknowledged by applicant.
- C-10 The final site plans shall show placement of fire easement signs. Acknowledged by applicant.
- C-11 A soils report must be submitted with the building permit application. Acknowledged by applicant.

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- C-12 Prior to submission of the Final Site Plan, the developer shall provide a fire flow analysis by a certified licensed fire protection engineer to assure adequate water supply for the structure being considered. **Condition not met. See F-2 above.**
- C-13 A Certificate of occupancy shall be obtained prior to any occupancy of the building or portion thereof, in accordance with USBC 118.0. Acknowledged by applicant.
- C-14 All exterior walls within 5 feet from an interior property line shall have a fire resistance rating of 1 hour, from both sides, with no openings permitted within the wall. As alternative, a 2 hour fire wall may be provided. The applicant shall clarify if Virginia Power intends to vacate the existing power substation and sell the land as part of incorporation as one parcel for this project. Should Virginia Power retain a lot within the block of this project, wall ratings and opening restrictions may apply based upon location of the lot line. Acknowledged by applicant.
- C-15 Required exits, parking, and facilities shall be accessible for persons with disabilities. Provide 2 additional handicap parking spaces on P2 garage level. Acknowledged by applicant.
- C-16 The public parking garage floor must comply with USBC and drain through oil separators or traps to avoid accumulation of explosive vapors in building drains or sewers as provided for in the plumbing code (USBC). This parking garage is classified as an S-2, Group 2, public garage. Floors of public garages must be graded to drain through oil separators or traps to avoid accumulation of explosive vapors in building drains or sewers (USBC). Acknowledged by applicant.
- C-17 Enclosed parking garages must be ventilated in accordance with USBC. Show vent locations. Condition met, vents shown on plans.
- C-18 Prior to the issuance of a demolition permit or land disturbance permit, a rodent abatement plan shall be submitted to Code Enforcement that will outline the steps that will taken to prevent the spread of rodents from the construction site to the surrounding community and sewers. Show note on plans. **Condition not met.**
- C-19 Roof drainage systems must be installed so as neither to impact upon, nor cause erosion/damage to adjacent property. Acknowledged, drains will be tied in to a closed system.
- C-20 Sheeting and shoring shall not extend beyond the property line; except when the developer has obtained a written release from adjacent property owners which has been recorded in the land records; or through an approved encroachment process. Acknowledged by applicant.

Police:

1. (Staff is not including the following comment as a condition. City staff generally recommends that City standard lighting levels be met on public streets, sidewalks, rights-of-way and common areas rather than the higher levels recommended here by the Police Department.)
The lighting for the parking garages, sidewalks, parking lots, and all common areas is to be a minimum of 2.0 foot candles minimum maintained.

Health Department:

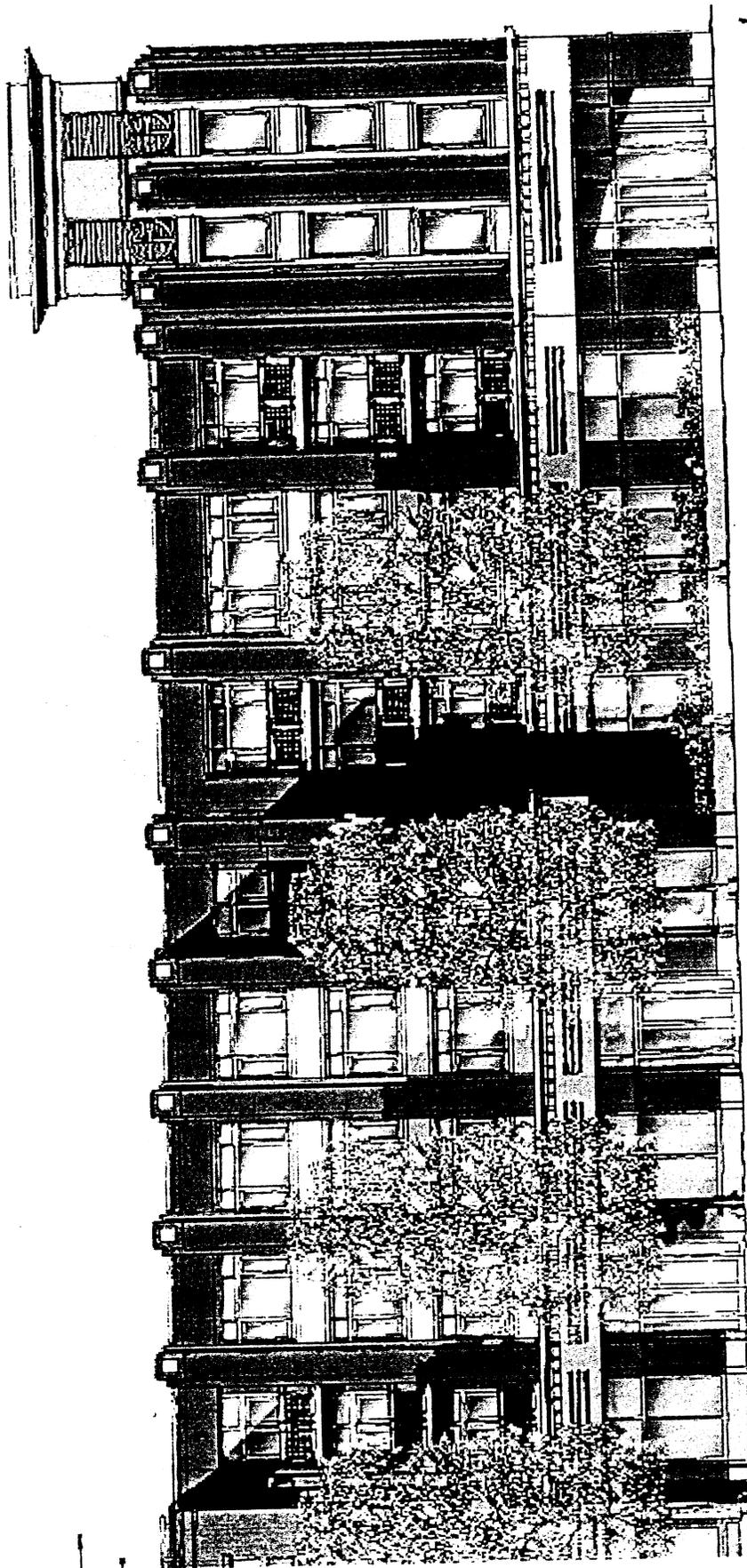
- C-1 An Alexandria Health Department Permit is required for all regulated facilities.
- C-2 Permits are non-transferable.
- C-3 Permits must be obtained prior to operation.
- C-4 Five sets of plans are to be submitted to and approved by this department prior to construction of any facility regulated by the health department.
- C-5 Plans for food facilities must comply with the Alexandria City Code, Title 11, Chapter 2, Food and Food Establishments. There is a \$135.00 fee for review of plans for food facilities.
- C-6 Pool plans must comply with Title 11, Chapter 11, Swimming Pools. Tourist establishment pools must have six (6) sets of plans submitted.
- C-7 Personal grooming facilities must comply with Title 11, Chapter 7, Personal Grooming Establishments.
- C-8 Tanning Salons must meet State Code Title 59.1, Chapter 24.1, Tanning Facilities.
- C-9 Massage facility plans must comply with Title 11, Chapter 4.2, Massage Regulations. All massage therapists must possess a current massage therapist certification, issued by the Commonwealth of Virginia in accordance with the Code of Virginia Chapter 599, § 54.1-3029 and must possess an Alexandria Massage permit in accordance with Alexandria City Code Title 11, Chapter 4.2 prior to engaging in any massage activity.
- C-10 Coin-operated dry cleaning facility plans must comply with Title 9, Chapter 4, Coin operated Dry Cleaning Establishments.
- C-11 Coin-operated laundry plans must comply with Title 9, Chapter 5, Coin Operated Laundries.
- C-12 Hotels/Motels must comply with State Code 35.1, Hotels, Restaurants, Summer Camps, and Campgrounds.
- C-13 Food must be protected to the point of service at any outdoor dining facility.

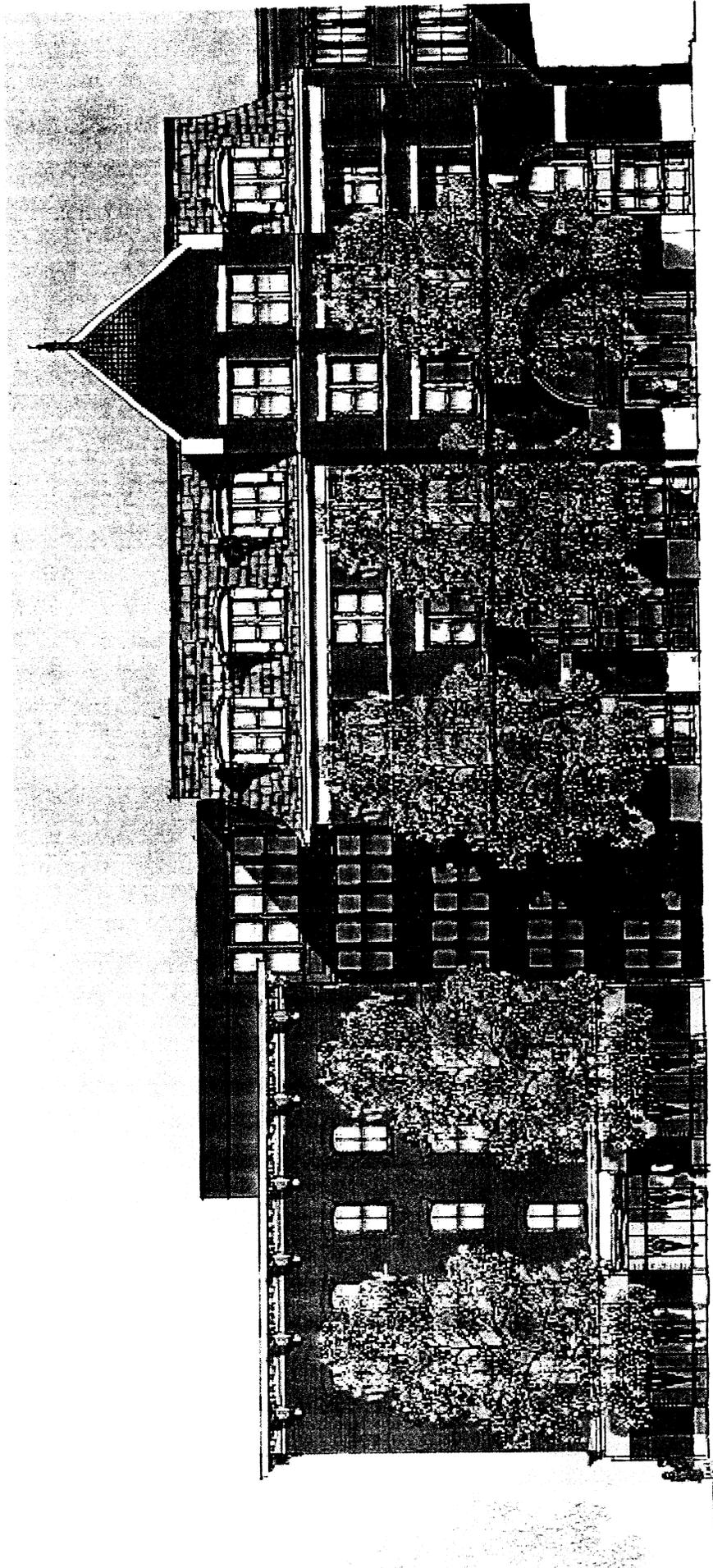
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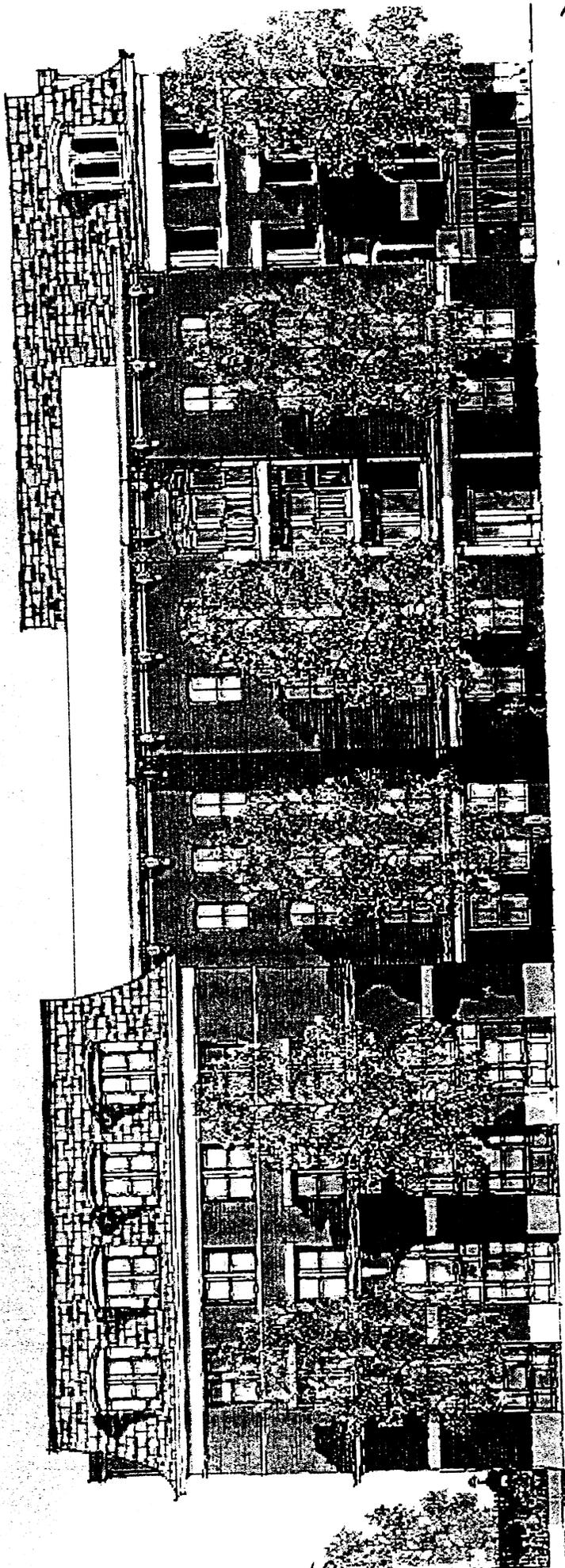
- R-1 Provide a menu or list of foods to be handled at this facility to the Health Department prior to opening.

Historic Alexandria (Archaeology):

- F-1 This property was the site of Colross, an early 19th-century estate which occupied the entire city block bounded by Henry, Pendleton, Oronoco and Fayette Streets. John Potts, secretary of the Potomac Company, began construction of the mansion in 1799, and it was completed in 1802. Potts sold the estate to Jonathan Swift, a diplomat and prominent Alexandria merchant, in 1803. Swift died in 1824 and was buried at Colross. Alexandria attorney and Circuit Court judge Thomas Mason, the grandson of George Mason, purchased the property after Swift's death. During the Civil War, the estate was seized and used by federal authorities. The development parcel therefore has the potential to yield significant archaeological resources relating to activities on the historic estate. In addition, it is possible that a family cemetery could be present on the site.







HART, CALLEY, GIBBS & KARP, P.C.

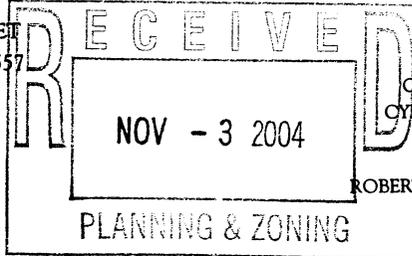
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November 3, 2004

HARRY P. HART
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HERBERT L. KARP



OF COUNSEL
CYRIL D. CALLEY
RETIREE
ROBERT L. MURPHY, 2001

*PC Docket Item #13
DSUP 2003-0019*

Mr. Eric Wagner, Chairman and Members of Planning Commission
c/o Eileen Fogarty, Director
Department of Planning & Zoning
City Hall, Room 2100
Alexandria, VA 22314

Re: DSUP #2003-0019, Docket Item #13

Dear Mr. Wagner and Members of the Planning Commission:

The applicant, Diamond Alexandria, LLC, has worked diligently with the staff on the redevelopment of the Hennage property and we are in agreement on just about every issue, save two described below. We would ask, however, that the Planning Commission include the following changes to Condition Nos. 1(n), 5, and 18(g) to which staff is in agreement:

- 1n. The railing for the roof-top open space for building # 2 shall be setback a minimum of 10 ft. from the building facade to ensure that railing is not visible from the adjoining streets or incorporated as part of the building's parapet.
- 5. The retail spaces shall be solely utilized by retail uses to include: a store engaged in the sale of goods for personal use that shall include bakeries, barber shop/beauty salon, banks, credit unions, bookstores, clothing, clothing accessories, copier/reproductions, department stores, drugstores, dry cleaners(not dry cleaning plant), florists, cigar shops, travel agencies, health & sport clubs, groceries, jewelry, restaurants and any similar uses deemed by the Director of Planning and Zoning to meet the intent of providing active pedestrian-oriented neighborhood-serving retail uses. Professional offices may be permitted but shall occupy no more than 30% of the gross floor area. Restaurants, except for European-style cafes, including those with outdoor seating, like Café'Deluxe, Peacock Café, Luna Café, Vie De France, La Madeleine, Marvelous Market, Au Bon Pan, Firehook Bakery and Bread & Chocolate, as well as coffee & tea shops, shall require approval of a special use permit. Such cafes shall be permitted with the following conditions:

- a. Outdoor seating is encouraged and shall be permitted. Outdoor dining operations shall be subject to separate administrative approval. Outdoor dining shall be limited to the Henry Street and those portions of Oronoco and Pendleton Streets directly in front of the retail space.
- b. The outdoor seating areas including umbrellas shall not include advertising signage. The design and quality of the outdoor seating shall be compatible with the design of the building. The seating capacity for the outdoor dining shall not exceed 30 seats. The outside dining area shall be cleaned at the close of each day of operation.
- c. No live entertainment is permitted inside the café or in the outdoor dining area without a separate SUP.
- d. The hours during which the indoor restaurant/café and outdoor dining is open to the public shall be restricted to between 7:00 a.m. and 10:00 p.m. Sunday through Thursday, and between 7:00 a.m. and 11:00 p.m. on Friday and Saturday. Meals ordered before the closing hour may be served, but no new patrons may be admitted and no alcoholic beverages may be served after the closing hour, and all patrons must leave by one hour after the closing hour;
- e. Off-site alcohol sales/services are not permitted from the café or outdoor seating.
- f. No delivery services permitted from the café;
- g. No food, beverages, or other material shall be stored outside.;
- h. Trash and garbage shall be placed in sealed containers which do not allow odors to escape and shall be stored inside or in a closed container which does not allow invasion by animals. No trash and debris shall be allowed to accumulate on site outside of those containers;
- i. Litter on the site and on public right of way and spaces adjacent to or within 50 feet of the premises shall be picked up at least twice a day and at the close of business, and more often if necessary, to prevent an unsightly or unsanitary accumulation, on each day that the business is open to the public. The applicant/operator of the restaurant shall control cooking odors, smoke and any other air pollution from operations at the site and prevent them from becoming a nuisance to neighboring properties, as determined by the Department of Transportation and Environmental Services;
- j. The applicant shall contact the Crime Prevention Unit of the Alexandria Police Department for a security survey and a robbery awareness program for all employees.
- k. The Director of Planning and Zoning shall review the special use permit one year after the use becomes operational and shall docket the matter for consideration by the Planning Commission and City Council if (a) there have been documented violations of the permit conditions, (b) the director has received a request from any person to docket the permit for review as a result of a complaint that rises to the level of a violation, or (c) the director has determined

Mr. Eric Wagner, Chairman
and Members of Planning Commission
November 3, 2004
Page 3

that there are problems with the operation of the use and that new or revised conditions are needed. (P&Z)

1. Kitchen equipment shall not be cleaned outside, nor shall any cooking residue be washed into the streets, alleys, or storm sewers.

- 18g. Irrigation shall be provided for landscaped and open space areas, including above and at grade areas or the Condominium Association shall require the landscape contractor to provide watering of plants during periods of minimal rain, in accordance with landscape maintenance standards.

There remain two issues to which we do not agree with Staff: 1) further redesign of the buildings which includes a reduction in units and a removal of four balconies; and 2) the number of affordable housing units. First and foremost, we believe any additional cuts of the units in the buildings are unnecessary and are detrimental to the project. The design of the buildings has been dramatically altered already in response to Staff's concerns, including cutting the building into two separate buildings and reducing the density from 2.5 FAR permitted with a use permit to 2.33 FAR, approximately 14,580 square feet (equivalent to 12 units). In addition, almost the entire facade of the buildings has changed. We also submit that eliminating the four balconies on Henry Street serves no purpose and is a detriment to the marketing of the building. Further, the Community has not suggested these cuts. The Inner City Civic Association voted in favor of the project as it was shown to them at their meeting in October, which included the units suggested to be removed as well as the balconies. Finally, we firmly believe the design of the building with these units is preferable to the design with the units removed, as can be seen in the attached comparisons.

Second, our recommended four affordable housing units exceeds the current City Council policy. We are recommending one two bedroom unit, two one bedroom units and one one bedroom/den unit, which amounts to approximately \$650,000.00 to \$700,000.00 depending on the final sales price of the units. That's \$2.92 per square foot, 2.25 times the current City Council policy. In addition, all the other projects to which this one is compared received either affordable housing bonuses, or were a part of a text amendment for increased density, except Cameron Station, which was \$2.00 per square foot. In addition, we are providing these units on site, near Metro, which is desired by the City's Office of Housing.

In that vein, we would suggest the following changes Condition Nos. 1(l), 1(p), 1(t), 1(w), 85 and 89, to which staff does not agree:

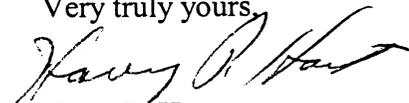
11. ~~The four proposed balconies within building # 2 shall be eliminated and replaced with traditional two-over-two windows as provided within the remainder of the building.~~

Mr. Eric Wagner, Chairman
and Members of Planning Commission
November 3, 2004
Page 4

- 1p. ~~For building # 1, two units shall be eliminated on the Henry Street facade as generally depicted in Attachment # 1. The Henry Street facade shall be revised as generally depicted in Attachment #1a shall be revised to provide the appearance of a more traditional style and a three-story element rather than the five-story element as currently depicted. The roof for the upper floor of this element shall be slate or synthetic slate.~~
- 1t. The central and western portion of the facade shall be revised by eliminating 2 units as generally depicted in Attachment #a. The roof for the upper floor shall be slate or synthetic slate.
- 1w. The southeastern portion of the building shall be revised as generally depicted in Attachment # 3a. The roof for the upper floor shall be slate or synthetic slate.
85. The developer shall provide one ~~three~~ affordable two-bedroom set-aside units at a price not to exceed \$225,000 and two affordable one-bedroom units at a price not to exceed \$175,000 and one one-bedroom/den unit at a price not to exceed \$175,000 (prices include one garage parking space for each unit) for sale to households with at least one member who lives or works in the City of Alexandria, and whose incomes do not exceed the City's income guidelines for the Moderate Income Housing Program (MIHP), which are currently \$68,700 for a one or two person household and \$79,500 for a three or more person household. (Housing)
89. The City shall have exclusive right to market the set-aside units for 90 days, after which the developer shall also have the right to market the units, at the affordable price, to buyers qualified for the set-aside program. The developer is encouraged to refer potentially qualified buyers to the Office of Housing's Implementation Division for participation in the set-aside and MIHP programs. In the event a qualified buyer cannot be found for a set-aside unit by the time the unit is completed and ready for occupancy, the developer shall have the right to sell the unit at full market price, and shall contribute to the Housing Trust Fund the equivalent of \$ 2.92 ~~4.28~~ per gross square foot of each unit sold. (Housing)

With these amended conditions we hope that you will recommend approval of the Development Special Use Permit.

Very truly yours,


Harry P. Hart

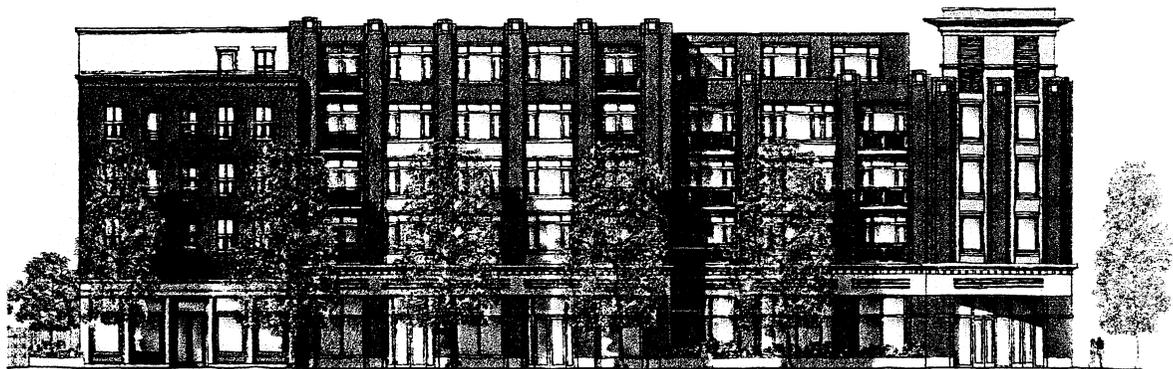
cc: M. O. Ibrahim

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Comparisons



NORTH HENRY STREET ELEVATION
(per City's recommendation)

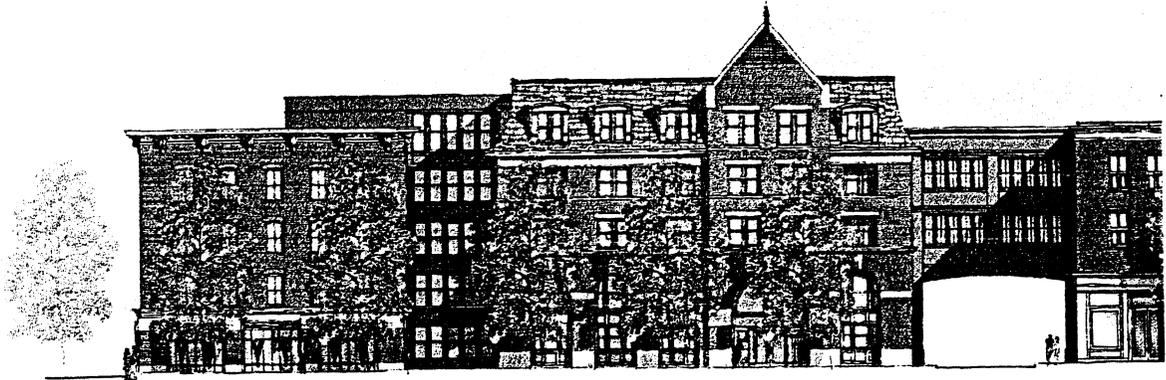


NORTH HENRY STREET ELEVATION
(as proposed)

DIAMOND
PROPERTIES, LLC

THE MONARCH
54

**rust
orling
and
neale
architects**



ORONOCO STREET ELEVATION
(per City's recommendation)



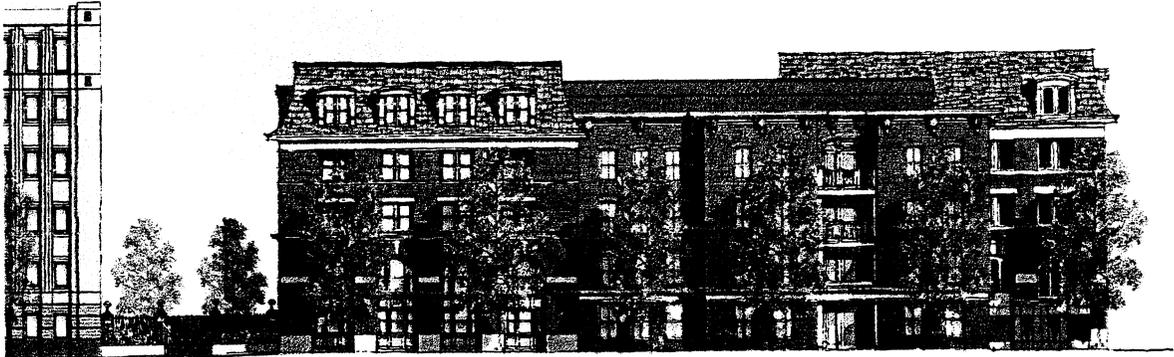
ORONOCO STREET ELEVATION
(as proposed)

DIAMOND
PROPERTIES, LLC

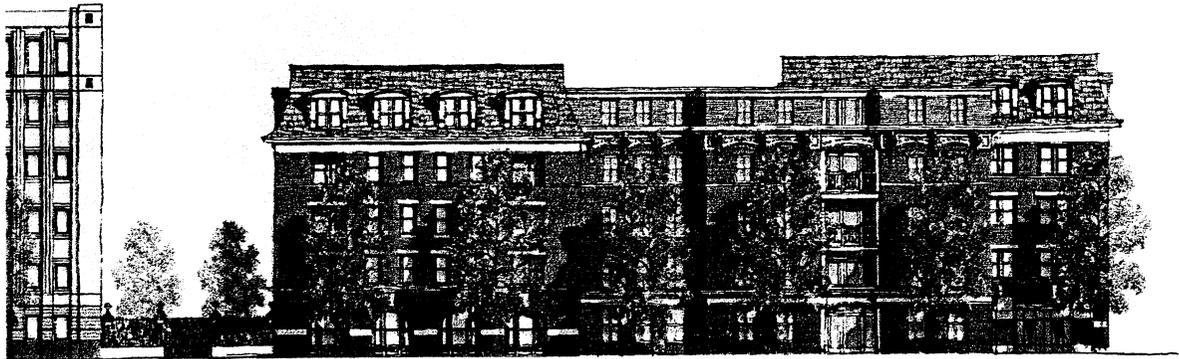
THE MONARCH

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orling
and
neale
architects**



FAYETTE STREET ELEVATION
(per City's recommendation)



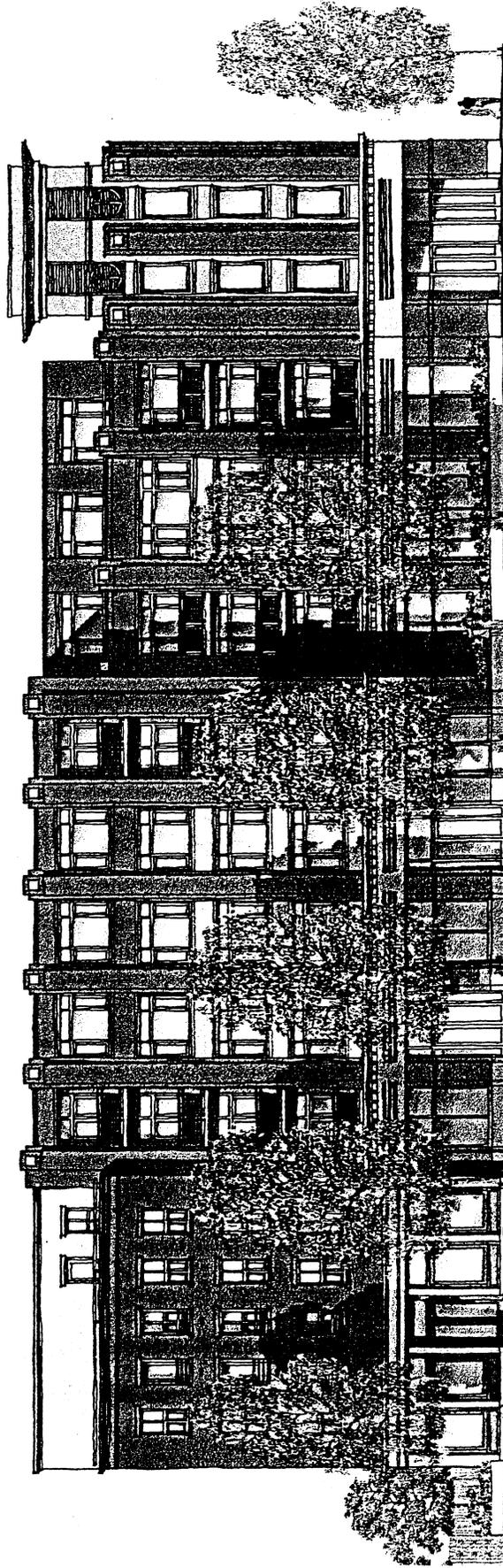
FAYETTE STREET ELEVATION
(as proposed)

DIAMOND
PROPERTIES.LLC

THE MONARCH

**rust
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and
neale
architects**

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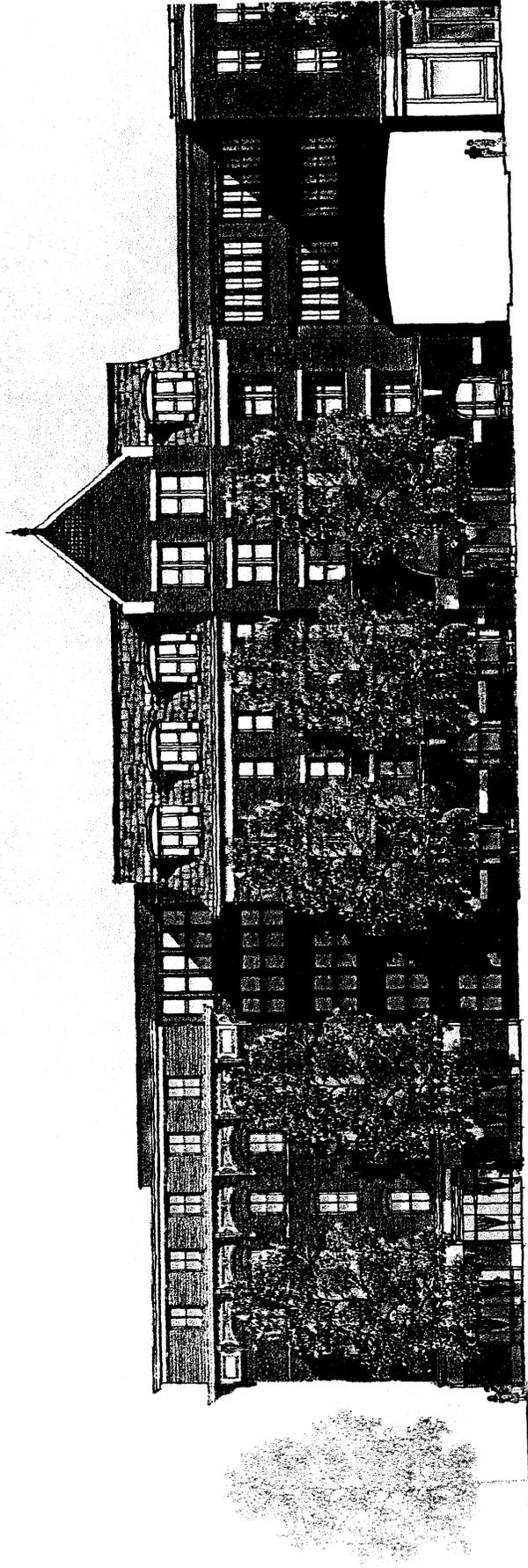


NORTH HENRY STREET ELEVATION
(as proposed)

DIAMOND
PROPERTIES, LLC

**rust
orling
and
neale
architects**

Attached for

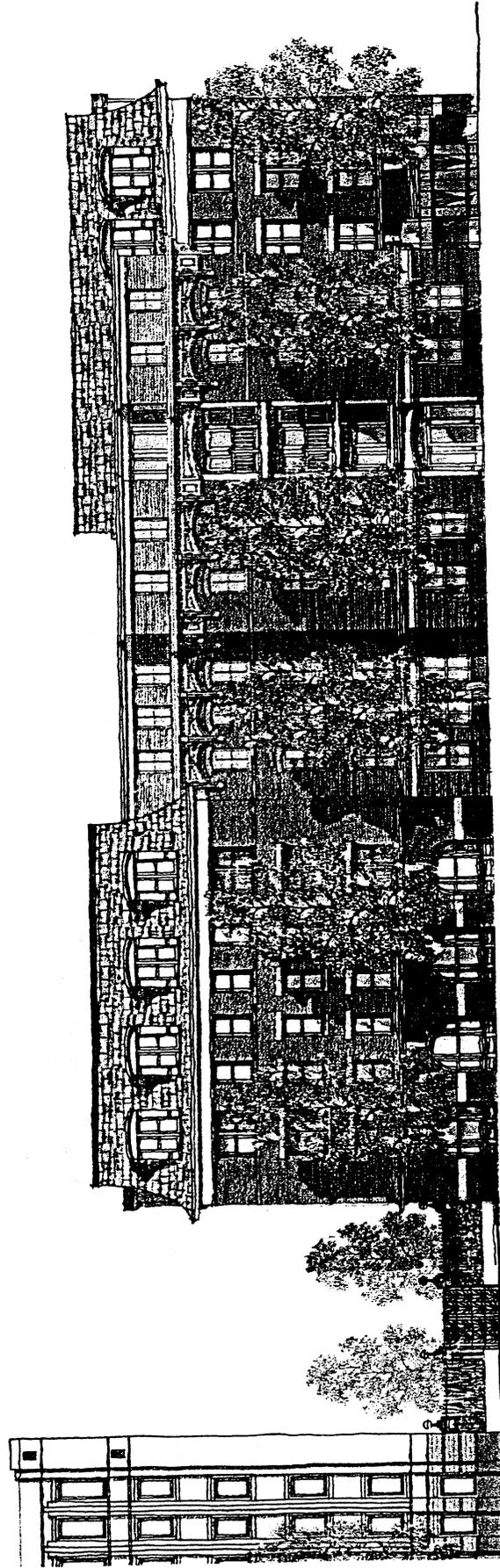


ORONOCO STREET ELEVATION
(as proposed)

DIAMOND
PROPERTIES, LLC

**rust
orling
and
neale
architects**

Attachment 2a



FAYETTE STREET ELEVATION
(as proposed)

DIAMOND
PROPERTIES, LLC

**rust
orling
and
neale
architects**

Attachment 3a

Third AMENDED
APPLICATION for
DEVELOPMENT SPECIAL USE PERMIT with SITE PLAN
DSUP # 2003-0019

PROJECT NAME: The Monarch

PROPERTY LOCATION: Block Bounded by N. Henry, Oronoco, Pendleton and N. Fayette Streets

TAX MAP REFERENCE: 64.01-03-01, -02, -04, -06, -07, -09 ZONE: CRMU-H

APPLICANT Name: Diamond Alexandria LLC

Address: 3919 Old Lee Hy #89a, Fairfax Va 22030

PROPERTY OWNER Name Diamond Alexandria LLC

Address: 3919 Old Lee Hy #89a, Fairfax Va 22030

SUMMARY OF PROPOSAL: Request to redevelop the Hennage Creative Printer Block with a
15,000 square foot retail center with 168 condominiums above, while reducing and screening
the power substation.

MODIFICATIONS REQUESTED: [REDACTED] reduction in the minimum
zone transition from the adjacent residential zone and reduction in the minimum 75' vision
clearance.

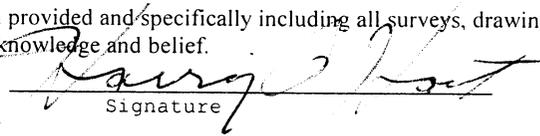
SUP's REQUESTED: FAR of 2.33.

THE UNDERSIGNED hereby applies for Development Site Plan, with Special Use Permit, approval in accordance with the provisions of Title 7, Chapter 5 of the Code of the City of Alexandria, Virginia.

THE UNDERSIGNED, having obtained permission from the property owner, hereby grants permission to the City of Alexandria to post placard notices on the property for which this application is requested, pursuant to Article XI, Section 11-301(B) of the 1992 Zoning Ordinance of the City of Alexandria, Virginia.

THE UNDERSIGNED also attests that all of the information herein provided and specifically including all surveys, drawings, etc., required of the applicant are true, correct and accurate to the best of their knowledge and belief.

Harry P. Hart
Print Name of Applicant or Agent


Signature

HART, CALLEY, GIBBS & KARP, P.C.
Mailing Address

(703) 836-5757
Telephone Number

307 N. Washington St., Alex. VA 22314
Mailing Address

10-7-04
Date

DO NOT WRITE BELOW THIS LINE - OFFICE USE ONLY

Application Received: _____
Fee Paid & Date: \$ _____
Legal Advertisement: _____

Received Plans for Completeness: _____
Received Plans for Preliminary: _____
Property Placard: _____

ACTION - PLANNING COMMISSION: _____

ACTION - CITY COUNCIL: _____

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All Applicants must complete this form. Supplemental forms are required for child care facilities, restaurants, auto oriented uses and freestanding signs requiring special use permit approval.

1. The Applicant is the (check one) Owner Contract Purchaser

Lessee or Other: _____

State the name, address and percent of ownership of any person or entity owning an interest in the applicant, unless the entity is a corporation in which case identify each owner of more than ten percent.

Diamond Properties, L.L.C., 3919 Old Lee Hwy, Suite 82A, Fairfax, VA 22030 – 4%

Virginia Investments, L.L.C., 3919 Old Lee Hwy, Suite 82A, Fairfax, VA 22030 – 48%

Ahmed Al-Matawakil, 3919 Old Lee Hwy, Suite 82A, Fairfax, VA 22030 – 12%

Ahmed Y. Al-Hussein, 3919 Old Lee Hwy, Suite 82A, Fairfax, VA 22030 – 12%

Al-Husain Y. Al-Hussein, 3925 Old Lee Hwy, Suite 100, Fairfax, VA 22030 – 12%

Yahya M. Al-Hussein, 3925 Old Lee Hwy, Suite 100, Fairfax, VA 22030 – 12%

If property owner or applicant is being represented by an authorized agent such as an attorney, realtor, or other person for which there is some form of compensation, does this agent or the business in which the agent is employed have a business license to operate in the City of Alexandria, Virginia?

Yes. Provide proof of current City business license.

No. The agent shall obtain a business license prior to filing application, if required by the City Code.

NARRATIVE DESCRIPTION

2. The applicant shall describe below the nature of the request in detail so that the Planning Commission and City Council can understand the nature of the operation and the use, including such items as the nature of the activity, the number and type of patrons, the number of employees, the hours, how parking is to be provided for employees and patrons, and whether the use will generate any noise. (Attach additional sheets if necessary.)

The Applicant, Diamond Alexandria, L.L.C. requests approval of the redevelopment of the Hennage Creative Printer block. The applicant proposes to build an approximately 15,000 square foot retail center with 168 condominium units above in two architecturally elegant buildings, all within walking distance of the Braddock Road Metro Station. A development special use permit is requested because the applicant proposes a 2.33 FAR building under the CRMU-H zone.

The proposal provides this neighborhood with retail uses within walking distance, as well as increasing the number of homeownership opportunities within walking distance of the Braddock Road Metro Station. All parking will be provided in two levels of underground parking as well as 13 surface parking spaces. The retail and the residential units will each use the underground garage.

The retail stores will occupy the ground floor of the N. Henry Street side of each building and the 168 condominium units will occupy the floors above the retail on the N. Henry Street Side as well as the Fayette Street side of both the buildings, stepped back at various heights around the building to better fit in with the neighboring properties, some of which will redevelop in the future, particularly the warehouses to the west of the property across N. Fayette Street.

The condominium will provide 76 one-bedroom units and 92 two-bedroom units. Included within this proposal is to provide either a contribution to the affordable housing trust fund commensurate with the gross square footage of the building (approximately 255,000 square feet) or three affordable housing units on site for a contribution of approximately \$475,000.00.

The proposal also reduces the size of the Dominion Virginia Power substation at the southwest corner of the block as well as providing significant screening of the remaining, smaller substation, removing an eyesore from this neighborhood.

- 3. How many patrons, clients, pupils and other such users do you expect? Specify time period (i.e., day, hour, or shift).

Normal amount for retail stores of this size.

- 4. How many employees, staff and other personnel do you expect? Specify time period (i.e. day, hour, or shift).

Normal amount for retail stores of this size.

- 5. Describe the proposed hours and days of operation of the proposed use:

Day	Hours	Day	Hours
<u>Sunday to Saturday</u>	<u>24 hours a day</u>	_____	_____
_____	_____	_____	_____
_____	_____	_____	_____

- 6. Describe any potential noise emanating from the proposed use:

A. Describe the noise levels anticipated from all mechanical equipment and patrons.
Normal levels from retail store use, and all deliveries will take place under the cover of first floor of the condos.

B. How will noise from patrons be controlled?
All patron noise will be contained within the stores itself, except accessory noise from patrons entering and exiting the stores. Noise will be controlled by the insulation.

- 7. Describe any potential odors emanating from the proposed use and plans to control them:

All odors from food preparation will be ventilated so as not to be a nuisance, if food stores are provided. No specific leases have been signed for the retail.

Development Special Use Permit with Site Plan (DSUP) #

2003-0019

8. Provide information regarding trash and litter generated by the use:

A. What type of trash and garbage will be generated by the use?

Normal type from retail stores and residential use.

B. How much trash and garbage will be generated by the use.?

Approximately 2 dumpsters per week.

C. How often will trash be collected?

Once a week, or more if necessary on certain occasions.

D. How will you prevent littering on the property, streets and nearby properties?

The retail stores and the condominium association's maintenance workers will monitor the site to prevent littering.

9. Will any hazardous materials, as defined by the state or federal government, be handled, stored, or generated on the property?

[] Yes. [X] No.

If yes, provide the name, monthly quantity, and specific disposal method below:

10. Will any organic compounds, for example, paint, ink, lacquer thinner, or cleaning or degreasing solvent, be handled, stored, or generated on the property?

[] Yes. [X] No.

If yes, provide the name, monthly quantity, and specific disposal method below:

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11. What methods are proposed to ensure the safety of residents, employees and patrons?

The retail stores will appropriately monitor their stores to ensure safety and the
residences will be secured at the entry points to the condominium.

ALCOHOL SALES

12. Will the proposed use include the sale of beer, wine, or mixed drinks?
 Yes. No.

If yes, describe alcohol sales below, including if the ABC license will include on-premises and/or off-premises sales. Existing uses must describe their existing alcohol sales and/or service and identify any proposed changes in that aspect of the operation.

PARKING AND ACCESS REQUIREMENTS

13. Provide information regarding the availability of off-street parking:

A. How many parking spaces are required for the proposed use pursuant to section 8-200 (A) of the zoning ordinance?

260 for the residences, and ⁷³ for the retail stores.

B. How many parking spaces of each type are provided for the proposed use:

288 Standard spaces (9 feet x 18.5 feet)

74 Compact spaces (8 feet x 16 feet)

12 Handicapped accessible spaces.

____ Other.

Development Special Use Permit with Site Plan (DSUP) #

2003-0019

- C. Where is required parking located? (check one) on site [] off-site.

If the required parking will be located off-site, where will it be located?

Pursuant to section 8-200 (C) of the zoning ordinance, commercial and industrial uses may provide off-site parking with 500 feet of the proposed use, provided that the off-site parking is located on land zoned for commercial or industrial uses. All other uses must provide parking on-site, except that off street parking may be provided within 300 feet of the use with a special use permit.

- D. If a reduction in the required parking is requested, pursuant to section 8-100(A)(4) or (5) of the zoning ordinance, complete the PARKING REDUCTION SUPPLEMENTAL APPLICATION.

14. Provide information regarding loading and unloading facilities for the use:

- A. How many loading spaces are required for the use, per section 8-200 (B) of the

zoning ordinance? 1 for retail stores.

- B. How many loading spaces are available for the use? 1

- C. Where are off-street loading facilities located? On-site in covered surface parking

- D. During what hours of the day do you expect loading/unloading operations to occur?

During regular business hours.

- E. How frequently are loading/unloading operations expected to occur, per day or per week, as appropriate?

On a daily basis for the retail stores.

15. Is street access to the subject property adequate or are any street improvements, such as a new turning lane, necessary to minimize impacts on traffic flow?

Street access is adequate.

DIAMOND ALEXANDRIA
City of Alexandria, Virginia

Traffic Impact Analysis

Prepared for:

Diamond Alexandria, LLC
Fairfax, VA

Prepared by:



Gorove/Slade Associates, Inc.

April 22, 2004

1140 Connecticut Ave, NW
Suite 700
Washington, DC 20036
202-296-8625

1175 Herndon Parkway
Suite 600
Herndon, VA 20170
703-787-9595

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EXECUTIVE SUMMARY

This report presents the findings of a traffic impact analysis for the planned development of the Hennage site located in the City of Alexandria, Virginia. The major components of the development include the following:

- ◆ 17,914 square foot grocery store; and
- ◆ 199 condominium units

The proposed development is scheduled to be complete by the year 2005. The analysis completed in this report analyzed future conditions through 2010.

Based on analysis, the following major conclusions were determined:

Existing Conditions (2003)

The existing intersection capacity analyses show that the intersections studied will operate at an acceptable level of service "D" or better during the A.M., P.M., and Saturday peak hours.

Build Year (2005) without Development (Future Background) Conditions

The future background capacity analyses show that the intersections studied will operate at an acceptable level of service "D" or better during the A.M., P.M., and Saturday peak hours.

Build Year (2005) with Development (Total Future) Conditions

The total future capacity analyses show that the intersections studied will operate at an acceptable level of service "D" or better during the A.M., P.M., and Saturday peak hours.

Build Year with Development +5 Years Additional Growth (2010) Conditions

The total future with five years additional growth condition capacity analyses show that, after a slight adjustment in the signal timings at Pendleton Street with Patrick Street during the AM peak hour, the intersections studied will operate at an overall level of service "D" or better during the A.M., P.M., and Saturday peak hours.

INTRODUCTION

This report presents the findings of a traffic impact analysis for the redevelopment of the Hennage Site with a 17,914 square foot retail center and 199 condominium units in Alexandria, Virginia. The project is scheduled to be complete by the year 2005. The major components of the development include the following:

- ◆ A 17,914 square foot specialty retail center;
- ◆ 199 condominium units;
- ◆ 395 below-grade parking spaces; and
- ◆ 29 at-grade parking spaces (including 2 handicap spaces);

The following tasks were undertaken as part of this study in accordance with direction received from city staff and documented in a scoping letter dated October 27, 2003:

- ◆ Field reconnaissance in the vicinity of the site was performed to collect information related to existing traffic controls, roadway geometry, and traffic flow characteristics;
- ◆ Manual vehicle traffic turning movement counts were conducted at six intersections within the vicinity of the site, during morning, evening, and Saturday peak hours;
- ◆ Future traffic conditions were projected based on the development's square footage, number of condominium units, and area transit usage; and
- ◆ Intersection capacity analyses were performed for existing, build year without development (2005), build year with development (2005), and build year with development +5 years additional background growth (2010) peak hour traffic conditions at the intersections contained within the study area.

Sources of data for this study include the City of Alexandria, the Virginia Department of Transportation (VDOT), and the office files and field reconnaissance efforts of Gorove/Slade Associates.

Scope of Study

The following intersections were identified for inclusion in this study:

- ◆ Fayette Street and Pendleton Street;
- ◆ Fayette Street and Oronoco Street;
- ◆ Henry Street (U.S. 1 Southbound) and Pendleton Street;
- ◆ Henry Street (U.S. 1 Southbound) and Oronoco Street;
- ◆ Patrick Street (U.S. 1 Northbound) and Pendleton Street; and
- ◆ Patrick Street (U.S. 1 Northbound) and Oronoco Street

This report presents a discussion and the findings of analyses performed for the following conditions:

- ◆ ***Existing Conditions (2003):***
Considers existing traffic volumes and existing roadway configurations during the A.M., P.M. and Saturday peak hours;
- ◆ ***Build Year (2005) without Development (Future Background) Conditions:***
Considers existing traffic volumes during the A.M., P.M. and Saturday peak hours after applying a growth rate of 3% per year for 2 years to account for an increase in background traffic;
- ◆ ***Build Year (2005) with Development (Total Future) Conditions:***
Considers the total future condition 2005 during the A.M., P.M. and Saturday peak hours, which consists of future background 2005 traffic volumes and future site volumes generated from 17,914 square feet of retail use and 199 condominium units;
- ◆ ***Build Year with Development +5 Years Additional Growth (2010) Conditions:***
Considers the total future condition with 5 years additional background growth at 3% per year, analyzed during the A.M., P.M. and Saturday peak hours in 2010.

Site Area Transportation System Characteristics

The project site is located near several major commuting corridors in Northern Virginia. Directly east of the site is Henry Street, the southbound component of a one-way pair of streets that create Route 1 in the middle of the City of Alexandria. The northbound one-way element of Route 1 is Patrick Street. Route 1 is a major north-south commuting corridor, running parallel to I-95/395 and the George Washington Memorial Parkway. It links to I-95/495 approximately one mile south of the site. Duke Street, a major east-west corridor, is one-half mile south of the site.

Roadway Network within the Vicinity of the Site

The regional roadway network and site location are given in Figure 1. A description of the major roadways linked to the site given below; note the roadways described below refer to the existing intersections within the immediate vicinity of the site. Figure 2 provides an illustration of the existing lane configuration of the roadway network.

Henry Street (U.S. Route 1 Southbound) is a three-lane arterial roadway with a lane for parking on the west side of the street. It is the southbound half of a one-way pair near the vicinity of the site, and borders the project site to the east. Henry Street is signal-controlled at its intersection with both Pendleton Street and Oronoco Street, is synchronized with the Route 1 coordination system. The roadway consists of one through lane, a shared left-through lane, and a shared right-through lane in the southbound direction. Restrictions in place on Henry Street are that one lane is reserved for high occupancy vehicles (HOV) from 3:00-7:00 P.M., and trucks are required to travel in the center lane. Also, there are pedestrian crosswalks at all approaches near the site.

Patrick Street (U.S. Route 1 Northbound) is a three-lane arterial roadway with a lanes for parking on the both sides of the street. It is the northbound half of a one-way pair near the vicinity of the site. Patrick Street is signal-controlled at its intersection with both Pendleton Street and Oronoco Street, is synchronized with the Route 1 coordination system. The roadway consists of one through lane, a shared left-through lane, and a shared right-through lane in the northbound direction. Restrictions in place on Patrick Street are that one lane is reserved for high occupancy vehicles (HOV) from 6:00-9:00 A.M., and trucks are required to travel in the center lane.

Fayette Street is a two-lane roadway that runs north-south in the vicinity of the site and borders the project site to the west. It has on-street parking on both sides of the street. Fayette Street is stop controlled (4-way) at its intersection with both Pendleton and Oronoco Streets, and consists of one left-through-right lane on all approaches.

Pendleton Street is a two-lane roadway that runs east-west in the vicinity of the site, and has some parking on both sides of the street. Pendleton Street is a four-way stop-controlled intersection at Fayette Street, and consists of a shared left-through-right lane along both the eastbound and westbound approaches to Fayette Street. Pendleton Street is signal-controlled at Henry and Patrick Streets. It consists of an eastbound shared through-right lane and a westbound through lane and left turn bay at Henry Street, and a shared westbound through-right lane and eastbound shared through-left lane at Patrick Street.

Oronoco Street is a two-lane roadway that runs east-west in the vicinity of the site, has parking on both sides of the street, and borders the project site to the south. Oronoco Street is stop-controlled along at Fayette Street, and consists of one left-through-right lane on both the eastbound and westbound approaches. Oronoco Street is signal-controlled at Henry and Patrick Streets, and features one-lane approaches to both intersections in either direction.



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Figure 1
Site Location Map

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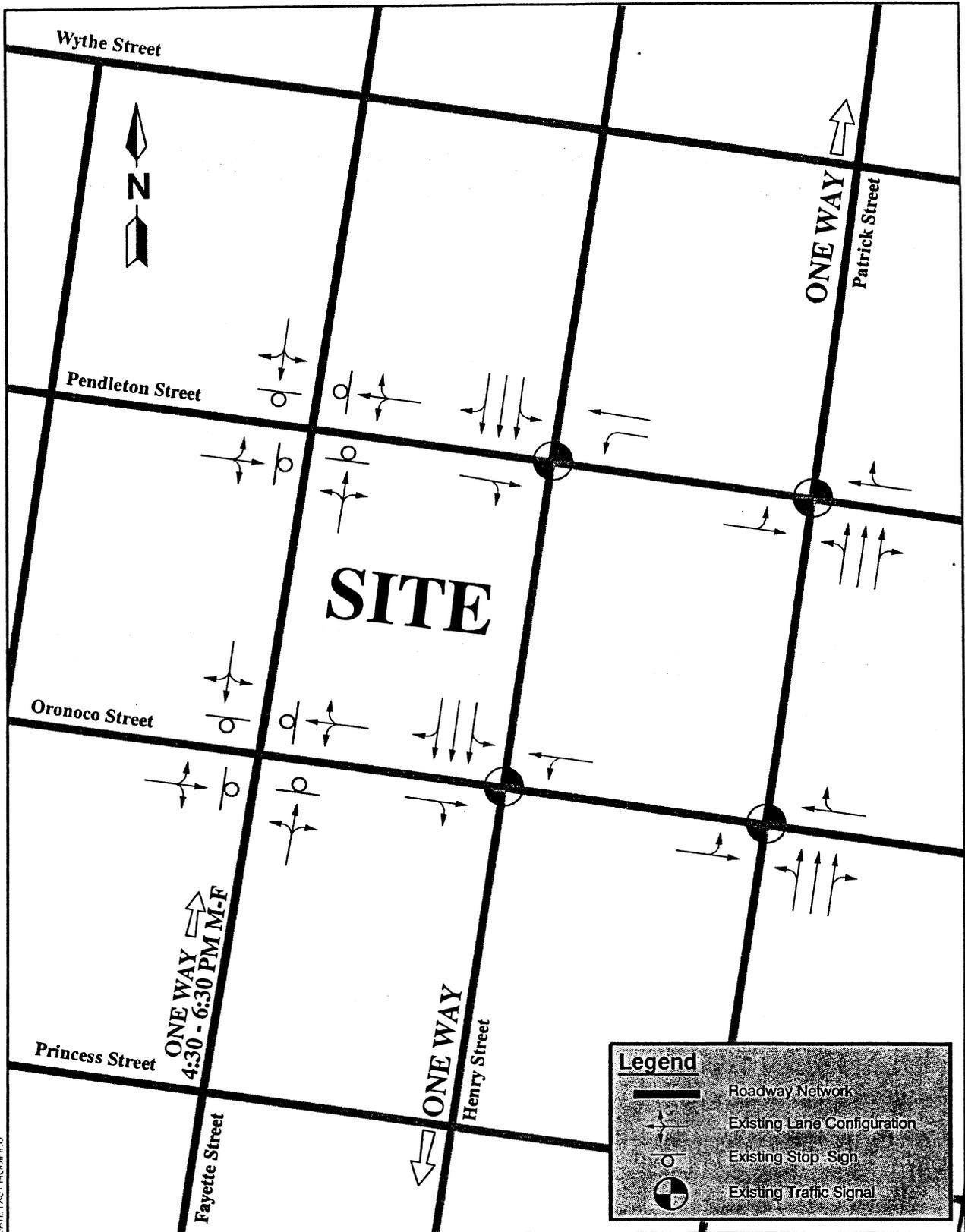


Figure 2
Local Roadway Network



EXISTING (2003) TRAFFIC CONDITIONS

Existing (2003) Traffic Volumes

Existing weekday and Saturday peak hour traffic counts were conducted at the intersections of Henry and Pendleton Streets, Henry and Oronoco Streets, Fayette and Pendleton Streets, and Fayette and Oronoco Streets on Tuesday, September 9 and Saturday, October 4, 2003. Weekday and Saturday peak hour traffic counts were conducted on Thursday, October 23 and Saturday, October 25, 2003 at the intersections of Patrick and Pendleton Streets, and Patrick and Oronoco Streets. The A.M. commuter peak hour occurs from 8:00 A.M. to 9:00 A.M., the P.M. commuter peak hour occur from 5:15 P.M. to 6:15 P.M., and the Saturday midday peak takes place from 12:30 P.M. to 1:30 P.M. The volumes during these commuter peak hours were considered in the analysis. The existing traffic volumes are illustrated in Figure 3.

Existing (2003) Capacity Analysis

Capacity analyses for the intersections within the study area were performed for the critical A.M. and P.M. commuter peak hours, as well as the Saturday midday peak hour. They were analyzed with Highway Capacity Manual methodology using the Highway Capacity Software 2000 (HCS 4.1c). The results of the capacity analyses can be found in Table 1, and are expressed in levels of service (LOS) and delay (in seconds).

Table 1
Existing (2003) Capacity Analysis Results

Intersection	Movement	LOS (Delay)		
		AM	PM	Saturday
Fayette Street and Oronoco Street <i>Unsignalized</i>	Overall Intersection	A (7.86)	A (7.91)	A (7.63)
	Eastbound Approach	A (8.05)	A (7.83)	A (7.63)
	Westbound Approach	A (7.49)	A (7.87)	A (7.61)
	Northbound Approach	A (7.84)	A (7.85)	A (7.55)
	Southbound Approach	A (7.81)	A (8.00)	A (7.71)
Fayette Street and Pendleton Street <i>Unsignalized</i>	Overall Intersection	A (8.81)	A (9.37)	A (8.24)
	Eastbound Approach	A (9.31)	A (9.57)	A (8.38)
	Westbound Approach	A (8.21)	A (9.16)	A (8.04)
	Northbound Approach	A (8.57)	A (8.90)	A (8.03)
	Southbound Approach	A (8.53)	A (9.63)	A (8.37)
Henry Street and Oronoco Street <i>Signalized</i>	Overall Intersection	B (12.3)	B (15.0)	B (14.1)
	Eastbound Approach	C (20.7)	C (20.8)	C (20.0)
	Westbound Approach	B (19.7)	C (20.5)	C (20.1)
	Southbound Approach	B (11.2)	B (14.3)	B (13.6)
Henry Street and Pendleton Street <i>Signalized</i>	Overall Intersection	B (12.7)	B (15.8)	B (14.8)
	Eastbound Approach	C (22.2)	C (22.3)	C (21.3)
	Westbound Approach	B (19.7)	C (20.4)	B (19.9)
	Southbound Approach	B (10.7)	B (14.8)	B (13.9)

**Table 1 (cont.)
Existing (2003) Capacity Analysis Results**

Intersection	Movement	LOS (Delay)		
		AM	PM	Saturday
Patrick Street and Oronoco Street <i>Signalized</i>	Overall Intersection	B (17.8)	B (12.5)	B (13.5)
	Eastbound Approach	C (20.3)	B (20.0)	C (20.2)
	Westbound Approach	C (20.7)	C (20.5)	C (20.1)
	Northbound Approach	B (17.5)	B (11.6)	B (12.9)
Patrick Street and Pendleton Street <i>Signalized</i>	Overall Intersection	C (21.5)	B (14.3)	B (14.1)
	Eastbound Approach	C (22.9)	C (22.6)	C (21.2)
	Westbound Approach	C (20.7)	C (22.5)	C (21.0)
	Northbound Approach	C (21.4)	B (11.9)	B (12.9)

The results of the existing capacity analysis indicate that all of the intersections operate at an acceptable level of service during the A.M. and P.M. commuter peak hours, as well as during the Saturday peak hour. The results of the capacity analyses are shown in Figure 4. The detailed analysis worksheets are contained in the Technical Appendix.

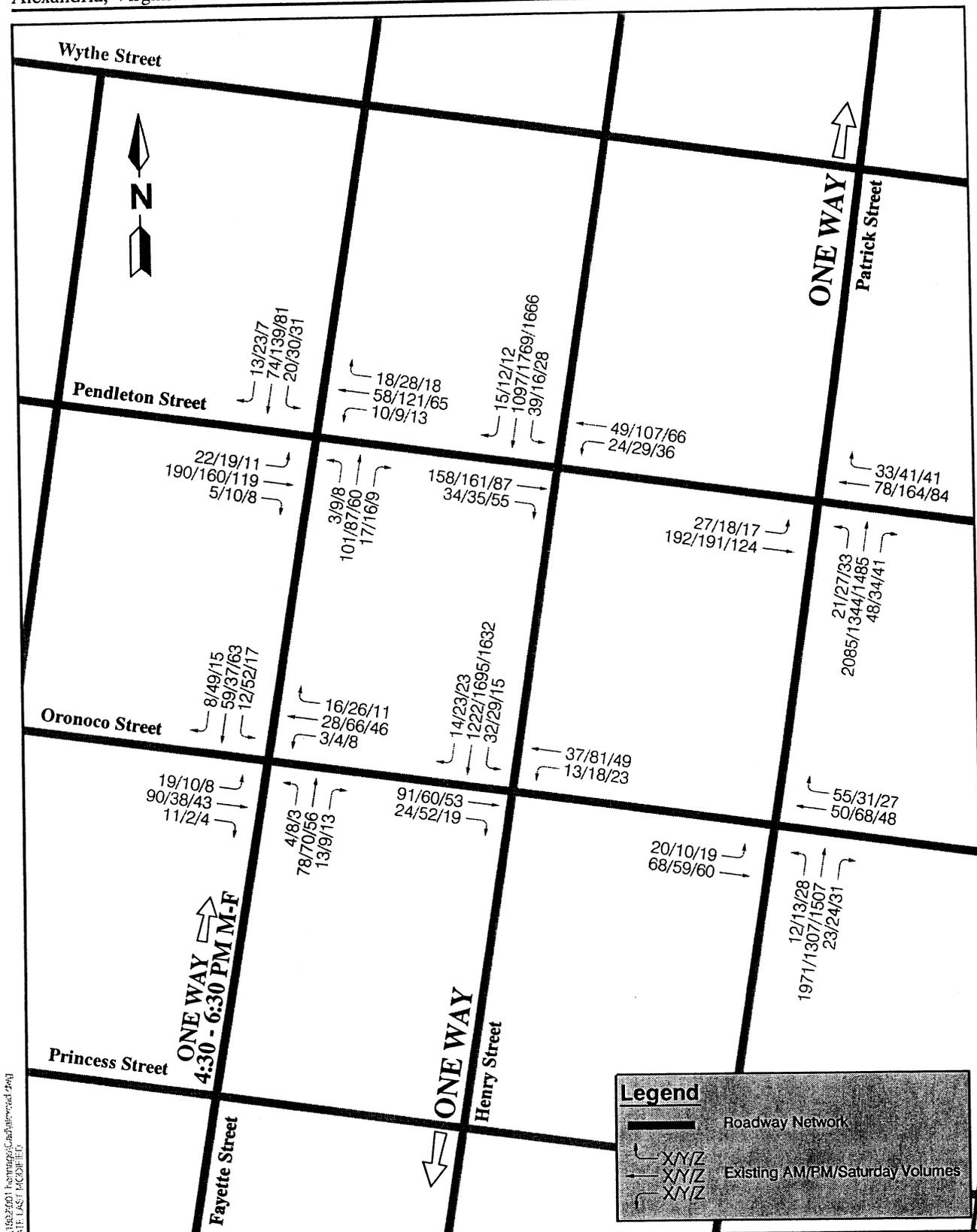


Figure 3
Existing (2003) Peak Hour Volumes
8:00 - 9:00 AM & 5:15 - 6:15 PM; Saturday 12:30 - 1:30PM



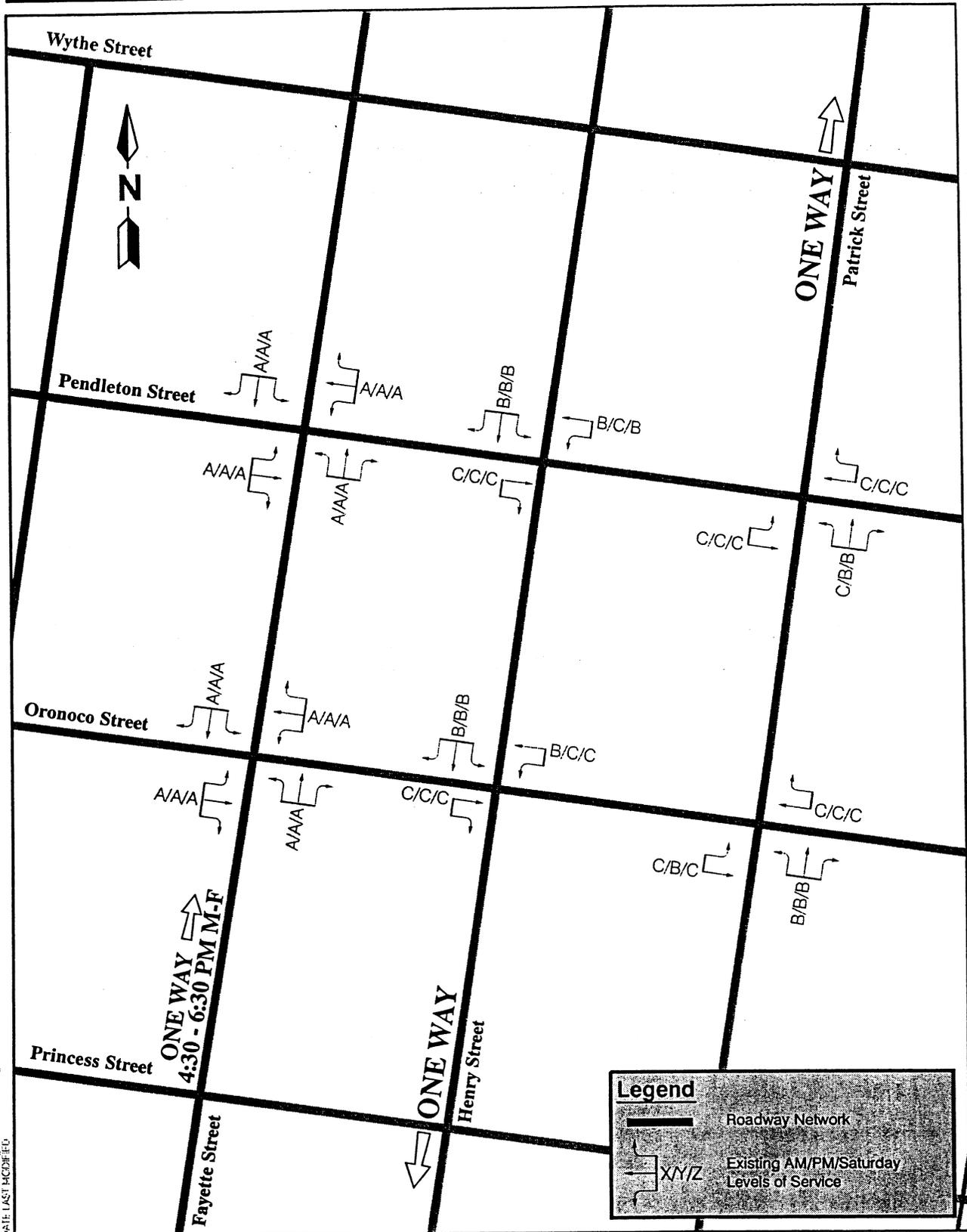


Figure 4
Existing (2003) Peak Hour Levels of Service

BUILD YEAR WITHOUT DEVELOPMENT (2005) CONDITIONS

Planned Roadway Improvements

There are no known roadway improvements planned near the vicinity of the site.

Future Background (2005) Traffic Volumes

The construction of the proposed development is anticipated to be complete in 2005. Based on historical data provided by VDOT, an inherent growth rate of 3 percent per year over a two-year period for a 6.09% total increase to existing traffic volumes was taken into consideration to account for regional increases in traffic due to background growth and development. Traffic generated by the nearby developments of Potomac Yards and the Patent & Trade Office was assumed to be part of background growth, not considered as traffic in addition to inherent growth. The inherent traffic growth was added to the existing volumes in order to establish future background 2005 traffic volumes without Hennage site trips. The future background traffic volumes are shown in Figure 5.

Future Background (2005) Capacity Analysis

Capacity analyses for the intersections within the study area were performed for the critical A.M. and P.M. commuter peak hours, as well as the Saturday midday peak hour. They were analyzed with Highway Capacity Manual methodology using the Highway Capacity Software 2000 (HCS 4.1c). The results of the capacity analyses can be found in Table 2, and are expressed in levels of service (LOS) and delay (in seconds).

Table 2
Future Background (2005) Capacity Analysis Results

Intersection	Movement	LOS (Delay)		
		AM	PM	Saturday
Fayette Street and Oronoco Street <i>Unsignalized</i>	Overall Intersection	A (7.94)	A (7.96)	A (7.67)
	Eastbound Approach	A (8.14)	A (7.88)	A (7.67)
	Westbound Approach	A (7.55)	A (7.92)	A (7.65)
	Northbound Approach	A (9.72)	A (7.90)	A (7.58)
	Southbound Approach	A (7.89)	A (8.06)	A (7.76)
	Fayette Street and Pendleton Street <i>Unsignalized</i>	Overall Intersection	A (9.00)	A (9.55)
Eastbound Approach	A (9.57)	A (9.76)	A (8.46)	
Westbound Approach	A (8.34)	A (9.32)	A (8.11)	
Northbound Approach	A (8.73)	A (9.03)	A (8.09)	
Southbound Approach	A (8.69)	A (9.83)	A (8.44)	
Henry Street and Oronoco Street <i>Signalized</i>	Overall Intersection	B (12.6)	B (15.6)	B (14.7)
	Eastbound Approach	C (20.9)	C (20.8)	C (20.1)
	Westbound Approach	B (19.7)	C (20.6)	C (20.2)
	Southbound Approach	B (11.6)	B (15.0)	B (14.2)

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**Table 2 (cont.)
Future Background (2005) Capacity Analysis Results**

Intersection	Movement	LOS (Delay)		
		AM	PM	Saturday
Henry Street and Pendleton Street <i>Signalized</i>	Overall Intersection	B (13.0)	B (16.5)	B (15.3)
	Eastbound Approach	C (22.5)	C (22.5)	C (21.4)
	Westbound Approach	B (19.7)	C (20.5)	B (19.9)
	Southbound Approach	B (11.0)	B (15.5)	B (14.5)
Patrick Street and Oronoco Street <i>Signalized</i>	Overall Intersection	C (20.4)	B (12.8)	B (13.9)
	Eastbound Approach	C (20.4)	B (20.0)	C (20.2)
	Westbound Approach	C (20.8)	C (20.6)	C (20.1)
	Northbound Approach	C (20.4)	B (11.9)	B (13.3)
Patrick Street and Pendleton Street <i>Signalized</i>	Overall Intersection	C (27.6)	B (14.6)	B (14.5)
	Eastbound Approach	C (23.3)	C (22.9)	C (21.4)
	Westbound Approach	C (20.8)	C (22.7)	C (21.1)
	Northbound Approach	C (28.4)	B (12.2)	B (13.3)

The results of the future background capacity analysis indicate that all of the intersections operate at an acceptable level of service during commuter peak hours. The results of the future background capacity analyses can be found in Figure 6. The detailed analysis worksheets are contained in the Technical Appendix.

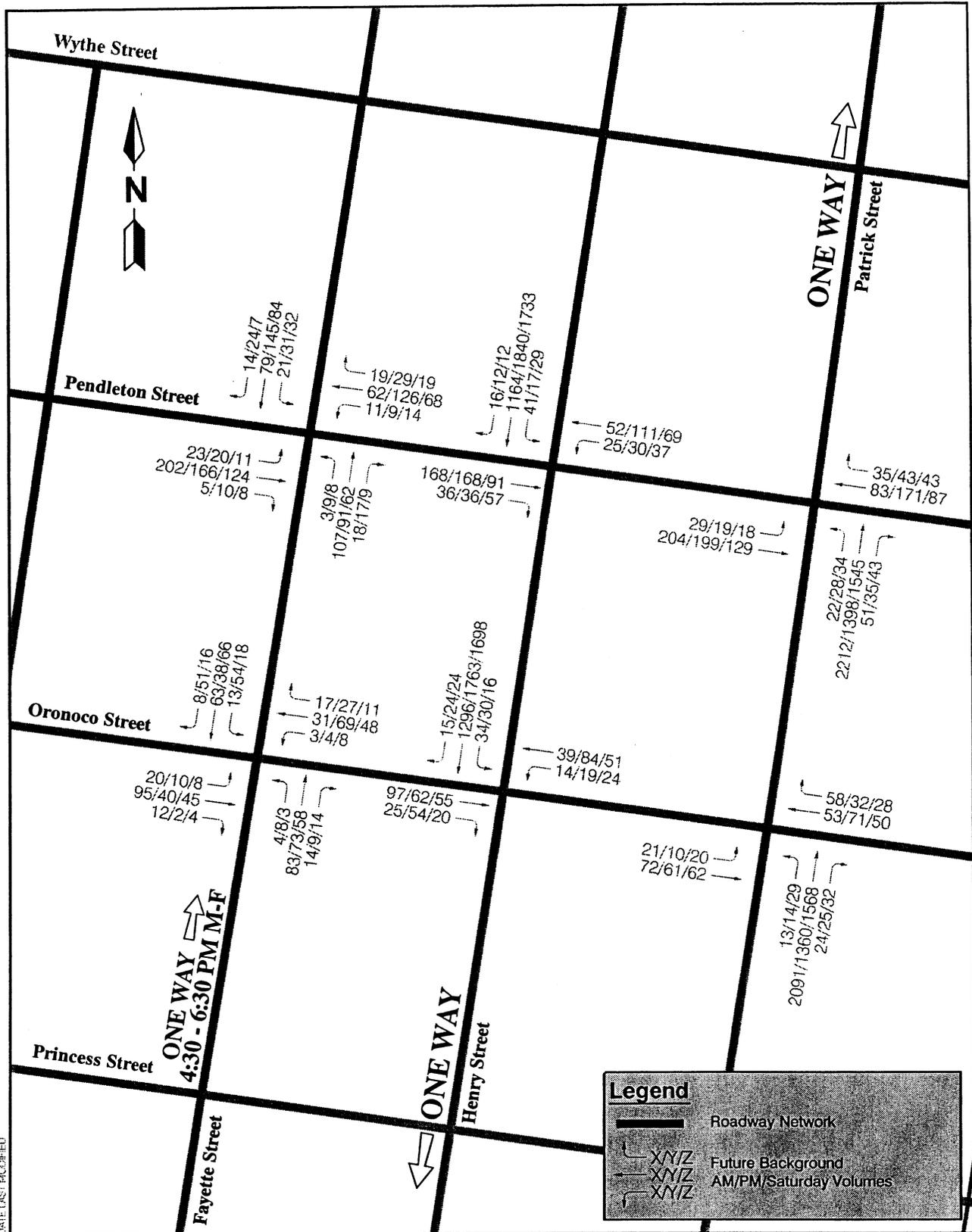


Figure 5
Future Background (2005) Peak Hour Volumes

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Figure 6
Future Background (2005) Peak Hour Levels of Service



BUILD YEAR WITH DEVELOPMENT (2005) CONDITIONS

Site Access

This residential & retail development will have two driveways that will serve the patrons of the property. The driveway access points will be off Pendleton Street and Oronoco Street. When entering this access, the driver of the vehicle may either go into the garage entrance near Pendleton Street, or park in one of the 22 on-site parking spaces.

Site Generated Traffic Volumes

In order to determine the appropriate trip generation for this new development the Institute of Transportation Engineers (ITE) *Trip Generation Manual, 6th Edition* was used to determine the trips into and out of the proposed site.

When using ITE's trip generation manual, the two land uses used in calculations were #814 (Specialty Retail Center), and #230 (Condominium/Townhouse). The summary of the trips generated for the weekday peak hours as well as the weekday daily traffic volume is presented in Table 3A. The peak hour for the retail center is on Saturday, therefore Saturday peak hour and daily traffic were calculated and presented in Table 3B. A reduction in trips, based on the results of the Development-Related Ridership Survey II performed by the Washington Metropolitan Area Transit Authority (WMATA) in 1989, was taken for the condominium use since the Braddock Street Metrorail Station is approximately one-quarter mile from the site, and a Metrobus stop is located at the intersection of Henry Street and Pendleton Street. The reduction can be seen in Tables 3A and 3B.

Table 3A
Weekday Trip Generation

Land Use	Quantity	AM Peak Hour			PM Peak Hour			Average Daily Traffic		
		In	Out	Total	In	Out	Total	In	Out	Total
Specialty Retail Center	17,914 S.F.	19	14	33	29	36	65	403	402	805
Condominium/Townhouse	199 Units	16	73	89	74	35	109	585	584	1,169
<i>WMATA Reduction (60%)</i>		-10	-44	-53	-44	-21	-65	-351	-351	-701
Total Condominium Trips		6	29	36	30	14	44	234	234	468
Total Proposed Trips		25	43	68	59	50	109	637	636	1,273

Table 3B
Saturday Trip Generation

Land Use	Quantity	Saturday Peak Hour			Saturday Daily Traffic		
		In	Out	Total	In	Out	Total
Specialty Retail Center	17,914 S.F.	38	38	76	377	377	745
Condominium/Townhouse	199 Units	54	46	100	574	574	1,148
<i>WMATA Reduction (60%)</i>		-32	-28	-60	-344	-345	-689
Total Condominium Trips		22	18	40	230	229	459
Total Proposed Trips		60	56	116	607	606	1,213

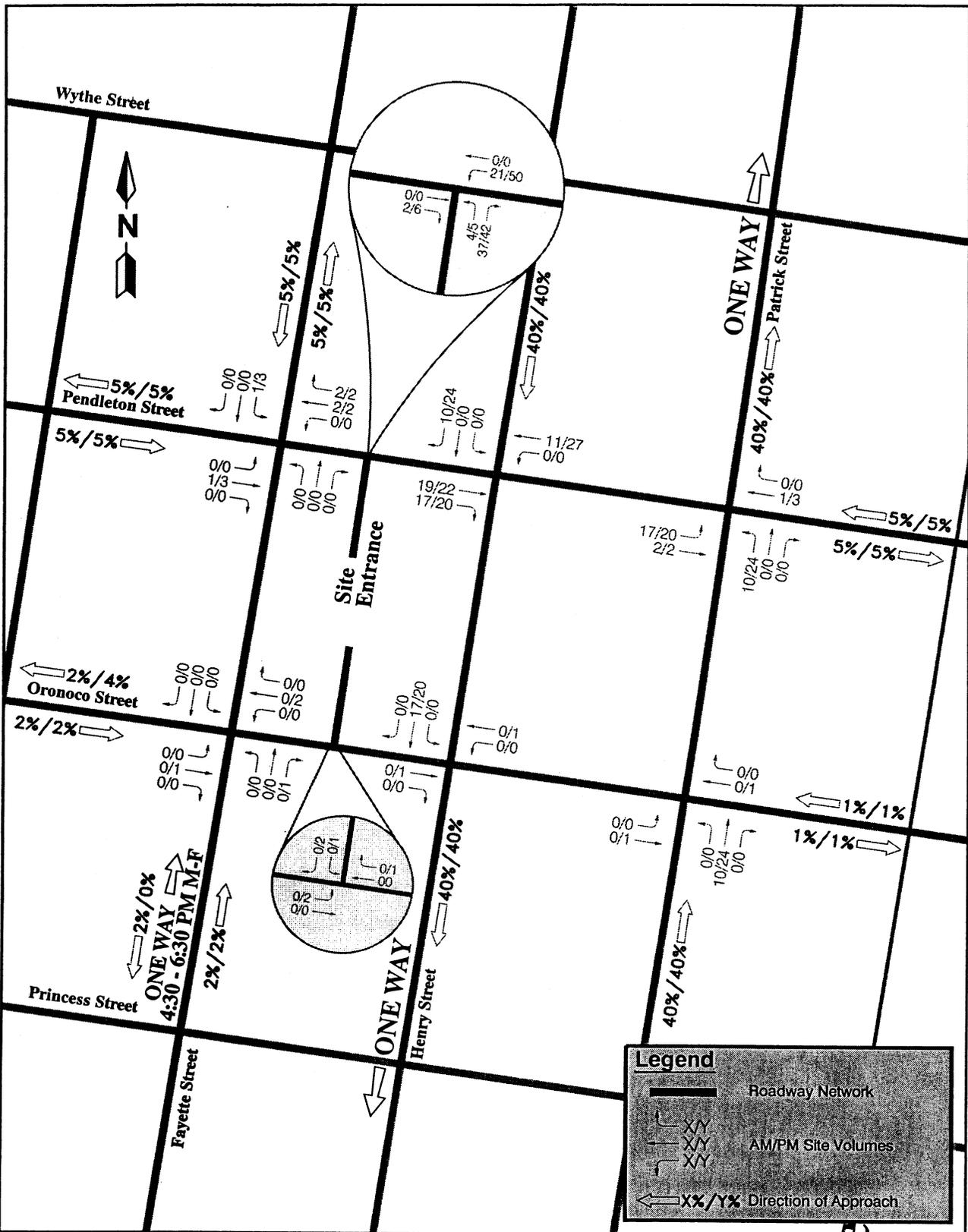
Directional Distribution and Trip Assignment

Based on the location of the subject site in the City of Alexandria and existing data, the assumption was made that the majority of the approaching traffic to the site will generally come on Henry Street and Patrick Street. Direction of approach percentages were found based on existing volumes and anticipated traffic patterns. The inbound trips estimated for the A.M. and P.M. peak hours were routed on the roadway network to the site based on the direction of approach assumptions. There is a sign prohibiting vehicles from traveling southbound on Fayette Street, south of Oronoco Street, from 4:30 P.M. to 6:30 P.M. This sign is to thwart cut-through traffic through the local neighborhoods. Though there is existing traffic making this movement, none of the site-generated traffic was assigned to this movement during the P.M. peak hour because of the sign. The inbound and outbound site traffic assignment for the weekday A.M. and P.M. peak hours is illustrated in Figure 7 and the Saturday peak hour is shown in Figure 8.

Parking

There will be both surface and garage parking provided at the Diamond Alexandria site. The surface parking will consist of 29 parking spaces and will be provided for the retail portion of the site. Two parking levels are planned for the residential development that will consist of a total of 395 parking spaces. Studies performed for multifamily residential units in proximity to area Metro stations have reported parking demand ranging from 1 to 1.2 parking spaces per unit. The proposed parking is consistent with city code and equates to almost 2 spaces per dwelling unit. Therefore, it may be possible to reduce parking and still meet the projected demand at this facility.

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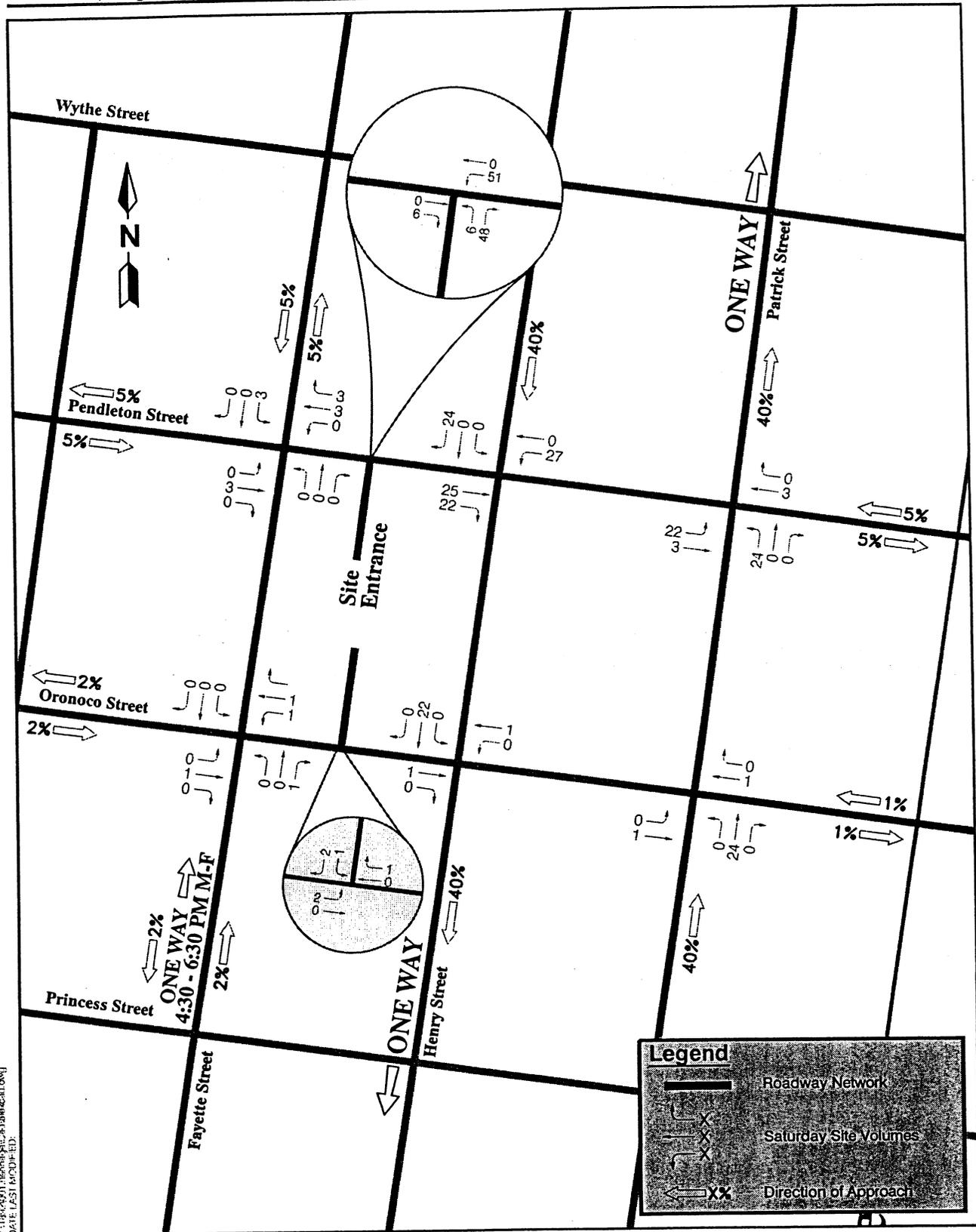
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Figure 7
Weekday Site Generated Traffic Volumes and Direction of Approach

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Total Future (2005) Capacity Analysis

Capacity analyses were performed to determine the operational levels of service of the studied intersections for the build year with development (total future) traffic conditions. Total future traffic volumes are comprised of the trips that will be generated by the subject site added to the background traffic volumes already on the roadway network. The total future traffic volumes are shown in Figure 9. The results of the level of service analysis for total future traffic conditions are presented in Table 4.

Table 4
Total Future (2005) Capacity Analysis Results

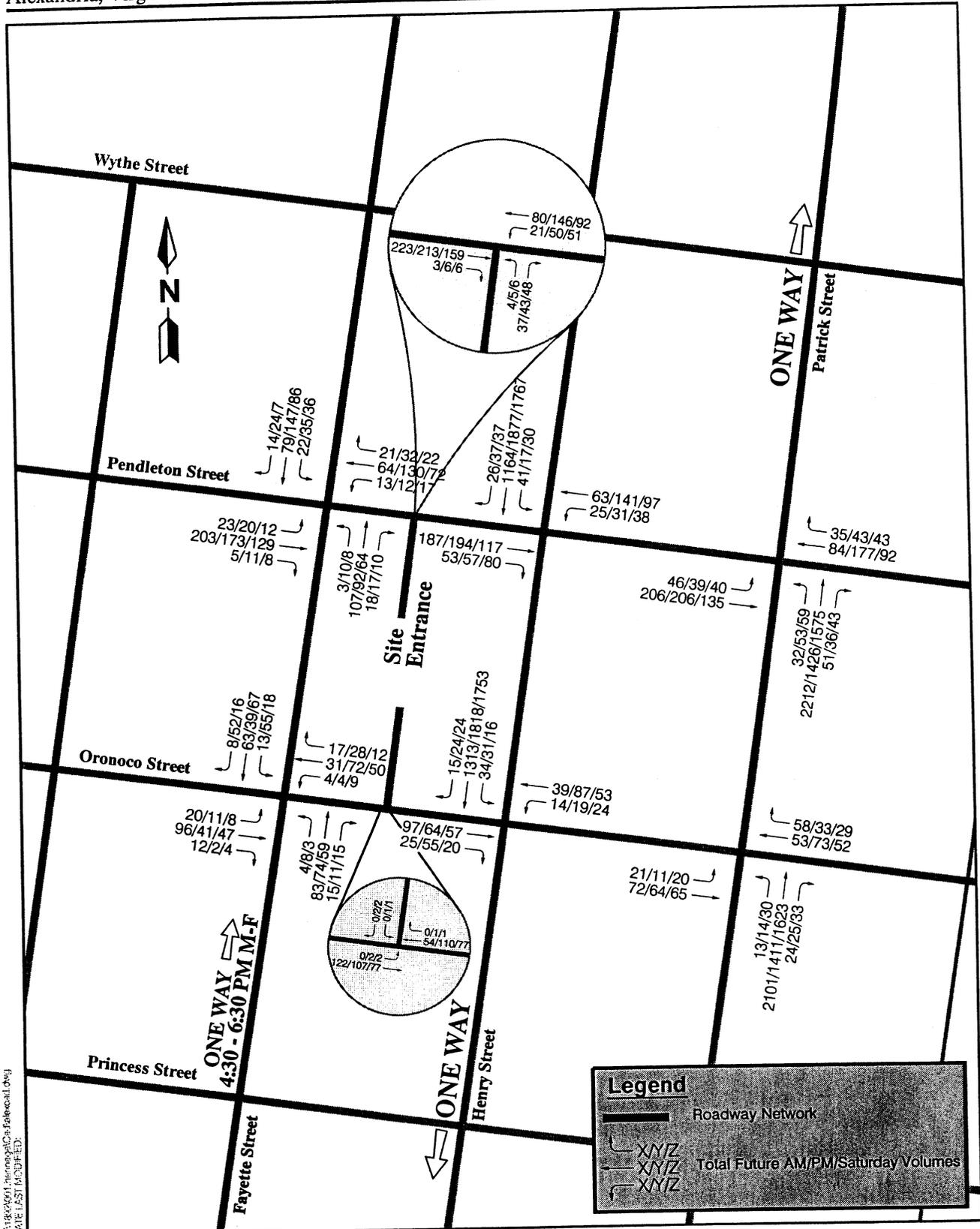
Intersection	Movement	LOS (Delay)		
		AM	PM	Saturday
Fayette Street and Oronoco Street <i>Unsignalized</i>	Overall Intersection	A (7.95)	A (8.01)	A (7.70)
	Eastbound Approach	A (8.16)	A (7.91)	A (7.70)
	Westbound Approach	A (7.58)	A (7.97)	A (7.68)
	Northbound Approach	A (7.93)	A (7.93)	A (7.61)
	Southbound Approach	A (7.90)	A (8.11)	A (7.78)
Fayette Street and Pendleton Street <i>Unsignalized</i>	Overall Intersection	A (9.02)	A (9.73)	A (8.41)
	Eastbound Approach	A (9.60)	A (9.96)	A (8.57)
	Westbound Approach	A (8.37)	A (9.51)	A (8.20)
	Northbound Approach	A (8.75)	A (9.16)	A (8.16)
	Southbound Approach	A (8.72)	B (10.03)	A (8.56)
Henry Street and Oronoco Street <i>Signalized</i>	Overall Intersection	B (12.7)	B (16.2)	B (15.2)
	Eastbound Approach	C (20.9)	C (20.9)	C (20.1)
	Westbound Approach	B (19.7)	C (20.6)	C (20.2)
	Southbound Approach	B (11.7)	B (15.6)	B (14.7)
Henry Street and Pendleton Street <i>Signalized</i>	Overall Intersection	B (13.5)	B (17.5)	B (16.2)
	Eastbound Approach	C (23.4)	C (23.9)	C (22.5)
	Westbound Approach	B (19.9)	C (21.0)	C (20.3)
	Southbound Approach	B (11.0)	B (16.4)	B (15.2)
Patrick Street and Oronoco Street <i>Signalized</i>	Overall Intersection	C (20.7)	B (13.1)	B (14.4)
	Eastbound Approach	C (20.4)	C (20.1)	C (20.3)
	Westbound Approach	C (20.8)	C (20.6)	C (20.2)
	Northbound Approach	C (20.7)	B (12.2)	B (13.8)
Patrick Street and Pendleton Street <i>Signalized</i>	Overall Intersection	C (28.5)	B (15.2)	B (15.0)
	Eastbound Approach	C (24.5)	C (25.0)	C (22.2)
	Westbound Approach	C (20.8)	C (22.8)	C (21.1)
	Northbound Approach	C (29.4)	B (12.6)	B (13.7)

**Table 4 (cont.)
Total Future (2005) Capacity Analysis Results**

Intersection	Movement	LOS (Delay)		
		AM	PM	Saturday
Pendleton Street and Site Driveway <i>Unsignalized</i>	Overall Intersection	n/a	n/a	n/a
	Westbound Left Turn	A (7.7)	A (7.7)	A (7.6)
	Northbound Approach	A (9.7)	A (9.9)	A (9.5)
Oronoco Street and Site Driveway 2 <i>Unsignalized</i>	Overall Intersection	n/a	n/a	n/a
	Eastbound Left Turn	A (7.3)	A (7.4)	A (7.3)
	Southbound Approach	A (8.5)	A (9.1)	A (8.9)

The results of the total future (2005) capacity analysis indicate that all of the intersections under A.M., P.M., and Saturday peak hour conditions operate at an acceptable level of service. Particularly, both site driveways operate at a LOS "A". The results of the total future capacity analyses are shown in Figure 10. The detailed HCS analysis worksheets are contained in the Technical Appendix.

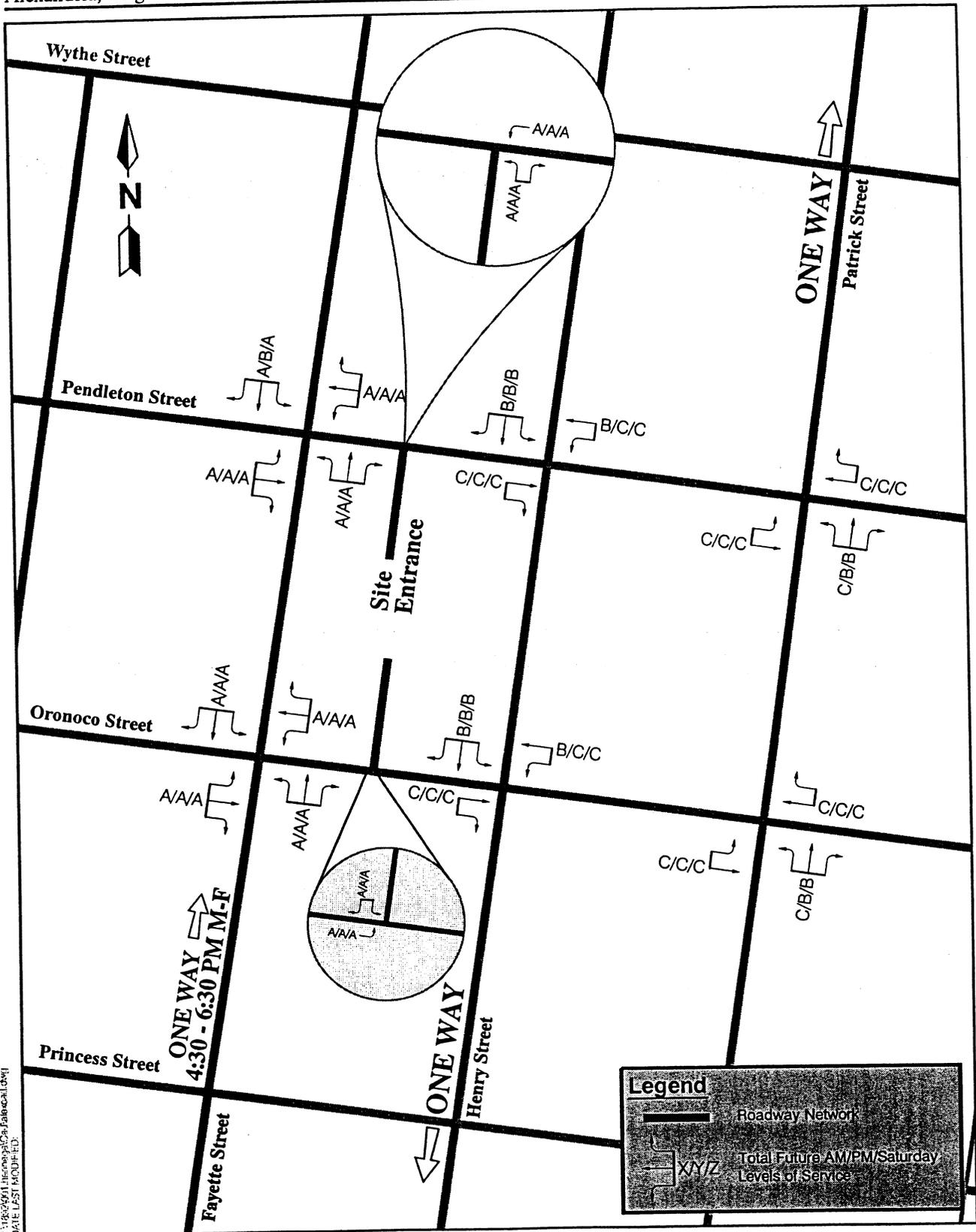
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Figure 10
Total Future (2005) Peak Hour Levels of Service

BUILD YEAR +5 YEARS ADDITIONAL GROWTH (2010) CONDITIONS

Total Future +5 Years (2010) Traffic Volumes

The inherent growth rate of 3 percent per year used in the future background (2005) condition was also assumed for the total future with five years of additional growth condition. The traffic volumes used in this scenario includes the existing (2003) volume, a 23.0% background growth (3% per year for 7 years) of the existing (2003) traffic volumes, and the site-generated volumes. The total future traffic volumes with the 5 years of additional inherent growth are shown in Figure 11.

Total Future +5 Years (2010) Capacity Analysis

Capacity analyses were performed to determine the operational levels of service of the studied intersections for conditions occurring 5 years beyond the build year. The results of the level of service analysis for this condition are presented in Table 5. The detailed analysis worksheets are contained in the Technical Appendix.

Table 5
Total Future (2010) Capacity Analysis Results

Intersection	Movement	LOS (Delay)		
		AM	PM	Saturday
Fayette Street and Oronoco Street <i>Unsignalized</i>	Overall Intersection	A (8.20)	A (8.28)	A (7.88)
	Eastbound Approach	A (8.45)	A (8.12)	A (7.86)
	Westbound Approach	A (7.75)	A (8.25)	A (7.86)
	Northbound Approach	A (8.18)	A (8.18)	A (7.79)
	Southbound Approach	A (8.12)	A (8.43)	A (7.98)
Fayette Street and Pendleton Street <i>Unsignalized</i>	Overall Intersection	A (9.63)	B (10.66)	A (8.99)
	Eastbound Approach	B (10.43)	B (10.97)	A (8.52)
	Westbound Approach	A (8.74)	B (10.35)	A (8.57)
	Northbound Approach	A (9.25)	B (9.82)	A (8.46)
	Southbound Approach	A (9.18)	B (11.09)	A (8.94)
Henry Street and Oronoco Street <i>Signalized</i>	Overall Intersection	B (13.8)	C (21.7)	B (19.1)
	Eastbound Approach	C (21.2)	C (21.2)	C (20.3)
	Westbound Approach	B (19.9)	C (21.0)	C (20.4)
	Southbound Approach	B (12.9)	C (21.8)	B (19.0)
Henry Street and Pendleton Street <i>Signalized</i>	Overall Intersection	B (14.4)	C (24.6)	C (20.6)
	Eastbound Approach	C (24.8)	C (25.2)	C (23.1)
	Westbound Approach	C (20.1)	C (21.3)	C (20.6)
	Southbound Approach	B (12.0)	C (24.8)	C (20.3)

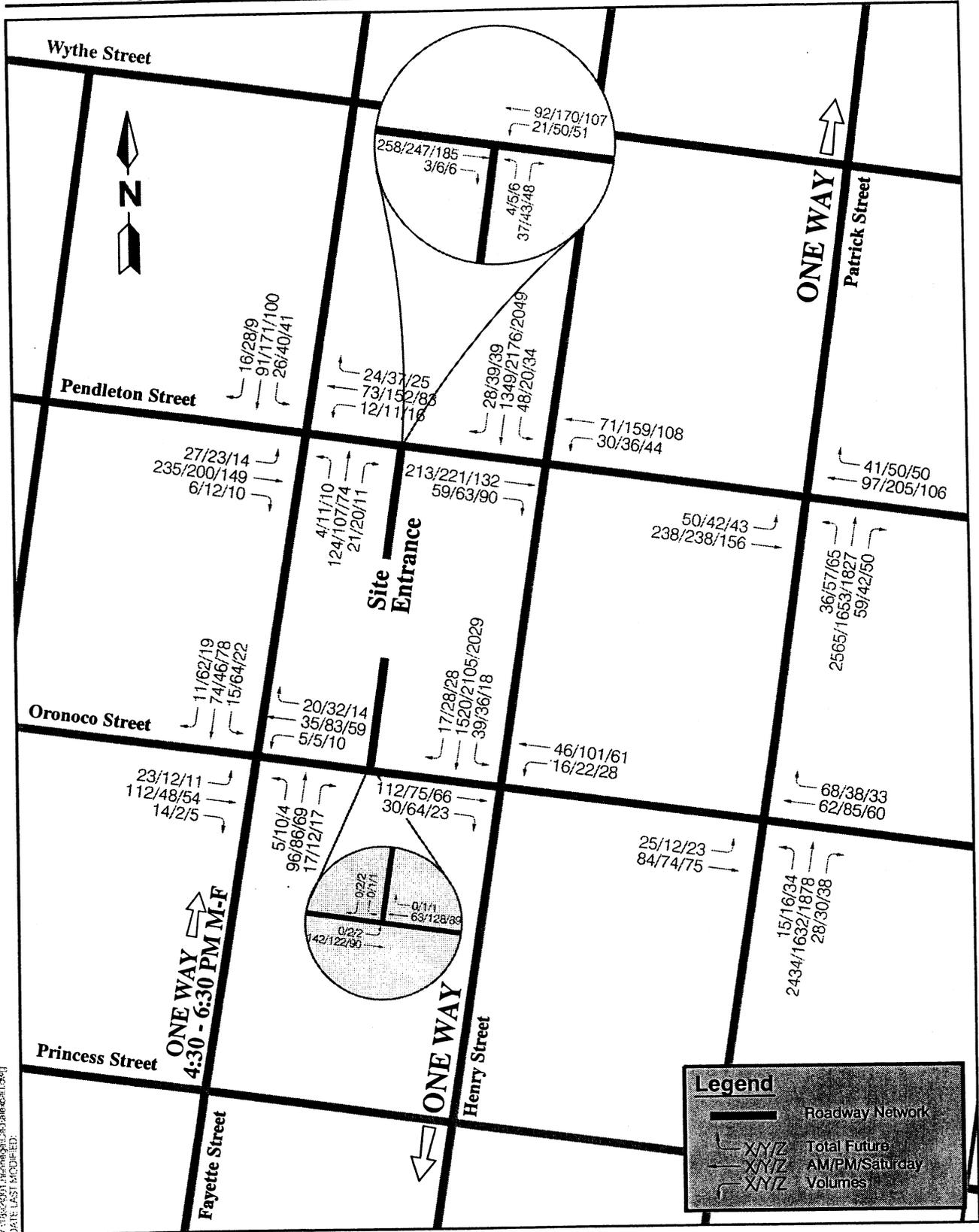
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**Table 5 (cont.)
Total Future (2010) Capacity Analysis Results**

Intersection	Movement	LOS (Delay)		
		AM	PM	Saturday
Patrick Street and Oronoco Street <i>Signalized</i>	Overall Intersection	D (47.4)	B (14.4)	B (17.0)
	Eastbound Approach	C (20.8)	C (20.2)	C (20.5)
	Westbound Approach	C (21.1)	C (20.9)	C (20.4)
	Northbound Approach	D (50.0)	B (13.7)	B (16.7)
Patrick Street and Pendleton Street ¹ <i>Signalized</i>	Overall Intersection	D (42.2)	C (23.9)	B (19.6)
	Eastbound Approach	D (41.8)	B (17.7)	C (21.0)
	Westbound Approach	C (24.5)	B (17.1)	B (20.0)
	Northbound Approach	D (43.2)	C (25.9)	B (19.4)
Pendleton Street and Site Driveway <i>Unsignalized</i>	Overall Intersection	n/a	n/a	n/a
	Westbound Left Turn	A (7.8)	A (7.8)	A (7.7)
	Northbound Approach	B (10.0)	B (10.1)	A (9.7)
Oronoco Street and Site Driveway ² <i>Unsignalized</i>	Overall Intersection	n/a	n/a	n/a
	Eastbound Left Turn	A (7.3)	A (7.5)	A (7.4)
	Southbound Approach	A (8.6)	A (9.2)	A (9.0)

1. Minor Adjustment made to signal timings during the AM, PM, and Saturday Peak Hours.

The results of the total future (2010) capacity analysis indicate that all of the intersections under A.M., P.M., and Saturday peak hour conditions operate at an overall LOS "D" or better except for Pendleton Street with Patrick Street. A minor change in the existing signal timing was made at Patrick and Pendleton Streets during the AM peak period to improve the overall intersection. This is attributable to the background growth that occurs beyond the build-out year. The results of the capacity analyses, with the adjusted signal timing at Patrick and Pendleton, are shown in Figure 12.

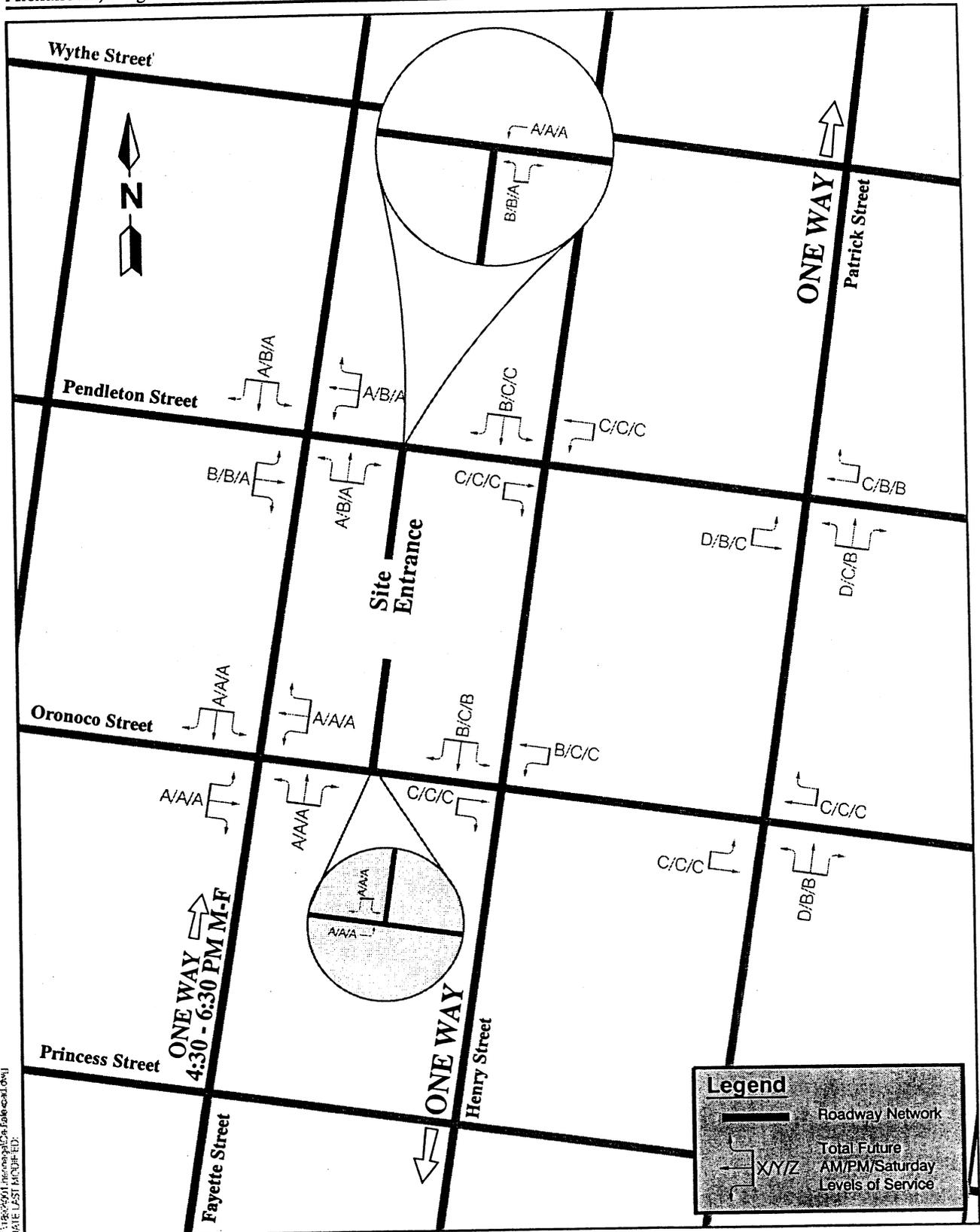


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DATE: LAST MODIFIED

Figure 12
Total Future +5 Years (2010) Peak Hour Levels of Service

TRAFFIC MANAGEMENT PLAN

Designated Transportation Management Plan Coordinator

Plan Coordinator. The developer agrees to designate a Transportation Plan Coordinator to promote and follow through with the Transportation Management Plan.

Ridesharing Marketing

Information Dissemination. The developer agrees to promote ridesharing by displaying information material in high-profile areas.

Transit Program and Metro Check

A Subsidized Program could be implemented.

On-site Construction

Bicycle Storage. Bicycle storage facilities could be provided at the site in a highly visible area, well protected from the weather.

Off-site Construction

Pedestrian Enhancements. No enhancements are necessary due to the crosswalks provided at all four intersections included within this study.

Monitoring and Compliance

The developer will fully comply with the transportation management plan set forth by the City.

Conclusions

This TMP program for the City of Alexandria by the developer will have several components so that these measures will reduce single occupancy vehicle use. The City's goal of reducing peak hour single occupant vehicle work trips will be completed by the following methods implemented by the developer of this site:

- Promote ridesharing
- Designated Transportation Management Plan Coordinator
- Conduct parking management
- Promote transit
- Construct on-site facilities
- Comply with City requirements

CONCLUSIONS

This report presents the findings of a traffic impact and parking analysis for the planned development of the Hennage site located in the City of Alexandria, Virginia. The major components of the development include the following:

- ◆ A 17,914 square foot retail center; and
- ◆ 199 condominium units

The proposed development is scheduled to be complete by the year 2005. The analysis completed in this report analyzed future conditions through 2010.

Based on analysis, the following major conclusions were determined:

Existing Conditions (2003)

The existing intersection capacity analyses show that the intersections studied will operate at an acceptable level of service "D" or better during the A.M., P.M., and Saturday peak hours.

Build Year (2005) without Development (Future Background) Conditions

The future background capacity analyses show that the intersections studied will operate at an acceptable level of service "D" or better during the A.M., P.M., and Saturday peak hours.

Build Year (2005) with Development (Total Future) Conditions

The total future capacity analyses show that, the intersections studied will operate at an acceptable level of service "D" or better during the A.M., P.M., and Saturday peak hours.

Build Year with Development +5 Years Additional Growth (2010) Conditions

The total future with five years additional growth condition capacity analyses show that, after a slight adjustment in the AM signal timings at Pendleton Street with Patrick Street, the intersections studied will operate at an overall level of service "D" or better during the A.M., P.M., and Saturday peak hours.



BOYS & GIRLS CLUBS
OF GREATER WASHINGTON

Alexandria-Olympic Branch
401 North Payne Street
Alexandria, VA 22314
Tel: (703) 549-3077
Fax: (703) 549-1318
www.bgcgw.org

October 28, 2004

To whom it may concern,

This letter is in support of the Hennage Property project that is being considered for your approval. I, as the Executive Director of the Boys & Girls Club in Alexandria, have had nothing but positive meetings and interactions with the group. From the conversations that I have had with the group and the meetings within the community that I have attended, I can only see a positive with the project that they are proposing. The parents, whose children attend the Club, have also seen the project as a worthy one that will not only help the Boys & Girls Club but also the community by offering services and establishing local businesses in the site. I have known several of the people associated with this project for a few years and have only positive things to say about my experiences with them. If you have any further questions of me I can be reached at 703-549-3077.

Sincerely,

Jim Almond
Executive Director
Alexandria-Olympic
Boys & Girls Club

OFFICERS

PRESIDENT
David E. Woodbury, Jr.

VICE-PRESIDENTS
Kerry Adams
Keith Burner
Elissa Dodge
Tonya Rideout

TREASURER
Patrick M. Adams

GENERAL COUNSEL
Peggy Taylor

BOARD MEMBER
EMERTIUS
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Tina LePoer
Michael Lindsay
Michelle Pitts
Carole Rogin
Mark Souder
Tom Skurlock
Jessie Suber

SENIOR BRANCH
DIRECTOR
Jim Almond

DR. ROBERT S. BARRETT
Founder

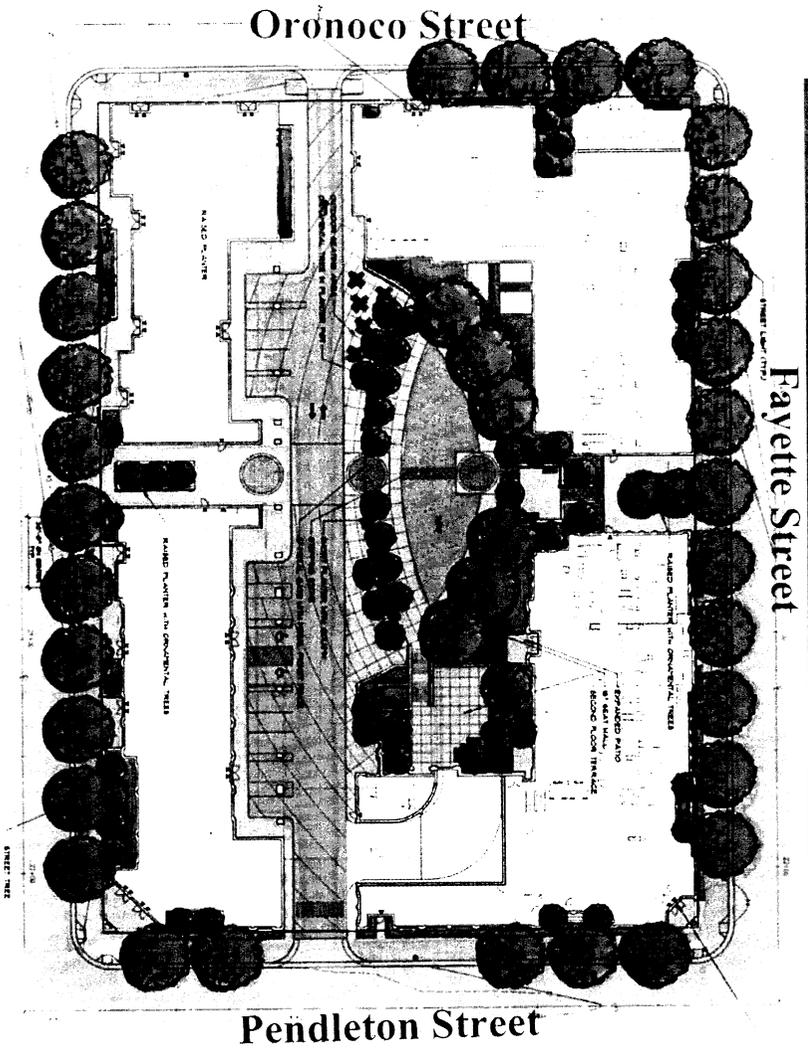
16
11-13-04

Site Characteristics DSUP2003-0019



- 87,174 sq. ft. site
- CRMU-H Zoning
- Height 50-77 ft.
- Braddock Metro Small Area Plan
- FAR: 1.5 – 2.5 (w/SUP)

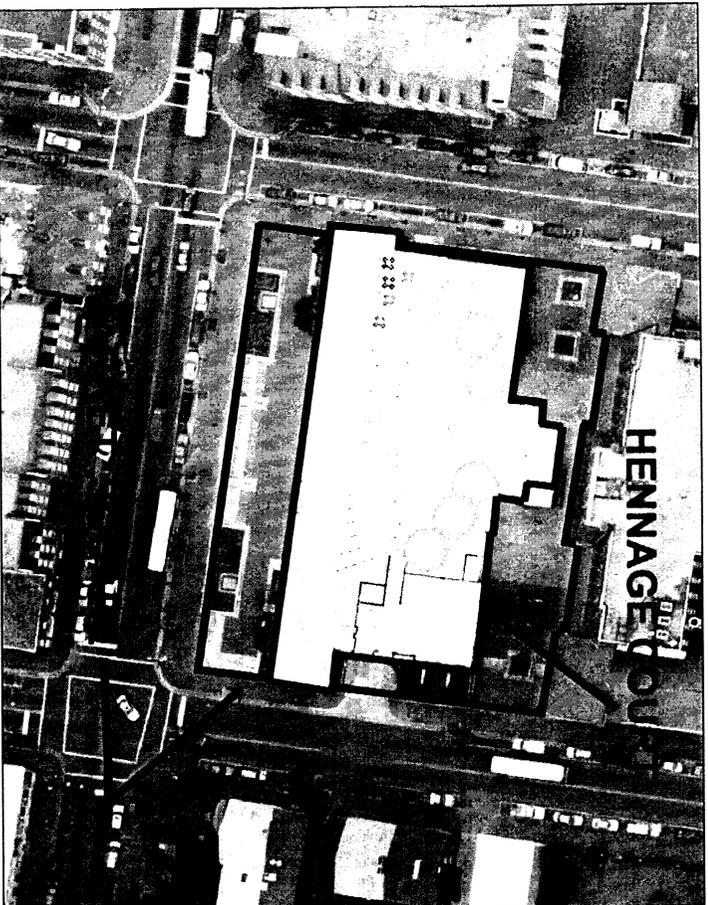
Proposal



Proposed site plan

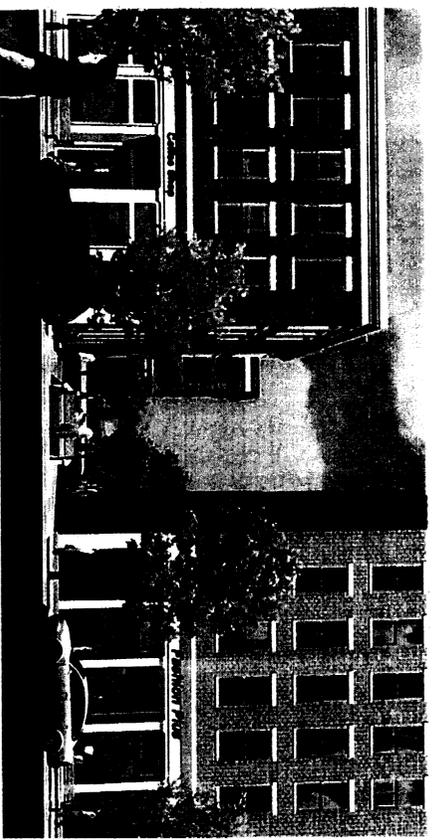
- 16,000 sq. ft. retail on Henry Street
- 168 condominium units
- 2.33 FAR
- Height: 40' - 62.5'
- Underground parking for retail, residents and visitors

Open Space



Hennage Courtyard vs. Market Square

- 43% size of Market Square
- 30% open space at ground level
- 42.8% total open space

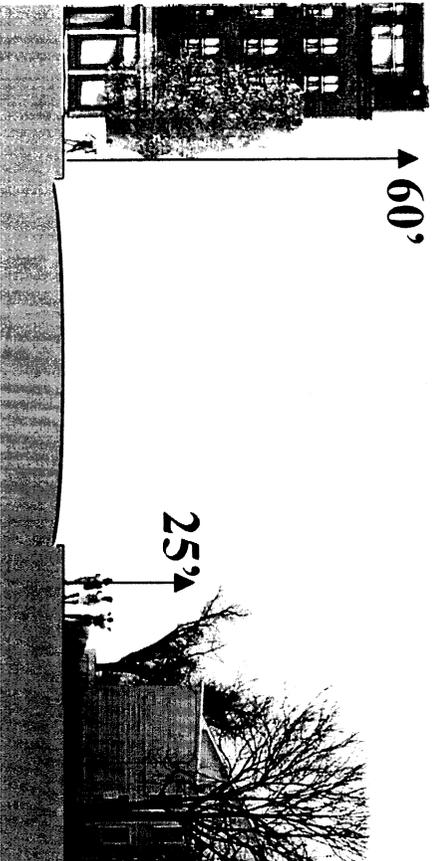


Building break on North Henry St.

Issues: SUP - Restaurants

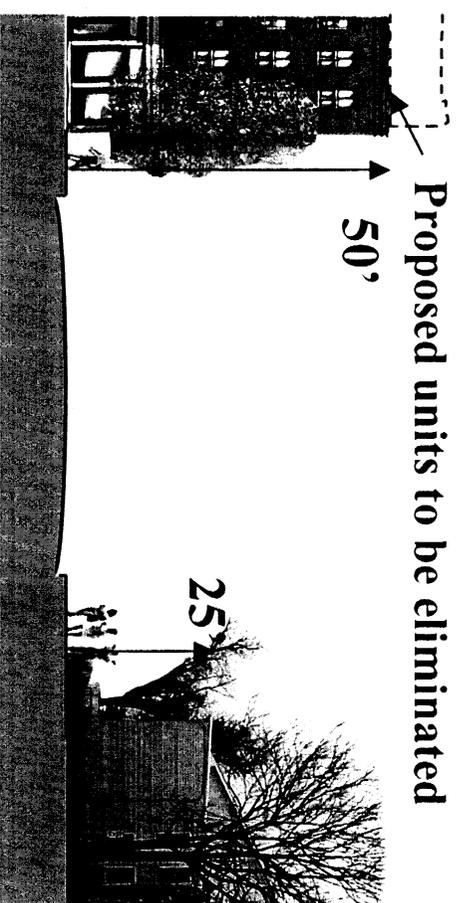
- No special use permits required for restaurants related to project
 - No public awareness or debate of issue
- Restaurant approval outside of long range planning initiatives
 - Braddock Road Metro Area study

Issues: Building Mass and Height



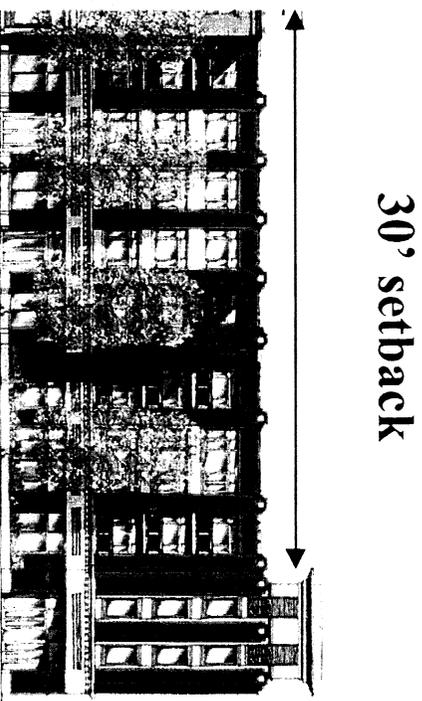
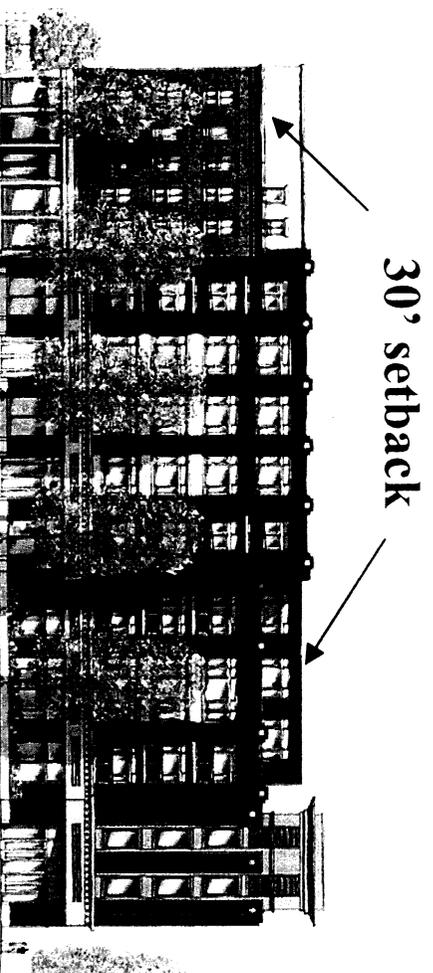
Applicant Proposal

North Henry Street



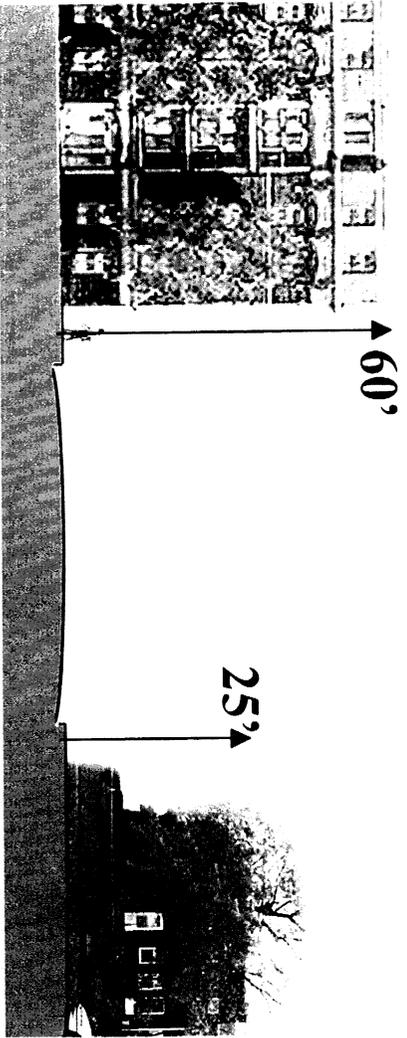
Staff Proposal

Issues: Building Mass and Height



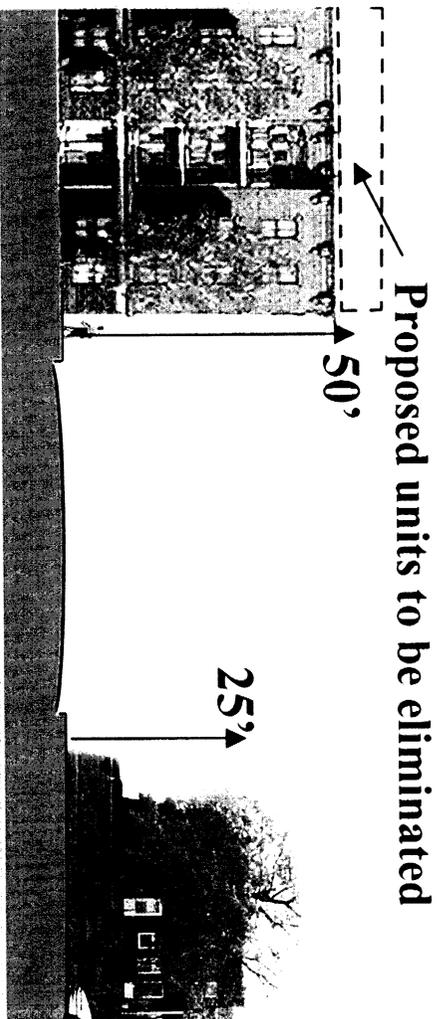
N. Henry St. without recommended changes ***N. Henry St. with staff recommended changes***

Issues: Building Mass and Height



Applicant Proposal

Fayette Street



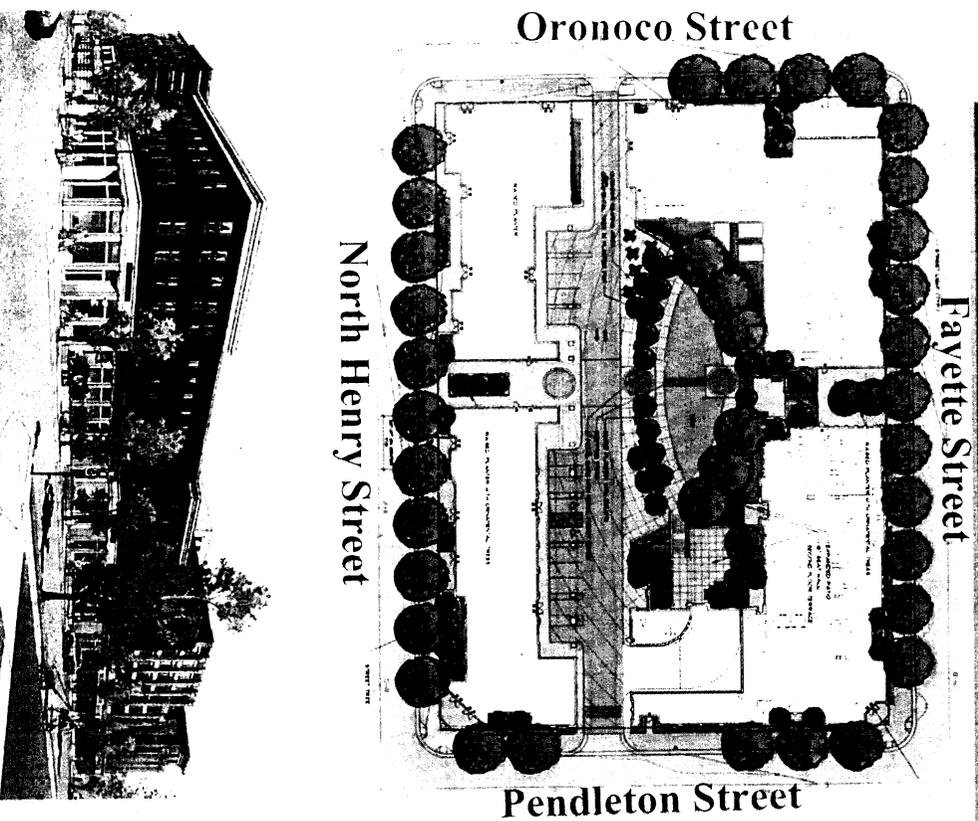
Staff Proposal

Planning Commission Recommendation

Recommended approval with staff conditions:

- One additional affordable unit (total of five)
- Architectural improvements recommended by staff
- Clarified traffic calming bulb outs on Henry and Princess Streets
- Must comply with future Braddock Road Metro Area Study

Conclusion



- Mixed use with underground parking
- Retail use on Henry Street
- New Residential in close proximity to Metro
- Ground level open space – accessible to public
- Height/mass compatibility with neighborhood
- On-site Affordable Housing

Docket Item 16
November 13, 2004

Mayor Euille, Vice Mayor Pepper and Members of Council:

I am Katy Cannady. I live at 20 East Oak Street. I attended the last meeting of the Planning Commission which approved the proposal for the Hennage Printing site. I didn't speak on that item. I did observe the public hearing discussion.

No one who spoke on any aspect of the Hennage application made any mention of allowing a restaurant use without a separate special use permit for that restaurant. That is because no one knew that a restaurant without a separate SUP was a possibility on this site. Earlier I attended a meeting in the Inner City neighborhood where Bud Hart made a presentation on the plans for retail uses and condos on the site. He mentioned that a possible retail use might be a coffee shop. Nothing was said about this particular coffee shop being treated differently than all other coffee shops approved under the current zoning laws in this city.

The idea of not requiring an SUP for any restaurant that might be part of this development was broached by a member of the commission only after the public hearing had been closed. I do not believe there was any debate or discussion of this aspect of the SUP even among the commissioners. It was quite late. The item I had come to speak on, the Mirant plant text amendment, didn't come up until around 1 a.m.

Whatever the pros and cons of this change to our zoning restrictions, it is a major change in policy and it should never have been approved without soliciting public comment first. We ought to have a hard and fast rule that once the planning and zoning staff has published their report on a particular project, the project must remain as described in that report. Any possible alternatives should be included in the report.

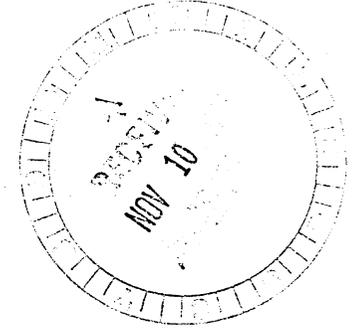
On the actual merits, I believe not requiring a special use permit for a restaurant would be bad policy. I think Alexandrians as a demographic group eat out a lot and being near good restaurants is generally considered a plus for any neighborhood. But what if a restaurant pops up on a block where no one expected one. And then problems related to the restaurant occur: parking problems, noise problems, or trash problems. I think you could be looking at a case of low level neighborhood conflict.

It is the sort of unfriendly, high tension situation that we usually avoid in Alexandria. When a new restaurant first gets a special use permit hearing and the near neighbors come forward and point out whatever problems might occur in that area, those problems are likely to get worked out. The restaurant operator also has a heads up as to what neighborhood concerns are.

It is a much better system and works best for all parties in the long run. Thank you.

CORINNE J. DIXON
1009 Woods Place
Alexandria Virginia 22302

November 9, 2004



The Honorable William D. Euille, Mayor
City of Alexandria, Virginia

Dear Mayor Euille:

I am very concerned about the redevelopment of the Hennage property as the proposal now stands. My property is located on the SE corner of North Henry and Pendleton Streets, directly in front of the project.

My father purchased this property in 1917 according to the city records. I was born and reared there along with my brother and sister. As a young child, I can remember no paved sidewalks or streets. There was a feed mill where the Hennage property is now located. There also was a railroad train with boxcars that serviced the feed mill and ran up and down Henry Street.

In the early twenties and thirties, we had a family wholesale bakery business. My father would deliver baked goods to local grocery stores, first with a horse and buggy.

There was a large display of the bakery and the horse and buggy in the Vola Lawson Lobby at City Hall, and also at the Black Heritage Museum on Alfred Street. My family worked hard on this property all through the great depression in the thirties.

I oppose this project in its current state for the following reasons:

1. First, it is too massive, and directly in front of my house, it's higher than the rest of the block. I understand the Staff of the Planning Commission recommended the massive size of the project be reduced, particularly in front of my house, and their recommendation was denied.
2. Second, Henry Street is US 1 going South, and on week days between the hours of 4 and 7:00pm, the traffic is bumper to bumper. It's very difficult to cross the street at that time of the day. Even though there is a traffic light at the corner, sometimes the intersection is blocked. At the present time, we can only park on one side of the street. I wonder when this project is completed if there will be parking on the street at all.
3. The third reason is the sunlight. I am aware a study was prepared which showed the sunlight would not affect my building, but I don't understand why a

Honorable William D. Euille, Mayor
City of Alexandria, Virginia
Page 2

two story building in front of a six or seven story building, would not be affected by the sunlight.

For the record, I do not object to the redevelopment of this property. I would just like to see something that is not so overwhelming.

Finally, the city places much emphasis on the historic preservation of our neighborhoods. There is no reason such emphasis should not continue with the Hennage project. Please consider this as a request that the Hennage project be reduced to a more manageable size.

Sincerely,

A handwritten signature in cursive script that reads "Corinne J. Dixon". The signature is written in dark ink and is positioned to the right of the word "Sincerely,".

Corinne J. Dixon

16
11-13-04



<landiscf@comcast.net>

11/11/2004 09:02 AM

Please respond to
<landiscf@comcast.net>

<alexvamayor@aol.com>, <delpepper@aol.com>, <council@joycewoodson.net>, <councilmangaines@aol.com>, <rob@krupicka.com>, <macdonaldcouncil@msn.com>

cc

bcc

City of Alexandria Website Contact Us - EMail for Mayor, Vice-Mayor and Council Members (alexvamayor@aol.com, delpepper@aol.com, council@joycewoodson.net, councilmangaines@aol.com, rob@krupicka.com, macdonaldcouncil@msn.com, paulcsmedberg@aol.com, rose.boyd@ci.alexandria.va.us, jackie.henderson@ci.alexandria.va.us, tom.raycroft@ci.alexandria.va.us)

City of Alexandria Website Contact Us - EMail for Mayor, Vice-Mayor and Council Members (alexvamayor@aol.com, delpepper@aol.com, council@joycewoodson.net, councilmangaines@aol.com, rob@krupicka.com, macdonaldcouncil@msn.com, paulcsmedberg@aol.com, rose.boyd@ci.alexandria.va.us, jackie.henderson@ci.alexandria.va.us, tom.raycroft@ci.alexandria.va.us)

Time: [Thu Nov 11, 2004 09:02:04] IP Address: [68.83.217.6]

Response requested:

First Name: Charlotte
Last Name: Landis
Street Address: 433 N Patrick St
City: Alexandria
State: VA
Zip: 22314
Phone: 703-549-2950
Email Address: landiscf@comcast.net

Comments: Item # 16 on the Docket for Saturday 13,2004 has been approved by Planning Commission 6-1. I am surprised and upset that the design provided by City Staff which allowed for greater variation at the roof top level was not accepted. Given our neighborhood of 1-3 story dwellings with great variation, I feel that 5-6 stories on the proposed Monarch building should, at the very minimum, have the variation that Staff has suggested. The incorporation of wording that will bind the future Condominium Association to maintain green space will provide the City of Alexandria one more

way to comply with the Clean Air Act.

Statement of Michael E. Hobbs
on behalf of the
Old Town Civic Association
City Council
November 13, 2004

Development Special Use Permit #2003-0019
The Monarch (Hennage Property)

Thank you, Mayor Euille, Vice Mayor Pepper, and Members of Council. I am Michael Hobbs, speaking on behalf of the Old Town Civic Association.

As with the Prescott/Hopkins project that you considered recently, this project is not within the formal boundaries of the Old and Historic District. But here, as there, we have a keen interest in the project, because of its proximity to us, its implications for future development in the surrounding neighborhoods, and particularly because of its potential impact on Parker-Gray, our sister historic district which is in its own right a treasured part of our city's heritage.

We supported this project, and the staff recommendations regarding conditions, before the Planning Commission, because we believe it represents a substantial improvement both over the structure first proposed, and over the existing Hennage plant and electric substation: ugly relics whose demise will be unlamented.

We do have two concerns about the project as it comes to you today, however, for which we urge your consideration.

First, we are concerned about the mass of the building as it faces the Parker-Gray district to the south and east, across N. Henry and Oronoco Streets. This is still, essentially, a city-block-long five-story building (albeit with a break in the middle) facing the very modest two-story houses across Henry Street and beyond. In the case of the Prescott project nearby, you conditioned approval on reductions in the scale on Henry Street to better relate to the context of the adjacent historic neighborhoods. We urge that you do so here as well, as the staff recommended before the Planning Commission.

We are concerned, secondly, about the implications for the City's review of restaurant applications if this application were approved as it is now before you. By their very nature, restaurants are the kind of commercial establishment most likely to have significant impacts on nearby residential neighborhoods, because of their hours of operation, the intensity of their use, and the attendant demand for on-street parking. We believe the City's existing policy of considering each restaurant application on its merits through the SUP process is wise and sound.

The staff recommended before the Planning Commission—in this case as in every other comparable case that we know of—the condition that “restaurants shall require approval of a special use permit.” But we understand that the Planning Commission's recommendation may have incorporated by reference a request by the applicant for what amounts to an open-ended advance approval of a restaurant in this building even before the identity of the restaurant and the

specifics of the proposed operation are known. We believe that would be a bad precedent and quite inappropriate, and we urge that you revert to the condition as it was stated in the staff report before the Planning Commission.

Thank you for your consideration.

16
11-13-04



<stewart@smartergrowth.net>

11/12/2004 05:38 PM

Please respond to
<stewart@smartergrowth.net>

To <alexvamayor@aol.com>, <delpepper@aol.com>, <council@joycewoodson.net>, <councilmangaines@aol.com>, <rob@krupicka.com>, <macdonaldcouncil@msn.com>,

cc

bcc

Subject City of Alexandria Website Contact Us - EMail for Mayor, Vice-Mayor and Council Members (alexvamayor@aol.com, delpepper@aol.com, council@joycewoodson.net, councilmangaines@aol.com, rob@krupicka.com, macdonaldcouncil@msn.com, paulcsmedberg@aol.com, rose.boyd@ci.alexandria.va.us, jackie.henderson@ci.alexandria.va.us, tom.raycroft@ci.alexandria.va.us)

City of Alexandria Website Contact Us - EMail for Mayor, Vice-Mayor and Council Members (alexvamayor@aol.com, delpepper@aol.com, council@joycewoodson.net, councilmangaines@aol.com, rob@krupicka.com, macdonaldcouncil@msn.com, paulcsmedberg@aol.com, rose.boyd@ci.alexandria.va.us, jackie.henderson@ci.alexandria.va.us, tom.raycroft@ci.alexandria.va.us)

Time: [Fri Nov 12, 2004 17:38:20] IP Address: [151.200.180.208]

Response requested:

First Name: Stewart
Last Name: Schwartz
Street Address: 1415 Oronoco Street
City: Alexandria
State: VA
Zip: 22314
Phone: 703-683-5704
Email Address: stewart@smartergrowth.net
Coalition for Smarter Growth
4000 Albemarle Street NW
Suite 310
Washington, DC 20016

November 12, 2004

Mayor William Eulle
City of Alexandria
301 King Street

Alexandria, VA 22314

Dear Mayor Euille and members of the Council:

On behalf of the Coalition for Smarter Growth, I am writing to endorse the proposed project for redevelopment of the Hennage Creative Printers property. Abandoned and underutilized for many years, this is a well-designed mixed use project which will contribute to the revitalization of the Route 1 corridor and Braddock Road neighborhood, which also happens to be my home. Specific features worthy of note include,

- Comments:**
- The mix of ground level retail/commercial and residential space, which will bring pedestrian traffic increasing eyes on the street and a sense of community;
 - The inclusion of the inner-courtyard that has street level access from the pedestrian thoroughways and allows public access;
 - The reduction and shielding of the existing power sub-station, currently an eyesore in the community;
 - The diversification of the building facades and heights.

We encourage the provision of affordable housing within the development and transportation demand management measures to ensure maximum transit usage and minimal auto ownership and use. We have some concern about the proposed architectural style of the condo building at the corner of N. Fayette and Pendleton Street. Too much like some of the less successful buildings in the City from the 1980's, we recommend redesign of this portion.

We applaud the developers and the City's efforts to bring quality mixed-use and walkable infill development to the City of Alexandria. This development will be an asset to the Braddock Road neighborhood. We look forward to the opportunity to provide input on similar projects and to working with the City on the Braddock Road Small Area Plan. If you have any questions or would like any additional information, do not hesitate to contact me. I can be reached by phone at 202.244.4408.

Sincerely,

Stewart Schwartz
Executive Director

SPEAKER'S FORM

DOCKET ITEM NO. 16

**PLEASE COMPLETE THIS FORM AND GIVE IT TO THE CITY CLERK
BEFORE YOU SPEAK ON A DOCKET ITEM.**

PLEASE ANNOUNCE THE INFORMATION SPECIFIED BELOW PRIOR TO SPEAKING.

1. NAME: Harry P. Hart
2. ADDRESS: 307 N. Washington St.
TELEPHONE NO. 708-325-5757 E-MAIL ADDRESS: hph@vopizon.net
3. WHOM DO YOU REPRESENT, IF OTHER THAN YOURSELF? The Applicant
4. WHAT IS YOUR POSITION ON THE ITEM?
FOR: AGAINST: _____ OTHER: _____
5. NATURE OF YOUR INTEREST IN ITEM (PROPERTY OWNER, ATTORNEY, LOBBYIST, CIVIC INTEREST, ETC.):
Attorney
6. ARE YOU RECEIVING COMPENSATION FOR THIS APPEARANCE BEFORE COUNCIL?
YES NO _____

This form shall be kept as a part of the permanent record in those instances where financial interest or compensation is indicated by the speaker.

A maximum of three minutes will be allowed for your presentation. If you have a prepared statement, please leave a copy with the Clerk.

Additional time not to exceed 15 minutes may be obtained with the consent of the majority of the council present; provided notice requesting additional time with reasons stated is filed with the City Clerk in writing before 5:00 p.m. of the day preceding the meeting.

The public normally may speak on docket items only at public hearing meetings, and not at regular legislative meetings. Public hearing meetings are usually held on the Saturday following the second Tuesday in each month; regular legislative meetings on the second and fourth Tuesdays in each month. The rule with respect to when a person may speak to a docket item at a legislative meeting can be waived by a majority vote of council members present but such a waiver is not normal practice. When a speaker is recognized, the rules of procedures for speakers at public hearing meetings shall apply. If an item is docketed for public hearing at a regular legislative meeting, the public may speak to that item, and the rules of procedures for speakers at public hearing meetings shall apply.

In addition, the public may speak on matters which are not on the docket during the Public Discussion Period at public hearing meetings. The Mayor may grant permission to a person, who is unable to participate in public discussion at a public hearing meeting for medical, religious, family emergency or other similarly substantial reasons, to speak at a regular legislative meeting. When such permission is granted, the rules of procedures for public discussion at public hearing meetings shall apply.

Guidelines for the Public Discussion Period

- (a) All speaker request forms for the public discussion period must be submitted by the time the item is called by the City Clerk.
- (b) No speaker will be allowed more than three minutes.
- (c) If more speakers are signed up than would be allotted for in 30 minutes, the Mayor will organize speaker requests by subject or position, and allocate appropriate times, trying to ensure that speakers on unrelated subjects will also be allowed to speak during the 30 minute public discussion period.
- (d) If speakers seeking to address council on the same subject cannot agree on a particular order or method that they would like the speakers to be called on, the speakers shall be called in the chronological order of their request forms' submission.
- (e) Any speakers not called during the public discussion period will have the option to speak at the conclusion of the meeting, after all docketed items have been heard.

#14

Third AMENDED
APPLICATION for
DEVELOPMENT SPECIAL USE PERMIT with SITE PLAN
DSUP # 2003-0019

PROJECT NAME: The Monarch

PROPERTY LOCATION: Block Bounded by N. Henry, Oronoco, Pendleton and N. Fayette Streets

TAX MAP REFERENCE: 64.01-03-01, -02, -04, -06, -07, -09 ZONE: CRMU-H

APPLICANT Name: Diamond Alexandria LLC

Address: 3919 Old Lee Hy #89a, Fairfax Va 22030

PROPERTY OWNER Name Diamond Alexandria LLC

Address: 3919 Old Lee Hy #89a, Fairfax Va 22030

SUMMARY OF PROPOSAL: Request to redevelop the Hennage Creative Printer Block with a
15,000 square foot retail center with 168 condominiums above, while reducing and screening
the power substation.

MODIFICATIONS REQUESTED: [REDACTED] reduction in the minimum
zone transition from the adjacent residential zone and reduction in the minimum 75' vision
clearance.

SUP's REQUESTED: FAR of 2.33.

THE UNDERSIGNED hereby applies for Development Site Plan, with Special Use Permit, approval in accordance with the provisions of Title 7, Chapter 5 of the Code of the City of Alexandria, Virginia.

THE UNDERSIGNED, having obtained permission from the property owner, hereby grants permission to the City of Alexandria to post placard notices on the property for which this application is requested, pursuant to Article XI, Section 11-301(B) of the 1992 Zoning Ordinance of the City of Alexandria, Virginia.

THE UNDERSIGNED also attests that all of the information herein provided and specifically including all surveys, drawings, etc., required of the applicant are true, correct and accurate to the best of their knowledge and belief.

Harry P. Hart
Print Name of Applicant or Agent

[Signature]
Signature

HART, CALLEY, GIBBS & KARP, P.C.
Mailing Address

(703) 836-5757
Telephone Number

307 N. Washington St., Alex. VA 22314
Mailing Address

10-7-04
Date

DO NOT WRITE BELOW THIS LINE - OFFICE USE ONLY

Application Received: _____ Received Plans for Completeness: _____
Fee Paid & Date: \$ _____ Received Plans for Preliminary: _____
Legal Advertisement: _____ Property Placard: _____

ACTION - PLANNING COMMISSION: 11/04/2004 RECOMMEND APPROVAL 6-1

ACTION - CITY COUNCIL: 11/13/04 CC approved Planning Commission recommendation 7-0
(see attached)

5060

and 3:00 p.m. EST, on school days. *In general, the applicant shall comply with applicable provisions of the Noise Control Ordinance, including Section 11-5-4 and Section 11-5-5 (related to noise levels for motorized construction devices);*" (4) modify Condition #90 to read as follows: "Prior to issuance of a demolition permit for existing structures on the site, the applicant shall submit an asbestos removal and fugitive dust control plan to the Code Enforcement Division;" (5) modify Condition #85(a) to read as follows: "Construction trucks removing debris shall arrive and depart from the site via North Royal Street unless directed otherwise by the Director of T&ES;" (6) include in condition #22 a noise suppression plan to be determined by the Director of Transportation and Environmental Services; and (7) encourage the applicant to work with Habitat for Humanity Restore program for reuse of leftover building materials.
Council Action: _____

16. DEVELOPMENT SPECIAL USE PERMIT #2003-0019
1180 & 1100 PENDLETON STREET, 1121 & 1101 ORONOCO STREET AND
511 N. FAYETTE STREET.
THE MONARCH (HENNAGE PROPERTY)
Public Hearing and Consideration of a request for a development special use permit, with site plan and modifications to construct a residential building with ground floor retail; zoned CRMU-H/Commercial Residential Mixed Use High.
Applicant: Diamond Alexandria, LLC by Harry P. Hart, attorney

PLANNING COMMISSION ACTION: RECOMMEND APPROVAL 6-1

City Council held the public hearing and approved the recommendation of the Planning Commission with the following modifications: (1) the addition of condition #101 stating, "the applicant shall relocate 2-3 units that are recommended to be eliminated to minimize the perceived mass and scale on North Henry Street to the satisfaction of the Director of Planning and Zoning;" (2) to include language in condition #16(2) concerning additional bike racks stating, " the applicant shall reserve additional spaces for expansion to the satisfaction to the Director of Transportation and Environmental Services;" (3) include language in condition #85 that gives staff the option of allowing the developer to make a monetary contribution of \$1 million to the Housing Trust Fund in lieu of the 5 affordable units; (4) remand the restaurant special use permit back to the Planning Commission for public hearing and public comment; (5) modify the language in condition 17(j) to state, "A public art feature that is an appropriate scale for the space shall be provided and permanently mounted within the public pocket park, located in consultation with the Director of Planning and Zoning and the Arts Commission;" (6) include language in condition #40 stating, "the applicant shall develop a plan of communication with the community in consultation with the Departments of Planning and Zoning and Transportation and Environmental Services;" and (7) encourage the applicant to work with Habitat for Humanity Restore program for reuse of leftover building materials.
Council Action: _____

17. TEXT AMENDMENT #2004-0008
AMENDMENT TO NONCOMPLYING USE REGULATION APPLICABLE TO
COAL FIRED POWER PLANTS
Public Hearing and Consideration of a request for a text amendment to the Zoning Ordinance to revoke the noncomplying use status of coal fired power plants, which was granted in 1992, and to categorize such plants as nonconforming uses, subject to abatement. Applicant: City of Alexandria, City Attorney's Office.

PLANNING COMMISSION ACTION: RECOMMEND APPROVAL 7-0

City Council held the public hearing and approved the Planning Commission recommendation.
Council Action: _____

18. VACATION #2004-0001
700 SOUTH LEE STREET