

*City of Alexandria, Virginia*

MEMORANDUM

DATE: SEPTEMBER 21, 2004

TO: THE HONORABLE MAYOR AND MEMBERS OF CITY COUNCIL

THROUGH: PHILIP SUNDERLAND, CITY MANAGER *PS*

FROM: EILEEN P. FOGARTY, DIRECTOR  
PLANNING AND ZONING *Eileen Fogarty*

SUBJECT: DSUP #2004-0001 – THE PRESCOTT

The Planning Commission recommended approval of the proposed development special use permit to construct 64 condominium units (4 on-site affordable units) at their September 9, 2004 hearing, contingent on the reduction in the building mass at the corner of Henry and Fayette Street and on Henry Street and the staff recommendations. The Commission also added conditions to address concerns raised by the community such as traffic calming and construction traffic.

At that meeting, ten citizens testified, with four citizens speaking in favor of the project and six citizens speaking against the project. Five of the speakers represented organizations such as the Old Town Civic Association, which recommended approval contingent on the reduction in the mass of the proposed buildings. Inner City Civic Association and Upper King Street Neighborhood Association also recommended approval of the proposal.

The concerns that were expressed by the citizens centered around impacts on traffic, parking, massing of the building and open space. A representative of the American Legion Post 129 and Antioch Church and several adjoining residents raised concerns regarding parking, traffic, access and proposed mass of the building. The primary concern regarding traffic was the vehicle and construction traffic impacts on Fayette Street. Some residents also expressed concern that the access was located on Fayette Street. To address the concern regarding vehicle and construction traffic on Fayette Street, the Commission added a condition that required \$10,000 for traffic calming on Fayette Street and a condition to control construction traffic impacts on the adjoining residential neighborhoods and Fayette Street. The concern regarding parking related to overflow parking into the adjoining residential neighborhoods. Based upon the parking analysis and the condition that precludes residents from obtaining residential parking permits, the Commission found that the parking concerns could be adequately addressed.

To address concerns regarding building massing raised by staff and the community, the Commission recommended conditions that require the fourth floor at the corner of Cameron and Henry Streets to be designed so that it is not visible from the adjoining sidewalks, with the possible exception of a corner element. The Commission also included a condition requiring that the northern portion of the building along Henry Street be reduced to provide a transition to the adjoining buildings and be more compatible with the scale of the existing buildings on Henry Street.

The Commission recommended approval of the proposal and the on-site affordable units contingent upon the reduction in mass of the building, added conditions to address the concerns of the community and the staff recommendations. The Planning Commission voted 6 to 1 to recommend approval of the development.

THE PRESCOTT  
(HOPKINS WAREHOUSE)



DEVELOPMENT SPECIAL USE PERMIT  
#2004-0001

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DEPARTMENT OF PLANNING AND ZONING  
PLANNING COMMISSION  
SEPTEMBER 9, 2004

Docket Item #13  
DEVELOPMENT SPECIAL USE PERMIT #2004-0001  
THE PRESCOTT (Hopkins Warehouse)

Planning Commission Meeting  
September 9, 2004

**ISSUE:** Consideration of a request for a development special use permit, with site plan and modifications, for construction of a residential condominium building with underground parking.

**APPLICANT:** Cameron Street L.L.C., c/o Carr Homes, Inc. by  
J. Howard Middleton, Jr. Attorney

**LOCATION:** 1115 Cameron Street and 200 North Henry Street

**ZONE:** CD/Commercial Downtown

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**PLANNING COMMISSION ACTION, SEPTEMBER 9, 2004:** On a motion by Mr. Dunn, seconded by Mr. Komoroske, the Planning Commission voted to recommend approval of the request, subject to compliance with all applicable codes, ordinances and staff recommendations, with amendments to Conditions 1a, 2b, 2p, 2q, 3, 6h, 47, 53, 54, and 75, and the addition of three conditions. The motion carried on a vote of 6 to 1, with Mr. Leibach voting against.

**Reason:** The Planning Commission agreed with the staff analysis, finding that the conditions recommended by staff and amended by the Commission, address the concerns of mass, scale and traffic raised by the community. The Planning Commission added three conditions to address traffic calming on Fayette Street, construction traffic and disclosure statements to future residents.

**Speakers**

Howard Middleton, attorney, representing the applicant.

Guy Boston, representing the American Legion Post, spoke against the project, expressing concerns regarding construction impacts and traffic.

Reverend Johnny Alexander, representing the Antioch Church at 1120 Queen Street, spoke against the project, expressing concerns regarding traffic and parking.

Wilson Thompson spoke against the project, expressing concerns about the proposed access on Fayette Street.

Michael Hobbs, representing the Old Town Civic Association, spoke in favor of the project, subject to staff's recommendations to minimize the visual impacts of the building massing.

Ellen Pickering, 103 Roberts Lane, spoke against the project, expressing concerns about the density and mass of the project and stating that the rooftop terrace should not count toward open space and that open space on the site should be visible to the public.

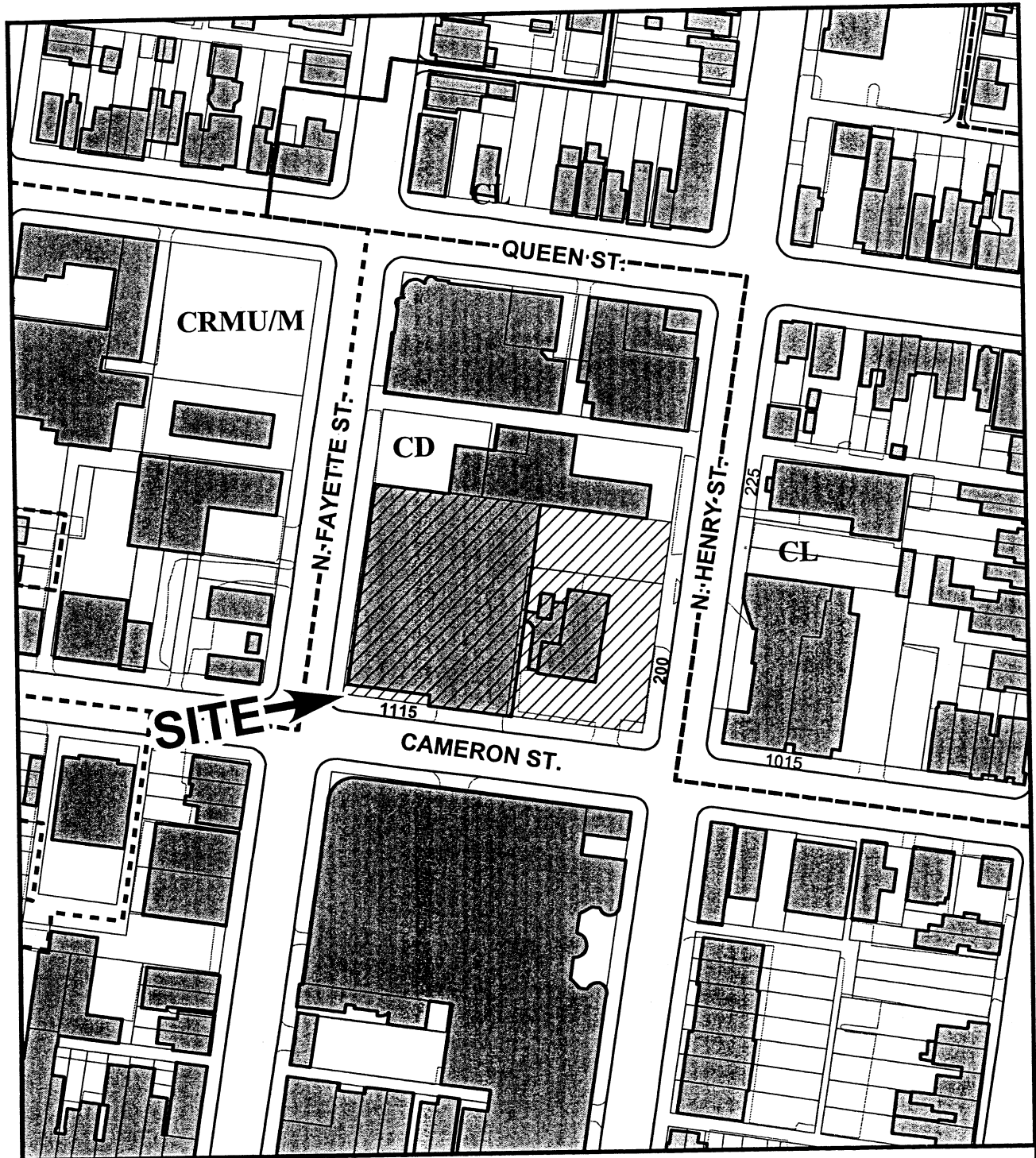
Amy Harris-White, representing the Inner City Civic Association, stated that the Association voted to support the project and is very happy with the design.

Trey Hanburg, representing the Upper King Street Neighborhood Association, stated that the Association supports the proposal and finds the open space that is provided to be suitable.

Debra Sabourin, 1212 Princess Street, spoke in favor of the project, subject to staff's recommendations, but stated that the driveway should be located on Cameron, that if the driveway is on Fayette then traffic calming measures should be taken, and that existing truck restrictions should be applied to construction vehicles.

Sarah Becker, 1200 Princess Street, spoke in opposition to the project, expressing concerns regarding precedent, the Fayette Street entrance, the project's density, construction hauling routes, and security issues.

Dan Koslov, 1219 Queen Street, spoke in opposition to the project, expressing concerns about density, building massing, traffic, parking, safety, and the changes to the neighborhood that would result from the development.



**DSUP #2004-0001**

**09/09/04**



## I. EXECUTIVE SUMMARY

### A. Overview:

The applicant is requesting approval of a development special use permit to permit an increase in density from 35 d.u./acre to 54.45 d.u./acre, as well as an additional 20% increase in density and floor area ratio to provide on-site affordable units. The proposed three- to four-story building will be located on the southern portion of a block surrounded by the streets of Cameron, Henry and Fayette. The proposal consists of 64 condominiums and one level of below-grade parking accessed from Fayette Street. The site currently contains two buildings – a former one-story furniture warehouse (Hopkins Warehouse) and a one-story service station.

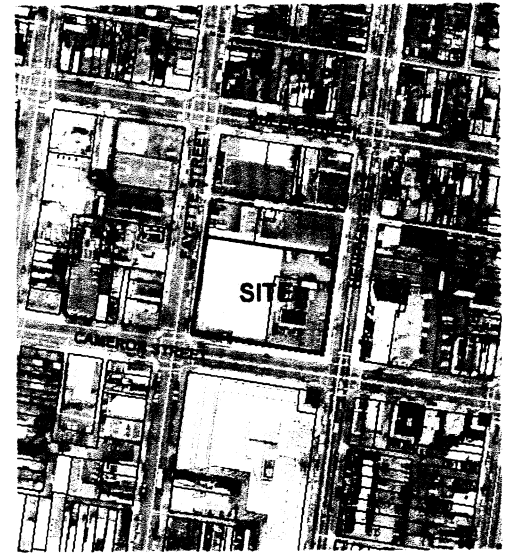


Figure 1 - Site Aerial

The site is located within the Parker Gray Historic District. On June 23, 2004, the Parker Gray Board approved the demolition of the existing warehouse and service station and the conceptual plans for the development, subject to the condition that the apartment segment at the corner of Cameron and Henry Streets be revised to appear as a three-story building, and that the mass of the northern building on Henry Street be reduced.

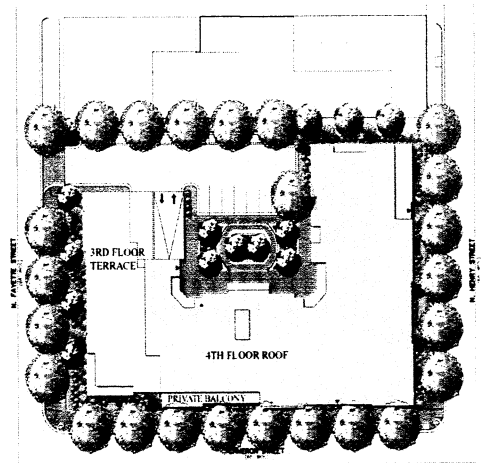


Figure 2 - Site Plan

The site is in a transitional location between the typically lower-scale vernacular townhouses within Parker Gray to the north and west, King Street to the south, and the Old and Historic District to the east. Therefore, it is critical to ensure that the mass and scale of the proposal appear as a natural extension of the fabric of these three neighborhoods. The redevelopment of this site presents numerous challenges and opportunities that include:

#### *Challenges*

- Providing a mass and scale that are compatible with the adjoining Parker Gray District, Old and Historic District, and Henry Street.
- Ensuring that the building is well designed as one of the first significant redevelopment projects within Parker Gray.
- Maintaining a level of open space and “openness” for the development.
- Providing sufficient parking.
- Minimizing traffic impacts.

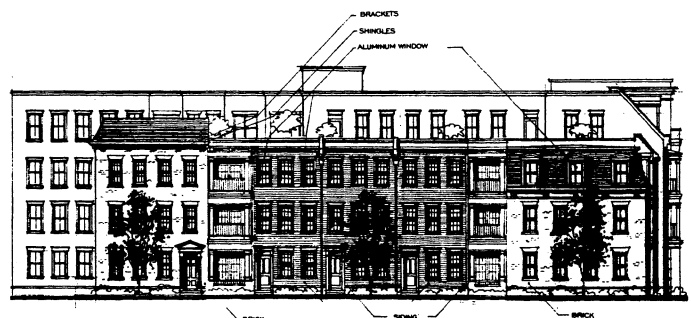


Figure 3 - Fayette Street Elevation

*Opportunities*

- Redevelopment of a vacant site that is considered a redevelopment site within the Braddock Road Metro Station Small Area Plan.
- Provision of on-site affordable housing.
- Provision of pedestrian and streetscape improvements.

While staff supports the redevelopment of the site and the proposed affordable housing, the additional density proposed to provide the affordable housing results in a mass and scale that are inconsistent with the scale of existing buildings on Henry Street and the neighborhood. Staff recommends approval of the proposal with significant changes to the mass, building design, open space and parking as discussed in detail below. These recommendations of approval are consistent with and reflect the findings of the Parker Gray Board, and are necessary to ensure that the development is compatible with the scale of the neighborhood, is consistent with the historic building pattern in the area, and provides an appropriate amount and quality of open space.

**B. Issues with the Current Proposal:**

***Building Design/Mass and Scale***

One of the characteristics that adds to the variety, richness and texture of the District is the variation of building styles and massing that occurs within each block. This is due to the fact that buildings within each block have been constructed over time, resulting in each block containing a collection of buildings, architectural styles and roof forms that are character-defining elements of Parker Gray and Old Town. Staff has encouraged the applicant to design the building to appear to be a collection of buildings that have been constructed over time, similar to the existing character of the district. The applicant has worked with staff to address this design issue.

The remaining area of concern for staff, as well as the Parker Gray Board, is the scale of the building at the intersection of Henry Street and Cameron Street and the northern portion of the building on Henry Street. Therefore, staff recommends that a portion of the building be set back at the corner of Cameron and Henry Street so that the fourth level will not be visible, as depicted in Figure 5, and that the northern portion of the facade on Henry Street be revised as depicted in Figure 16. These proposed changes will likely result in the elimination of some of the units or a change in the number of one and two bedroom units. These revisions are necessary to ensure that the proposed building is compatible with the existing two- and three-story buildings on Henry Street. In addition to reducing the overall height, the proposed modifications provide additional variation in the rooflines and greater building articulation, both of which help to reduce the perceived mass of the building. With these changes, the overall massing will reflect the comments of the BAR and can be supported by staff.





**Figure 4 - Applicant Proposal - Cameron Street**



**Figure 5 - Staff and BAR Proposal - Cameron Street**

### ***Open Space and Streetscape***

The proposal provides 26% ground-level open space and 14% open space on the rooftop terrace, for a total of 40% open space. The ground-level open space consists of the front yards on Fayette and Henry Streets and an internal courtyard. Conditions have been included to:

- Require the ground-level open space to be enhanced through additional landscaping, irrigation and fencing;
- Require improvements to the central courtyard, such as a focal element, special paving surfaces and additional amenities.
- Improved design of the rooftop terrace;
- Improve and increase the amount of ground-level open space by reconfiguring some surface parking; and
- Providing a \$10,000 contribution for pedestrian, landscape and signage improvements to the nearby Hunter Miller Park.

### ***Visitor Parking***

Typically, the City requires that a minimum of 15% visitor parking be provided in conjunction with new residential developments, except in those locations where adequate on-street parking is available to meet visitor demand. The applicant is requesting approval to provide 7 of the 17 (15%) visitor parking spaces on the adjoining streets and to provide all of the parking spaces as “hybrid” (smaller than a standard space but larger than a compact space) parking spaces. A parking study submitted

by the applicant indicates that 50 to 85 (24% to 41%) of the on-street parking spaces within a block of the site are available during peak visiting times. Additionally, the proposed development eliminates three curb cuts, which will allow the creation of an additional five to seven on-street parking spaces directly adjacent to the site. Staff is recommending approval to provide a limited amount (7 spaces) of the visitor spaces on the adjoining street because of the availability of parking indicated by the parking study. In addition, staff is recommending that the residents not be eligible for residential parking permits to ensure that the residents park within the underground garage. Staff believes this is an acceptable balance between open space and parking needs.

### ***Affordable Housing***

The applicant's proposal includes a request for a 20% increase in the allowable FAR and density in exchange for providing affordable dwelling units on-site. The applicant is proposing a total of 64 dwelling units, 10 of which are the "bonus" units that are allowed because of the affordable housing contribution. The applicant proposes to provide four affordable dwelling units in exchange for the bonus units. The estimated value of this contribution is approximately \$451,800. Staff believes that this is an appropriate location for affordable housing, as it is adjacent to services and transportation. However, as discussed above, staff believes that the additional mass proposed by the applicant in exchange for affordable units needs to be revised to provide a development that is compatible with the scale and character of the neighborhood. Reducing the mass and scale could result in a loss of up to four units, which in turn could result in a loss of affordable units under the current affordable housing proposal.

### **C. Community:**

The applicant and staff conducted community meetings for this project with the Inner City Civic Association and adjoining residents and two hearings by the Parker Gray Board of Architectural review. The primary issues identified by the community include:

- Minimizing the traffic impact of construction on North Henry and Cameron Streets.
- Increasing variation in roof height.
- Reducing the mass and scale along Cameron and Henry Streets.
- Making Fayette Street pedestrian-friendly.
- Adding more ground-level open space, visible to the street.
- Setting an appropriate precedent for future redevelopment.

To address these concerns, staff has included conditions for construction phasing, reduction in the overall massing, streetscape improvements, additional open space and landscaping.

## II. BACKGROUND

### *Site Description*

The property is located at the southeastern boundary of the Parker Gray District. The remainder of the block is developed with one- and two-story buildings that consist of a mix of uses, including auto repair, a bookstore, hair salons, a restaurant, and a church.

The surrounding blocks to the east, north, and west are generally developed with two and three-story buildings. The block to the south, across Cameron Street, is developed with a seven-story, tiered office and retail building and a two-and-a-half story single-family house used as a commercial office.

The site consists of two lots and contains two vacant buildings – a former one-story furniture warehouse (Hopkins Warehouse) and a one-story service station (All Tune and Lube). The site is essentially flat and is almost entirely covered with impervious surfaces.

### *Proposal*

The applicant has worked with staff to address many of the concerns raised by staff and the community and has made considerable changes to plans. The applicant's initial proposal was for a four-story condominium building. The fourth story was set slightly back from the first three floors, but otherwise the proposal was designed as a single massive four-story building with only moderate building articulation. In subsequent iterations of the plan, the applicant reduced portions of the building to three stories, designed the project to have the appearance of multiple buildings of varying styles, increased the setbacks on Fayette Street, and provided greater building articulation.

The proposed building will be located along Cameron Street between North Henry Street and North Fayette Street, with a footprint of approximately 175 by 250 feet. The 64-unit residential condominium building is proposed to consist of five one-bedroom units (approximately 750 square feet each) and 59 two-bedroom units (1,050 to 2,000 square feet each). The building will be three to four stories in height. Vehicular access will be via Fayette Street, and parking will be provided in a 110-space underground parking garage and a surface parking area consisting of 10 spaces for visitors, for a



**Figure 6 - Historic District Boundaries**



**Figure 7 - Building to the East**



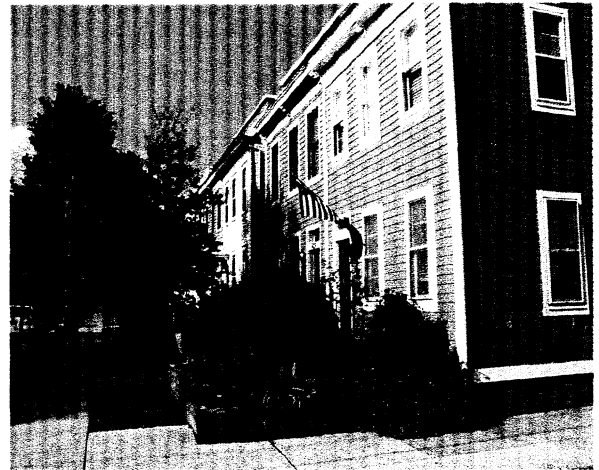
**Fig 8 - Buildings to the South**



**Figure 9 - Site**

total of 120 off-street parking spaces. Controlled access will be provided for the underground garage. The primary pedestrian and visitor access into the building will be from Cameron Street and from the parking area at the rear of the building, midway between North Henry and North Fayette Streets.

Along Henry Street, the building will be three to four stories, with a maximum height of 48 feet. The fourth level will be set back so that along parts of Cameron and Fayette Streets the building will appear to be three stories. Terraces will be located on the top of the three-story section. Along Fayette Street, which is closer to the center of the Parker Gray District, the articulation of the building are smaller townhouses with the small front yards adjacent to Fayette Street, similar to other townhouses within Parker Gray district, such as those pictured on West Street.



**Figure 10 - West Street Townhomes**

The proposed building is U-shaped with an interior courtyard. The building will be constructed nearly to the street line on Cameron Street, but set back approximately 8 feet from North Henry Street and approximately 15 feet from North Fayette Street.

***Parker Gray Board of Architectural Review:***

The subject property is located in the Parker Gray District, and on June 23, 2004, the Board approved the demolition of the existing warehouse and service station, with the condition that demolition does not take place until City Council approves the special use permit. At that same meeting, the Board approved the conceptual plans for the development, with the condition that the apartment segment at the corner of Cameron and Henry Streets be revised to appear as a three-story building. Additionally, the BAR directed the applicant to address mass and scale concerns raised by staff.

***Affordable Housing***

The development proposal includes a request for a 20% increase in the allowable FAR and density in exchange for providing on-site affordable housing, and in accordance with Section 7-700 of the Zoning Ordinance. The applicant proposes to provide one-third of the “bonus” dwelling units as affordable, and pay a cash contribution of \$1 per gross square foot of “non-bonus” dwelling units. While this would equate to three dwelling units (3.3 units, rounded down) and approximately \$62,650 in cash, the applicant is instead proposing to provide 4 affordable dwelling units as discussed in more detail below.

**III. ZONING:**

The applicant is requesting approval for a development special use permit for condominiums in the CD Commercial Downtown District.

<b>THE PRESCOTT</b>		
<b>Property Address:</b>	1115 Cameron Street and 200 North Henry Street	
<b>Total Site Area:</b>	43,585 square feet (1.00 acre)	
<b>Zone:</b>	CD Commercial Downtown	
<b>Current Use:</b>	Warehouse and Vacant Service Station	
<b>Proposed Use:</b>	Condominiums	
	<u>Permitted/Required</u>	<u>Proposed</u>
<b>FAR</b>	1.25	1.50*
<b>Density</b>	35 d.u./acre, by right 54.45 d.u./acre, w/ SUP	63.96 d.u./acre*
<b>Open Space</b>	40%, 17,434 s.f.	40.2%, 17,527 s.f.
<b>Height</b>	50 feet	48 feet
<b>Side Yard</b>	25'; 1:2	14.6'; 1:3.3**
<b>Parking, inc. visitor parking</b>	127	120
	* 20% increase for affordable housing requested.	
	**Modification requested	

#### **IV. STAFF ANALYSIS:**

The site is identified in the Braddock Road Metro Station Small Area Plan as a potential redevelopment site, and represents one of the largest redevelopment sites in the Parker Gray District. Both staff and the community support redevelopment of the site. The site is located on the visually prominent intersection of Henry and Cameron Streets. Situated near the edge of the Old and Historic Alexandria District, the site serves as an important transition property between the two historic districts.

The applicant is requesting a special use permit for a 20% increase in density to provide affordable units. There is a balance between approving additional density for much needed on-site affordable units and approving a mass and scale compatible with the context of this relatively modest scale neighborhood. In fact, one of the reasons for the creation of Parker Gray District in 1984 was to ensure that the opening of the Braddock Metro Station in 1982 would not result in the loss of the architectural character and scale of the neighborhood. There are also several adjoining sites and small infill sites on Henry Street that may redevelop in the future.

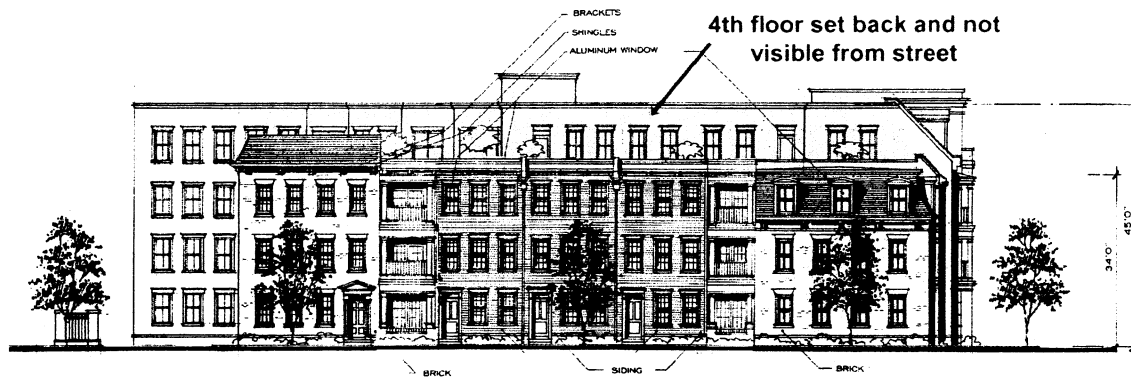
Staff strongly supports affordable units at this location; however, the increased density allowed with the provision of affordable units, will result in a mass and scale that are significantly greater than the adjoining buildings. The proposed four-story elements on Henry Street will overwhelm the adjoining two to three-story buildings. Therefore, staff recommends elimination of portions of the fourth level on Henry Street to provide a better transition to the height on Henry Street and revisions to the overall design. These recommendations are also consistent with the concerns expressed by the Parker Gray Board and members of the community. With these revisions the overall massing will be more compatible with Henry Street and the adjoining Parker Gray District, provide additional variation on Henry Street, and address concerns expressed by the community. Staff believes that with the recommended changes to the building design, massing, parking, and open space, the proposed development can be a positive addition to the Parker Gray neighborhood.

##### **A. Building Design/Mass and Scale**

The site is located within a “transitional area” on the boundary of the Parker Gray District, Old and Historic District and a large office building on Cameron Street. Each of these street frontages has a different scale and character.

##### ***Fayette Street***

The Fayette Street elevation is the shortest (132.5 feet) facade and is intended to be smaller in scale in response to the buildings on the west side of Fayette Street and to be compatible with the generally smaller scale of the Parker-Gray District. In addition, the entire facade is set back from the sidewalk by approximately 15 feet to mimic the front yards of many houses of the District, such as the townhouses in the 400 block of North West Street. The Fayette Street facade is generally well resolved in mass, scale and architectural character.



**Figure 11 - Fayette Street Elevation**

Staff recommends the following changes to the Fayette Street elevation:

- Each of the doors as depicted on the preliminary elevations shall be operable.
- The top floor of the balconies-hyphens shall not include a roof.
- The three central townhouses shall consist of cementitious siding.
- That the roof and parapets be designed in a way to screen the roof-top deck.
- The three central clapboard townhouses shall be recessed 2' from the adjoining facades.

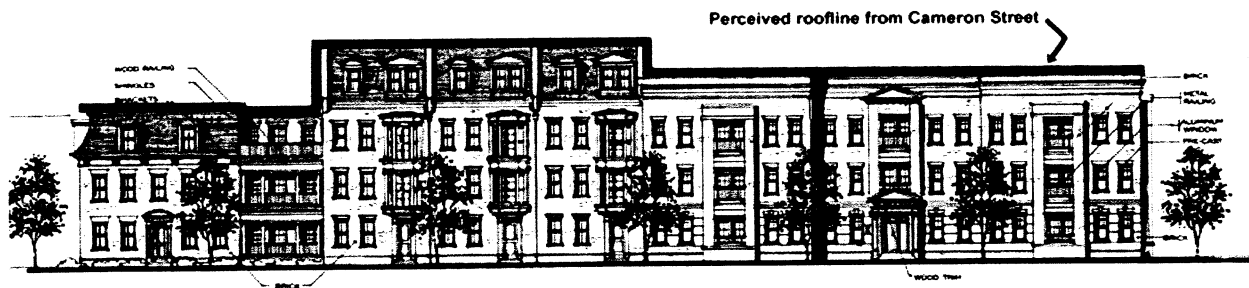
Cameron Street

The Cameron Street elevation is the longest elevation (230 feet) and contains the principal entrance for the condominium building. The proposed building consists of three sections, the longest being the four-story apartment building segment, followed by a section consisting of three 3-story townhomes with full mansards, and then, at the corner of Fayette and Henry Streets, a two-story house with full mansard. The corner house and townhouses are linked by a smaller segment intended to appear as an architectural hyphen or wing.



**Figure 12 - Applicant's Proposed Cameron Street Elevation**

Overall, the proposed facades of an apartment building, three townhouses and a single-family building on the corner provides an appropriate mix of building types for this portion of the City. However, the primarily four-story element on the corner of Cameron and Henry Street is inappropriate to the scale and fabric of the neighborhood. Therefore, staff recommends that a portion of the fourth floor be eliminated, as generally depicted in Figure 13, so that the building will appear as a three-story building from the adjoining streets. This was also a recommendation of the Parker Gray Historic District Commission and a concern expressed by the community. The modification would result in the elimination of up to 4 units and 4,400 square feet of floor area. This elimination of mass at the corner is essential to enable a better transition to the adjoining two- and three-story buildings.



**Figure 13 - Staff Proposal - Cameron Street**

The following changes to the Cameron Street elevations are also recommended:

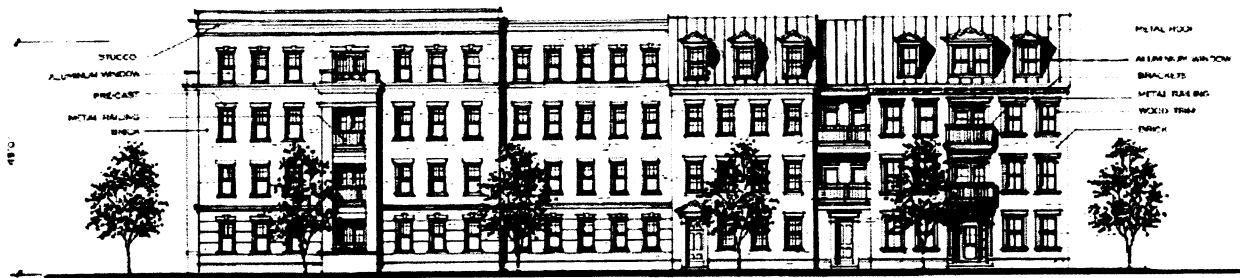
- The balcony-hyphen shall be revised to be subservient to the single-family building.
- The fourth level balconies for the three central townhomes shall be converted to dormers.
- The two-story balconies for the three central townhomes shall be converted to enclosed balconies consistent with the architectural style of the units.
- The eastern portion of the facade shall be revised per Condition #2.
- The setback for the hyphen shall be at least 8 feet, recessed behind the adjoining building faces, and the balconies shall either be flush with or recessed behind the adjoining building faces.

With these revisions the facade will appear to be two- to three-story, consistent with the character of the historic district. The proposed revisions to the mass also provide more variation in the roof heights, further reducing the perceived mass and scale of the proposed building.

### Henry Street

The Henry Street elevation is 159 feet long and consists of three major segments. Starting from the south at the corner with Cameron Street is the continuation of the multi-family segment. This segment is intended to have the appearance of a secondary elevation of a mid-size multi-family block with a recessed wing extending to the north. The multi-family segment is followed by a four-bay, gable-roofed townhouse and a two-bay hyphen. The Henry Street elevation is terminated by a substantial, five-bay, gable-roofed townhouse segment on the north end.

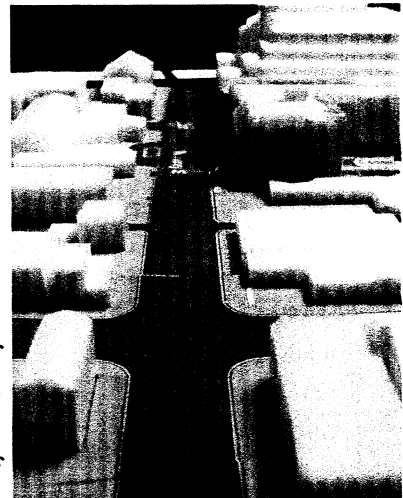




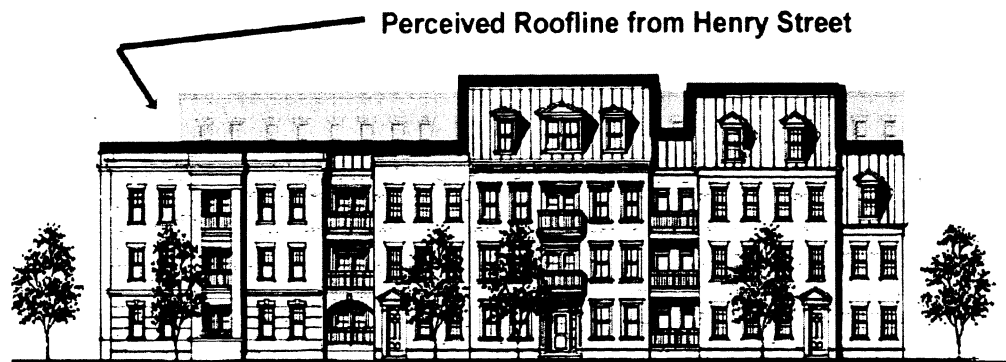
**Figure 14 - Applicant's Proposed Henry Street Elevation**

As previously discussed, this facade is the one to which significant revisions need to be made before the mass and scale are compatible with the existing character of Henry Street. As with the Cameron Street facade, staff recommends that the building be designed to appear as a three-story building and that a portion of the building be redesigned with more townhouse bays as generally depicted in Figure 16.

The other concern raised by the Parker Gray Board and shared by staff is the perception of the northern portion of the building on Henry Street. The proposed three- to four-story element is significantly larger than adjoining buildings and will provide an inappropriate transition to the adjoining Bradham auto repair facility. While the Bradham property may redevelop in the future, the northern portion of the building needs to be designed at a scale that is appropriate for the existing condition rather than an assumed future condition. The scale is also significantly greater than the adjoining two- to three-story buildings on Henry Street. For all of these reasons, staff is recommending that the north end of the building be reduced to three stories to provide a better transition. The scale and height of the proposal, even with the staff recommendations, will be considerably more than the one-story gas station and two-story building that are on the site today.



**Fig 15 - Massing Study  
(Proposal in Pink)**



**Figure 16 - Staff Proposal - Henry Street**

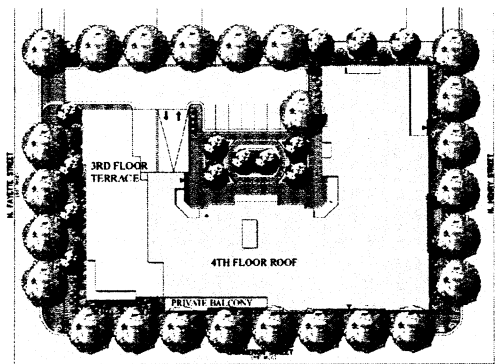
**B. Green Building**

In order to minimize the environmental impact of the proposed building, staff recommends that the developer incorporate green building technology into the design. A green building will incorporate elements such as non-toxic or recycled-content building materials, maintain good indoor air quality, use water and energy efficiently, feature natural lighting, and use energy-efficient window systems. A recommendation of approval is that the applicant incorporate these or comparable elements as part of the building design.

**C. Open Space and Pedestrian Streetscape**

The CD zone requires 40% open space, a portion of which may be roof-top decks. The proposal provides 40% open space, 26% of which will be at grade level, consisting of front yards and an internal courtyard, with the remaining 14% as a third-floor rooftop terrace. The proposed front yards on Fayette are visually accessible from North Fayette Street, consistent with other townhouses within Parker Gray that have small front yards. Additionally, the applicant is proposing landscaped areas along Henry Street and the north property line, and a central courtyard to serve residents.

Staff has concerns regarding open space that are related to both quantity and quality. To address the issue of quality, additional landscaping, irrigation and amenities such as benches are recommended to make the ground-level open space more useable and attractive. In addition, staff recommends that the four parallel, at-grade parking spaces be eliminated, that all of the surface parking spaces be compact spaces, and that two additional 90° parking spaces be added. This reconfiguration will result in the net loss of two parking spaces, but will provide additional open space, increase the buffer from the adjoining auto repair facility, and increase the amount of visible open space from Fayette Street.



**Figure 17 - Landscaping Plan**

In order to enhance the pedestrian environment on this block, staff recommends generous 10 foot wide brick sidewalks with street trees. This will significantly enhance an environment which is now characterized by curb cuts and limited landscaping. Staff is further recommending additional improvements, including:

- Street trees and landscaping along North Fayette, Cameron and North Henry Streets;
- Sodded lawns;
- Brick sidewalks;
- Stamped crosswalks;
- Bicycle parking; and
- Locating all overhead utilities below grade.

While the applicant is providing 26% of the site as ground-level open space, a significant portion of that ground-level open space is within the internal courtyard and is not visually or physically accessible to the public. Therefore, staff recommends that the applicant contribute \$10,000 for additional landscaping and park improvements to the nearby Hunter Miller Park (on the west side of Fayette Street), benefitting both the residents of the proposed development and the adjoining neighborhood.

**D. Parking**

The applicant has provided the on-site parking required by the zoning ordinance for each condominium unit, plus 10 (9%) surface parking spaces for visitors. Typically, a minimum of 15% visitor parking is required in conjunction with new residential developments, except in locations the applicant can demonstrate through a parking study that adequate on-street parking is available to meet visitor demand. This approach is one that requires an evaluation based upon the size and location of each project and staff will continue to closely evaluate with each application. In other cases, on-street parking is unavailable or in short supply, percentages higher than 15% have been applied. For example, recent developments have been required to provide as much as 20% visitor parking (Old Town Crescent), or as little as no on-site visitor parking at all (Braddock Lofts).

At the request of staff, the applicant submitted a parking study prepared by Gorove/Slade Associates, Inc. The parking study shows parking availability in the area in the evenings and on weekends, the peak periods for visitors. The analysis showed that during all times over five sample periods there is significant available parking for the proposed development within a one-block radius.

**Average Daily Parking Availability – 1-Block Radius**

Study Period	Total Spaces*	% Available	# Available
Thursdays (2 evenings)	206 Spaces	41% Available	85 Spaces Available
Friday (2 evenings)	206 Spaces	29% Available	59 Spaces Available
Saturday (2 days)	206 Spaces	24% Available	50 Spaces Available
Saturday (2 evenings)	206 Spaces	40% Available	81 Spaces Available
Sunday (2 days)	210 Spaces	28% Available	58 Spaces Available
Averages	207 Spaces	32% Available	67 Spaces Available

\*Note – the total number of spaces varies due to day-specific parking restrictions.

The streets adjacent to the site are Henry Street, Cameron Street, and Fayette Street. Henry Street and Cameron Street restrict parking for non-permit holders to two hours, while Fayette Street has no parking restrictions other than to prohibit parking two hours a week for street cleaning. The parking study indicates that within a 1-block radius, during peak visiting times, an average of 67 (32%) and a minimum of 50 (24%) on-street parking spaces are available. Therefore, the six visitor spaces which will not be provided on-site can easily be accommodated on the surrounding streets. Although the public streets can accommodate the expected number of periodic visitors, staff is concerned that the residents adjoining the public streets will find it more convenient to park on the streets rather than within the underground garage. Therefore, staff is recommending that a condition

of the parking reduction approval be that none of the residents of the development be eligible to apply for or receive any residential parking permits pursuant to City Code Sec. 5-8-71.

As discussed above, staff recommends the elimination of 2 surface parking spaces to increase open space and provide a landscaped buffer. This would reduce the number of visitor parking spaces to 8. However, adequate on-street parking is available within one block of the site to accommodate any visitors who cannot park on site, and the parking lot reconfiguration provides an appropriate balance between open space and parking needs.

#### **E. Traffic and Site Access**

Based on the applicant's traffic assessment report, this development will generate approximately 440 trips per day (220 entering and 220 exiting). During the AM peak hour, 37 trips will be generated (7 entering and 30 exiting), while during the PM peak hour, 42 trips will be generated (29 entering and 13 exiting). At build-out in 2006, site-generated traffic is expected to constitute 11 percent of the AM peak hour traffic on Fayette Street and 8 percent of the PM peak hour traffic. The total 2006 traffic volume on Fayette Street at the project site is projected to be approximately 320 vehicles during the AM peak hour and 510 vehicles during the PM peak hour. During both peak hours, these volumes are well below the operating capacity of Fayette Street.

At build-out, site-generated traffic will increase traffic demand at the adjacent intersections of Fayette Street with Cameron and Queen Streets by one to five percent during the peak hours. Both intersections currently operate at Level-of-Service A or B during the AM and PM peak hours, respectively. At build-out, these operating conditions are not projected to change. At build-out plus ten years, the applicant's traffic analysis concluded that both intersections will continue to operate at acceptable Levels-of-Service.

Overall, the traffic assessment concluded that the proposed development will not adversely impact Fayette Street or its intersections with Cameron and Queen Streets.

#### **Access**

Several of the adjoining residents have suggested that site access for this development be relocated from Fayette Street to Cameron Street in order to reduce potential impacts to Fayette Street. Staff appreciates this neighborhood concern; however, the findings of the traffic assessment do not suggest the need for such change in site access. The predominate access and egress patterns for this development are expected to be via Cameron and Queen Streets to the 200 block of Fayette Street where the site entrance is proposed. This access pattern combined with the existing through-traffic restriction on Fayette Street north of the site will effectively minimize any site-generated impacts.

#### **F. Affordable Housing**

The applicant is requesting a 20% increase in allowable FAR and density in exchange for providing affordable housing. The affordable housing bonus will yield 10 more dwelling units than would otherwise be allowed. Under the City's current program, the applicant is asked to provide one-third of the "bonus" dwelling units as affordable, plus pay a cash contribution of \$1 per gross square foot

of “non-bonus” dwelling units. This would equate to 3 dwelling units (3.3 units, rounded down), plus approximately \$62,650 in cash. As an alternative, the applicant proposes to provide 4 affordable dwelling units, three 2-bedroom units and one 1-bedroom unit, and no cash contribution. The proposal has been accepted by the Affordable Housing Advisory Committee.

The table below provides a comparison of the affordable housing contribution that is being provided for other recently approved projects. The table also lists the proposed contribution for the Prescott, as well as the approximate contribution that would be provided if reductions in building massing result in the loss of four dwelling units (the maximum number of units that staff anticipates may be lost).

**Comparison of Affordable Housing**

Project	Type	Units	Aff. D.U.s	% of Units	Value of Contribution	Contrib. per s.f.	% of Standard Contribution
Mill Race	Sales	326	13	4.0%	\$1,229,600	\$2.01	201%
Mill Race	Rental	369	15	4.1%	*	*	*
Preston	Sales	63	6	9.5%	\$100,435	\$1.04	104%
Cameron Station Condos	Sales	148	7	4.7%	\$975,000	\$2.07	207%
Northampton	Rental	572	25	4.4%	\$1,605,680	\$2.73	273%
Park Center	Sales	173	9	5.2%	\$720,000	\$3.48	348%
Prescott Proposed	Sales	64	4	6.3%	\$451,800	\$5.87	587%
Prescott Alternative	Sales	60	2	3.3%	\$312,650	\$4.06	406%

\*Note – figures not available, but the \$2.01 per gross square foot contribution value applies to the entire Mill Race project, including the office building.

**V. Conclusion**

The subject property is in a transitional location between the Parker Gray District, King Street, and the Old and Historic District. Therefore, it is critical to ensure that the mass and scale of the proposal is appropriate, and that the development appears to be a natural extension of the fabric of these three neighborhoods. As such, the proposed condominium building has been designed to appear to be multiple buildings with a variety of styles and types, including an apartment building, single-family homes, and townhomes. The project is designed with four stories on Cameron Street and Henry Street, to provide a transition to the three stories along Fayette Street. However, additional changes are needed in order to ensure that the project will not be out of character with the surrounding neighborhood. Most critical is for the portion of the building that is at the corner of Cameron and Henry Streets to be designed to have the appearance of a three-story building, so that it is not out of character with other buildings on Henry Street. The reduction in the overall mass and scale and the provision of affordable housing provide a project that is more compatible with the character and scale of the neighborhood.

## VI. STAFF RECOMMENDATION:

Staff recommends **approval** subject to compliance with all applicable codes and ordinances and the following conditions:

### Building Design:

1. **CONDITION REVISED BY THE PLANNING COMMISSION:** The building at the corner of Henry and Cameron Street shall be revised as depicted in *Attachment # 1* to the satisfaction of the Director of P&Z. The revisions to this portion of the building shall at a minimum include the following:
  - a. The fourth floor shall provide a setback of approximately 10 ±5 feet, as generally depicted on the sight line study prepared by The Lessard Architectural Group, Inc., and shall not be visible from the adjoining sidewalks. A corner element, along with a vision clearance modification if necessary, may be permitted, subject to the satisfaction of the Director of Planning and Zoning.
  - b. A portion of the building shall be converted to appear as two townhouses on Henry Street.
  - c. The height of the northern portion of the building on Henry Street shall be reduced in height to appear as a two-story element with a roof and dormers. (P&Z) (PC)
  
2. **CONDITION REVISED BY THE PLANNING COMMISSION:** The final design for the building shall be consistent in massing, articulation, general design and the architectural styles and character as generally depicted in the architectural elevations dated August 23, 2004, and also provide additional refinements to the satisfaction of the Director of P&Z that shall include:
  - a. With the exception of the multi-family building entrance, all entrances should replicate the scale, pattern, and architectural detail of a typical townhouse entrance.
  - b. With the exception of architectural detailing and the screening for the mechanical equipment, the materials of the entire building shall be entirely masonry (brick, precast, stone), except the three townhouses on Fayette as required below.
  - c. The proportions of the windows, doors and design elements shall be consistent with the architectural style of each facade.
  - d. The HVAC units and mechanical appurtenances shall be located on the roof-tops, recessed and screened from view from the public streets. Details on the screening methods shall be provided on the final site plan.
  - e. Color elevations shall be submitted with the final site plan.
  - f. All refinements to the design and materials shall be revised prior to the release of the final site plan .
  - g. Architectural elevations (front, side and rear) shall be submitted with the final site plan. Each elevation shall indicate the average finished grade line.
  - h. The units shall continue to provide varying roof materials such as composite shingles and metal roofs.
  - i. The balconies shall be painted and the railing spacing shall be designed to provide screening for the balconies.

Fayette Street

- j. Each of the doors as depicted on the preliminary elevations shall be operable.
- k. The balconies serving the top floor of the hyphens shall not include a roof.
- l. The three central townhouses shall consist of cementitious or wood siding.
- m. The roof terrace on the top of the three central units shall be set back an additional 8 feet so that the railing is not visible from the adjoining streets, or the railing incorporated as part of the parapet.
- n. The three central clapboard townhouses shall be recessed a minimum of 2 feet from the adjoining facades.

Cameron Street

- o. The balconies serving the top floor of the hyphens shall not include a roof.
- p. The fourth level balconies for the three central townhomes shall be converted to dormers, unless otherwise determined by the Director of Planning and Zoning.
- q. The two story balconies for the three central townhomes shall be converted to enclosed balconies consistent with the architectural style of the units, unless otherwise determined by the Director of Planning and Zoning.
- r. The eastern portion of the facade shall be revised per Condition #1.
- s. The hyphen shall be recessed behind the adjoining building faces, and the front edges of the balconies shall either be recessed behind or coplanar with the adjoining building faces.

Henry Street

- t. The southern portion of the facade shall be revised per Condition #1.
- u. The height of the north end of the northern townhouse shall be reduced to appear as three stories from the north per Condition # 1. (P&Z) (PC)

- 3. **CONDITION REVISED BY THE PLANNING COMMISSION:** ~~The building shall incorporate the use of green building technology and sustainable techniques for building systems design and efficiency to the satisfaction of the Director of P&Z. The building shall reflect the use of sustainable techniques for building systems design and efficiency through the use of sustainable materials such as Hardi plank, lower emission paint, and energy efficient appliances, windows, and mechanical systems.~~ (P&Z) (PC)
- 4. All stairs extend thru the roof so that door access to the roof for firefighting purposes is provided. (Code)

**Landscaping**

- 5. The applicant shall contribute an amount of \$10,000 for landscaping and park improvements to the nearby Hunter Miller Park. The amount shall be paid prior to the release of the final site plan. (P&Z) (RP&CA)
- 6. **CONDITION REVISED BY THE PLANNING COMMISSION:** The final landscape plan shall be provided with the final site plan to the satisfaction of the Director of P&Z and RP&CA. The plan shall include the level of landscaping depicted on the preliminary landscape plan and shall also provide:

- a. Street trees along Fayette and Cameron Streets shall be planted in 5 foot by 12 foot tree wells. A manual irrigation system shall be provided for the tree wells.
- b. The developer shall be responsible for the installation and maintenance of trees adjacent to the public streets through bond release. This maintenance shall include, but not be limited to, pruning, watering, pest control, and replacement of street trees as necessary.
- c. The tree wells will be planted with an evergreen ground cover such as vinca minor or lirioppe.
- d. The tree wells on Henry Street shall be replaced with a 5 foot wide continuous planting strip for the street trees. The landscape strip shall include groundcover. A manual irrigation system shall be provided for the planting strip.
- e. The caliper of the street trees shall be a minimum of 3 ½" - 4" at the time of planting.
- f. The planting depth on top of the planting deck shall be a minimum of 2'-3' for the shrubs, groundcover, and ornamental trees, and a minimum of 4' of soil depth for shade trees, with adequate drainage to support the trees without the use of raised planters. The raised planters on the perimeter of the building shall be eliminated.
- g. Ornamental trees shall be provided with the open space-front yards for each of the units on Fayette Street
- ~~h. The stamped asphalt crosswalks at the intersection of Henry and Cameron shall be eliminated.~~
- i. Additional street trees on Henry Street and Cameron Street shall be provided.
- j. The parking garage vents shall be located at grade and be of a size and type to minimize the impact on open space and visibility from adjoining streets. Landscape screening shall be provided, if necessary to screen the vents.
- k. The landscape, hardscape and roof-top open space plans shall be prepared and sealed by a licensed landscape architect
- l. All lawn areas shall be sodded.
- m. All utility structures, including transformers, shall be depicted on the final development plan. All utility structures (except fire hydrants) shall be clustered where possible and located so as not to be visible from a public right-of-way or adjoining property. When such a location is not feasible, such structures shall be located behind the front building line and screened.
- n. Crown coverage for street trees shall be denoted under a separate tabulation.
- o. All plant specifications shall be in accordance with the current and most up to date edition of the American Standard For Nursery Stock (ANSI Z60.1) as produced by the American Association for Nurserymen; Washington, D.C.
- p. All work shall be performed in accordance with Landscape Specifications Guidelines, 4th Edition as produced by the Landscape Contractors Association (LCA) of Maryland, District of Columbia and Virginia; Gaithersburg, Maryland.
- q. Utility lines such as water, storm sewer and electric lines shall be located to minimize impacts on proposed street trees and open space.
- r. The location of all light poles shall be coordinated with the street trees.
- s. As trees mature they are to be limbed up to a minimum 6 feet or greater as necessary to maintain traffic sign visibility. Trees are not to be planted under or near light poles
- t. The maximum height for the shrubs is 36 inches.
- u. No shrubs higher than three feet shall be planted within six feet of walkways.



- v. All landscaping shall be maintained in good condition and replaced as needed. (P&Z) (RP&CA) (PC)
7. The open space and landscaping for the each unit on Fayette Street shall be revised to have the appearance of individual front yards, through the use of landscaping and/or fencing and lead walks for each unit. The two “single-family” homes on each end of Fayette Street shall provide open decorative metal fencing adjacent to the right-of-way to delineate the yards. (P&Z)
8. The central courtyard shall provide the amount of landscaping and amenities as depicted on the preliminary plan and to the satisfaction of the Director of P&Z and shall also at a minimum provide:
- a. A focal element such as a sculpture or water feature within the courtyard to provide a focal element that is an appropriate scale for the space of the courtyard.
  - b. Additional amenities such as special paving surfaces, materials, benches, trash receptacles, landscaping, etc. shall be provided within the courtyard to encourage its use.
  - c. Planters within the courtyard for shade trees as well as other landscaping.
  - d. The plaza shall be redesigned to the building entrance in the corner of the courtyard.
  - e. A manual irrigation system for all landscaping.
  - f. Low scale pathway or bollard lighting.
  - g. Wheel stops or curbing shall be provided for the adjoining ninety-degree parking spaces to prevent cars from projecting onto the adjoining sidewalk. (P&Z)
9. The applicant shall provide pedestrian streetscape improvements that at a minimum shall provide the level of improvements depicted on the preliminary plan and shall also provide the following to the satisfaction of the Director of P&Z:
- a. All sidewalks for the development shall be brick and shall comply with City standards.
  - b. The brick sidewalk shall continue over the proposed curb cut for the parking garage on Fayette Street to provide a continuous uninterrupted brick sidewalk.
  - c. The sidewalk width on Cameron and Fayette Streets shall be reduced from 7 feet to 6 feet to increase the landscape buffer adjacent to the building.
  - d. All sidewalks shall align and connect with the existing sidewalks.
  - e. A public access easement shall be provided for the portion of the sidewalk on Henry Street not located within the public right-of-way.
  - f. All streetscape improvements shall be completed prior to the issuance of a certificate of occupancy permit.
  - g. Provide two (2) city standard street cans, to the satisfaction of the Director of T&ES.
  - h. All appropriate on-street parking signage and any other signage for control of pedestrians and vehicles adjoining the site shall be installed by the developer to the satisfaction of the Director of T&ES. (P&Z) (T&ES)

10. The rooftop open space shall be designed to function as high-quality usable open space for the residents. At a minimum, the revised plans for the plaza decks shall include the following to the satisfaction of the Directors of P&Z and RP&CA:
  - a. Features and elements such as seating, trash receptacles, and pedestrian-scale lighting;
  - b. Varied and high quality paving materials;
  - c. A significant amount of additional landscaping, including manual irrigation systems;
  - d. The railing for the roof-top deck shall be set back a minimum of 8 feet or incorporated as part of the parapet so that the railing is not visible from the adjoining streets; and
  - e. The lighting for the roof-top open space shall be pedestrian scale lighting and shall not be visible from the adjoining streets. Noise generated in conjunction with the roof top open space shall be limited to the type and hours normally associated with a residential use. (P&Z)
11. Coordinate landscape plan with fire service plan. (Code)
12. In order to provide the required corner vision clearance, all street trees shall be located a minimum of 35 feet from the back-of-curb of the nearest intersecting street. (T&ES)

### **Parking**

13. Residents of the building shall be ineligible to apply for or receive any residential parking permits pursuant to City Code Sec. 5-8, Article F. (P&Z) (T&ES)
14. The four parallel spaces on the northern portion of the site shall be removed and replaced with open space and landscaping. Two additional 90° spaces shall be located adjacent to the internal courtyard, to the satisfaction of the Director of Planning and Zoning. (P&Z)
15. In order to promote teleworking, each of the units will be wired for high-speed internet access. (P&Z)
16. A minimum of 110 parking spaces, as represented on the preliminary plan, shall be located in the underground garage. (P&Z)
17. A minimum of one space for each unit shall be provided within the garage as part of the purchase price for each unit. (P&Z)
18. The applicant shall provide controlled access into the underground garage. The controlled access to the garage shall be designed to allow convenient access to the underground parking for residents and visitors. (P&Z)
19. The applicant shall provide a minimum of seven (7) bicycle parking spaces for use by residents within the garage and two at-grade (2) visitor bicycle parking spaces, to the satisfaction of the Director of T&ES. (T&ES)

20. The applicant shall provide off-street parking for all construction workers without charge. For the construction workers who use Metro, DASH, or another form of mass transit to the site, the applicant shall subsidize a minimum of 50% of the fees for mass transit. Compliance with this condition shall be based on a plan, which shall be submitted to the Department of P&Z and T&ES prior to the issuance of the Excavation/Sheeting, and Shoring Permit. This plan shall set forth the location of the parking to be provided at various stages of construction, how many spaces will be provided, how many construction workers will be assigned to the work site, and mechanisms which will be used to encourage the use of mass transit. The plan shall also provide for the location on the construction site at which information will be posted regarding Metro schedules and routes, bus schedules and routes. If the plan is found to be violated during the course of construction, a correction notice will be issued to the developer. If the violation is not corrected within ten (10) days, a "stop work order" will be issued, with construction halted until the violation has been corrected. (P&Z) (T&ES)
21. Provide 1 additional handicap parking space for a total of 4 spaces per USBC. (Code)
22. A second, rated egress stair is required in the garage. The garage ramp does not qualify as a means of egress. (Code)

#### **Site Plan**

23. All existing and proposed utility poles and overhead electrical/telephone lines adjoining the site shall be located underground and the cost of such undergrounding shall be the sole responsibility of the developer. All transformers shall be located adjacent to the internal street to the satisfaction of the Director of P&Z. (P&Z) (T&ES)
24. The portion of the drive aisle that is located on top of the parking garage shall be designed to AASHTO HS-20 loading requirements of delivery trucks and fire apparatus to the satisfaction of the Directors of P&Z, T&ES and Code Enforcement. (P&Z)
25. The two existing lots shall be consolidated. The plat of consolidation shall be submitted with the submission of the first final site plan. The plat of consolidation shall be approved and recorded prior to the release of the final site plan. (P&Z)
26. A freestanding subdivision or development sign shall be prohibited. (P&Z)
27. Show all utility structures, including transformers, on the final development plan. All utility structures (except fire hydrants) shall be clustered where possible and located so as not to be visible from a public right-of-way or property. When such a location is not feasible, such structures shall be located behind the front building line and screened to the satisfaction of the Director of P&Z. (P&Z)
28. The applicant shall be allowed to make minor adjustments to the building location if the changes do not result in off-street parking or open space below that required by the Zoning Ordinance and do not result in an increase in building height or floor area ratio. (P&Z)

29. Temporary structures for construction shall be permitted and the period of such structures shall be subject to the approval of the Director of P&Z. The trailer shall be removed prior to the issuance of the certificate of the last certificate of occupancy permit. (P&Z)
30. All condominium association covenants shall be approved by the Director of P&Z and the City Attorney prior to applying for the first certificate of occupancy permit for the project. The association covenants shall include the conditions listed below, which shall be clearly expressed in a separate section of the covenants.
  - a. The principal use of the underground garage and parking spaces shall be for passenger vehicle parking only; storage which interferes with the use of a parking space for a motor vehicle is not permitted.
  - b. The designated visitor parking spaces shall be reserved for the use of the condominium guests.
  - c. No more than two parking spaces shall be assigned to a specific condominium unit; all unassigned spaces in the garage shall be made generally available to residents and/or visitors.
  - d. That a public access easement for the pedestrian connection is provided for the sidewalk on Henry Street.
  - e. All landscaping and open space areas within the development, shall be maintained by the homeowners and condominium owners.
  - f. Exterior building improvements or changes by future residents shall require the approval of the Parker Gray BAR and/or City Council, as determined by the Director of P&Z. (P&Z)
31. Any inconsistencies between the various drawing submitted by the applicant shall be reconciled to the satisfaction of the Director of P&Z and T&ES. (P&Z)
32. The applicant shall identify a person who will serve as liaison to the community throughout the duration of construction. The name and telephone number of this individual shall be provided in writing to residents, property managers and business owners whose property abuts the site, and to the Directors of P&Z and T&ES. (P&Z)
33. A temporary informational sign shall be installed on the site prior to approval of the first final site plan for the project and shall be displayed until construction is complete or replaced with a marketing sign incorporating the required information: the sign shall notify the public of the nature of the upcoming project and shall provide a phone number for public questions regarding the project. (P&Z)
34. Temporary construction and sales trailer(s) shall be permitted and be subject to the approval of the Director of P&Z. The trailer(s) shall be removed prior to the issuance of a certificate of occupancy permit for the building. (P&Z)
35. Provide a site lighting plan to the satisfaction of the Director of T&ES in consultation with the Chief of Police. The plan shall show the existing and proposed street lights and site lights. Indicate the type of fixture, and show mounting height, and strength of fixture in Lumens or Watts. Provide manufacturer's specifications for the fixtures. Provide lighting

calculations to verify that lighting meets city standards and are located to prevent excessive spillover lighting and glare from adjacent properties. (T&ES) (P&Z)

36. Before commencing any clearing or grading of the site, the applicant shall hold a meeting with all adjoining property owners to review the hauling routes, location of construction worker parking, plan for temporary pedestrian and vehicular circulation, and hours and overall schedule for construction. The Departments of P&Z and T&ES shall be notified of the date of the meeting before the permit is issued. Copies of plans showing the hauling route, construction worker parking and temporary pedestrian and vehicular circulation shall be posted in the construction trailer and given to each subcontractor before they commence work on the project. (P&Z)
37. The applicant shall submit a final location survey for the buildings and interior of the parking garage prior to issuance of a certificate of occupancy permit. (P&Z)
38. Provide all pedestrian and traffic signage to the satisfaction of the Director of T&ES. (T&ES)
39. All Traffic Control Device design plans, Work Zone Traffic Control plans, and Traffic Studies shall be sealed by a professional engineer, registered in the Commonwealth of Virginia. (T&ES)
40. Prior to the release of the final site plan, provide a Traffic Control Plan for construction detailing proposed controls to traffic movement, lane closures, construction entrances, haul routes, and storage and staging. (T&ES)
41. The proposed Cameron Street crosswalk at Henry Street shall be moved closer to the intersection. (T&ES)
42. Provide a brick paver pedestrian crossing across the entrance at Fayette Street, designed to the satisfaction of the Director of T&ES. (T&ES)
43. All driveway entrances and sidewalks in public ROW or abutting public ROW shall meet City standards. (T&ES)
44. Show all existing and proposed easements, both public and private. (T&ES)
45. Replace existing curb and gutter, sidewalks, and handicap ramps adjacent to the site that are in disrepair or broken. (T&ES)
46. Where needed, replace inlets along the frontage of the site and provide new inlets, to the satisfaction of the Director of T&ES. (T&ES)
47. **CONDITION REVISED BY THE PLANNING COMMISSION:** Applicant to provide the capital cost to install countdown pedestrian signals and Prisma accessible pedestrian signals at the intersection of Henry and Cameron Streets, not to exceed \$5,000. (T&ES) (PC)

48. Applicant to provide conduit for future traffic signal at intersection of Fayette Street and Cameron Street, as shown on preliminary plan. (T&ES)
49. Applicant shall provide two four-inch traffic signal conduits running along Henry Street and Cameron Street under the sidewalk, behind the tree planters. These conduits shall terminate in a junction box at the corner of Henry and Cameron Streets. (T&ES)
50. Slopes of garage ramps shall not exceed 10%, to the satisfaction of the Director of T&ES. (T&ES)
51. All archaeological work shall be carried out in accordance with the *City of Alexandria Archaeological Standards* and is subject to the approval of the City Archaeologist. (Archaeology)

### **Stormwater**

52. Developer to comply with the peak flow requirements of Article XIII of AZO, as determined by the Director of T&ES. (T&ES)
53. **CONDITION REVISED BY THE PLANNING COMMISSION:** Note 7 on Sheet 3A states that the “applicant requests a waiver of water quality due to the elimination of discharge into the combined system.” The City will not waive the requirements of Article XIII of the AZO because a) the infrastructure required to make the connection to the separate sanitary sewer is not unreasonable, and b) a written request was not submitted in accordance with Memorandum to Industry No. 2002-0001, dated January 4, 2002. Remove Note #7 from Sheet 3A and references to this note throughout the plans. This condition shall not preclude appropriate credits if deemed proper by the Director of T&ES. (T&ES) (PC)
54. **CONDITION REVISED BY THE PLANNING COMMISSION:** Plan must demonstrate to the satisfaction of the Director of T&ES that adequate stormwater outfall is available to the site ~~or else~~ If adequate outfall is unavailable, the developer is to design and build any on- or off-site improvements to discharge to an adequate outfall, to the satisfaction of the Director of T&ES. (T&ES) (PC)
55. The applicant is advised that all stormwater designs that require analysis of pressure hydraulic systems and/or inclusion and design of flow control structures must be sealed by a professional engineer, registered in the Commonwealth of Virginia. If applicable, the Director of T&ES may require resubmission of all plans that do not meet this standard. (T&ES)
56. Provide proposed elevations (contours and spot shots) in sufficient details on grading plan to clearly show the drainage patterns. (T&ES)

57. The project lies within the Combined Sewer District; thus storm water management and compliance with the City's Chesapeake Bay program must be coordinated with City's policy for management of the Combined Sewer District. Applicant shall connect the sanitary sewer for the project to the Potomac Yard Trunk Sewer located at the intersection of Queen and Fayette Streets. (T&ES)
58. Provide complete pre- and post-development drainage maps including topographic information, storm drains, BMP's and either Worksheet A or B. (T&ES)
59. Provide complete drainage divide map for the project area including areas that contribute surface runoff from beyond project boundaries. Include topographic information, storm drains and storm water Best Management Practices (BMPs). (T&ES)
60. The stormwater Best Management Practices (BMPs) required for this project shall be constructed and installed under the direct supervision of the design professional or his designated representative. Prior to release of the performance bond, the design professional shall submit a written certification to the Director of T&ES that the BMPs are:
  - a. Constructed and installed as designed and in accordance with the approved Final Site Plan.
  - b. Clean and free of debris, soil, and litter by either having been installed or brought into service after the site was stabilized. (T&ES)
61. The applicant shall submit a storm water quality BMP Maintenance Agreement with the City to be reviewed as part of the Final #2 Plan. It must be executed and recorded with the Land Records Division of Alexandria Circuit Court prior to approval of the final site plan. (T&ES)
62. The applicant shall be responsible for maintaining storm water Best Management Practices (BMPs) until activation of the homeowner association (HOA), if applicable, or until sale to an owner. Prior to transferring responsibility for the BMPs to the HOA or owner, the applicant shall execute a maintenance service contract with a private contractor for a minimum of three years and transfer the contract to the HOA or owner. A copy of the contract shall also be placed in the BMP Operation and Maintenance Manual. Prior to release of the performance bond, a copy of the contract shall be submitted to the City (T&ES)
63. If the units will be sold as individual units and a homeowner's association established the following two conditions shall apply:
  - a. The applicant shall furnish the Homeowner Association with an Owner's Operation and Maintenance Manual for all the Best Management Practices (BMP's) used on site. The manual shall include at a minimum: an explanation of the functions and operations of the BMP(s); drawings and diagrams of the BMP(s) and any supporting utilities; catalog cuts on maintenance requirements including mechanical or electrical equipment; manufacturer contact names and phone numbers; a copy of the executed maintenance service contract; and a copy of the maintenance agreement with the City.

- b. The Developer shall furnish each home purchaser with a brochure describing the storm water BMP(s) installed on the site, outlining the responsibilities of the homeowners and the Homeowner s Association (HOA) with respect to maintenance requirements. Upon activation of the HOA, the Developer shall furnish five copies of the brochure per unit to the HOA for distribution to subsequent homeowners. (T&ES)
64. Prior to release of the performance bond, a copy of the Operation and Maintenance Manual shall be submitted to the City on a digital media. (T&ES)
65. Prior to release of the performance bond, the applicant is required to submit a certification by a qualified professional to the satisfaction of the Director of T&ES that the existing storm water management facility adjacent to the project and associated conveyance systems were not adversely affected by the construction and that they are functioning as designed and are in a condition similar to prior to construction began. If maintenance of the facility or systems were required in order to make this certification, provide a description of the maintenance performed. (T&ES)

### **Housing**

66. The developer shall set aside one-third of any bonus density units, rounded to the nearest whole unit, at prices not to exceed \$225,000 each for two-bedroom units, for sale to households with incomes not exceeding the City's income guidelines for the Moderate Income Housing Program (MIHP), which are currently \$68,700 for a one or two person household and \$79,500 for a three or more person household. All bonus density affordable units provided shall be two-bedroom units. The developer shall make a \$54,500 cash contribution to the City's Housing Trust Fund for the base building, or, if the project is approved with 10 bonus density units as proposed, the developer shall include one-bedroom unit for sale at \$173,200 to households with incomes as described above, in lieu of the cash contribution for the base building. Prices include one garage parking space for each unit. (Housing)
67. The set-aside units shall be of the same size, type and with the same amenities as other similar units in the development. The units shall be scattered throughout the building. (Housing)
68. Whatever incentives are offered to any potential home buyers shall also be offered to purchasers of the set-aside units. (Housing)
69. Thirty year afford ability of the set-aside units shall be provided through deed restrictions recorded as covenants at the time of sale of each of the set-aside units, in accordance with the City's set-aside resale policy. Language for the covenants shall be provided by the City in advance of the final sale of any unit. (Housing)



70. The City shall have exclusive right to market the set-aside units for 90 days, after which the developer will also have the right to market the units, at the affordable price, to buyers qualified for the set-aside program. The developer is encouraged to refer potentially qualified buyers to the Office of Housing's Implementation Division for participation in the set-aside and MIHP programs. In the event a qualified buyer cannot be found for a set-aside unit by the time the unit is completed and ready for occupancy, the developer shall have the right to sell the unit at full market price, and shall contribute to the Housing Trust Fund the equivalent of \$5.87 per gross square foot of each unit so sold. (Housing)
71. If the developer sells the market rate units for less than expected, the applicant shall index the price on the affordable units proportionately. (Housing)
72. Real estate commissions shall be paid (or not paid) for set-aside units in the same manner and on the same basis as for market rate units. (Housing)

**General**

73. In the event that Section 5-1-2(12b) of the City Code is amended to designate multi-family dwellings in general, or multi-family dwellings when so provided by SUP, as required user property, then refuse collection shall be provided by the City. (T&ES)
74. The developer agrees to deliver all solid waste, as defined by the Code of the City of Alexandria, to a refuse disposal facility designated by the Director of T&ES. The developer further agrees to stipulate in HOA documents that all tenants and/or property owners shall also comply with this requirement. (T&ES)
75. **CONDITION REVISED BY THE PLANNING COMMISSION:** The sidewalk along Route 1 shall remain open during construction, to the satisfaction of the Director of T&ES. (T&ES) (PC)
76. Plan does not indicate whether or not there are any known soil and groundwater contamination as required with all preliminary submissions. Should any unanticipated contamination or underground storage tanks, drums and containers are encountered at the site the applicant must immediately notify the City of Alexandria Department of Transportation and Environmental Services, Division of Environmental Quality. (T&ES)
77. If fireplaces are to be included in the development, the applicant is required to install gas fireplaces to reduce air pollution and odors. Animal screens must be installed on chimneys. (T&ES)
78. A "Certified Land Disturber" must be named on the Erosion & Sedimentation Control sheets prior to commencement of activity in accordance with the Virginia Department of Conservation and Recreation guidelines. (T&ES)
79. During the construction phase of this development, the site developer, its contractor, certified land disturber, or owner's other agents shall implement a waste and refuse control program.

This program shall control wastes such as discarded building materials, concrete truck washout, chemicals, litter or trash, trash generated by construction workers or mobile food vendor businesses serving them and sanitary waste at the construction site and prevent its off site migration that may cause adverse impacts to the neighboring properties or the environment to the satisfaction of Directors of Transportation and Environmental Services and Code Enforcement. All wastes shall be disposed off site properly in accordance with all applicable federal, state and local laws. (T&ES)

80. The applicant shall present a disclosure statement to potential buyers disclosing the following to the satisfaction of the Director of P&Z and the City Attorney:  
That a major highway, with its associated issues such as traffic and noise, is located adjacent to the project. (T&ES)
81. Due to the close proximity of the site to the Jefferson Davis Highway the following conditions shall be included in the development requirements:
  - a. Applicant shall prepare a noise study identifying the levels of interior noise residents of the project will be exposed to at the present time and 10 years into the future in a manner consistent with the Noise Guidance Book used by the Department of Housing and Urban Development (HUD).
  - b. Identify options to minimize noise exposure to future residents at the site, particularly in those units closest to railroad, including: triple-glazing for windows, additional wall/roofing insulation, installation of resilient channels between interior gypsum board and wall studs and any other special construction methods to reduce sound transmission. If needed, the applicant shall install some combination of the above to the satisfaction of the Directors of P&Z and T&ES.
  - c. The noise study shall be submitted and approved prior to final site plan approval. (T&ES)
82. **CONDITION ADDED BY PLANNING COMMISSION: The applicant shall contribute \$10,000 toward the design and installation of traffic calming measures on Fayette Street prior to the release of the final site plan. (PC)**
83. **CONDITION ADDED BY PLANNING COMMISSION: Hauling routes shall be subject to existing truck restrictions and done in a way to minimize impact on the neighborhood to the satisfaction of the Director of T&ES. (PC)**
84. **CONDITION ADDED BY PLANNING COMMISSION: The applicant shall present a disclosure statement to potential buyers disclosing all existing traffic restrictions in the neighborhood surrounding the property, to the satisfaction of the Director of P&Z. (PC)**

STAFF: Eileen Fogarty, Director, Department of Planning and Zoning;  
Jeffrey Farner, Chief, Development;  
David Sundland, Urban Planner;  
Laura Durham, Urban Planner.

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Staff Note: In accordance with Section 11-506(c) of the zoning ordinance, construction or operation shall be commenced and diligently and substantially pursued within 18 months of the date of granting of a special use permit by City Council or the special use permit shall become void.

ADDITIONAL CITY DEPARTMENT CODE COMMENTS

Legend: C - Code Requirement R - Recommendation S - Suggestion F - Finding

Code Enforcement

- C-1 The developer shall provide a building code analysis with the following building code data on the plan: a) use group; b) number of stories; c) type of construction; d) floor area per floor; e) fire protection plan.
- C-2 The developer shall provide a separate Fire Service Plan which illustrates: a) emergency ingress/egress routes to the site; b) two fire department connections (FDC) to the building, one on each side/end of the building; c) fire hydrants located within one hundred (100) feet of each FDC; d) on site fire hydrants spaced with a maximum distance of three hundred (300) feet between hydrants and the most remote point of vehicular access on site; e) emergency vehicle easements (EVE) around the building with a twenty-two (22) foot minimum width; f) all Fire Service Plan elements are subject to the approval of the Director of Code Enforcement. Fire Service Plan shall be incorporated as an additional sheet in plan set.
- C-3 Fire Department ladder truck access is required for the two long sides of all buildings over 50 feet in height. This requires a truck to be able to position itself between 15 and 30 feet from the face of the building. All elevated structures used for this purpose shall be designed to AASHTO HS-20 loadings.
- C-4 Provide two Siamese connections to the building located to the satisfaction of the Director of Code Enforcement. Provide hydrants located within 100 feet of each FDC and on the same side of the street as each FDC.
- C-5 A separate tap is required for each building fire service connection.
- C-6 The public parking garage (Use Group S-2) is required to be equipped with a sprinkler system and shall be mechanically ventilated.
- C-7 The public parking garage floor must comply with USBC and drain through oil separators or traps to avoid accumulation of explosive vapors in building drains or sewers as provided for in the plumbing code. This parking garage is classified as an S-2, Group 2, public garage. Floors of public garages must be graded to drain through oil separators or traps to avoid accumulation of explosive vapors in building drains or sewers.
- C-8 Prior to the issuance of a demolition permit or land disturbance permit, a rodent abatement plan shall be submitted to Code Enforcement that will outline the steps that will be taken to prevent the spread of rodents from the construction site to the surrounding community and sewers.

- C-9 Prior to submission of the Final Site Plan, the developer shall provide a fire flow analysis by a certified licensed fire protection engineer to assure adequate water supply for the structure being considered.
- C-10 A soils report must be submitted with the building permit application.
- C-11 A Certificate of occupancy shall be obtained prior to any occupancy of the building or portion thereof, in accordance with USBC 118.0.
- C-12 Required exits, parking, and accessibility within the building for persons with disabilities must comply with USBC Chapter 11. The applicant shall define if handicap persons will have access to the underground parking structure. In addition, the plans do not show how surface parking pedestrians will access the structure at the rear of the building. There is too much overlying detail to clearly assess whether there is sufficient handicap access to the front lobby from Cameron Street.
- C-13 Sheeting and shoring shall not extend beyond the property line; except when the developer has obtained a written release from adjacent property owners which has been recorded in the land records; or through an approved encroachment process.
- R-1 For firefighting reasons it is recommended that all stairs extend thru the roof so that door access to the roof is provided.

Transportation and Environmental Services (T&ES)

- C- 1 An appropriate performance bond for the public improvements must be posted prior to release of the plan.
- C- 2 All downspouts must be connected to a storm sewer by continuous underground pipe.
- C- 3 The sewer tap fee must be paid prior to release of the plan.
- C- 4 All easements and/or dedications must be recorded prior to release of the plan.
- C- 5 Plans and profiles of utilities and roads in public easements and/or public right-of-way must be approved prior to release of the plan.
- C- 6 All drainage facilities must be designed to the satisfaction of T&ES. Drainage divide maps and computations must be provided for approval.
- C- 7 All utilities serving this site to be underground.
- C- 8 Provide site lighting plan to meet minimum city standards.
- C- 9 The applicant shall comply with the City of Alexandria's Noise Control Code, Title 11, Chapter 5, which sets the maximum permissible noise level as measured at the property line.

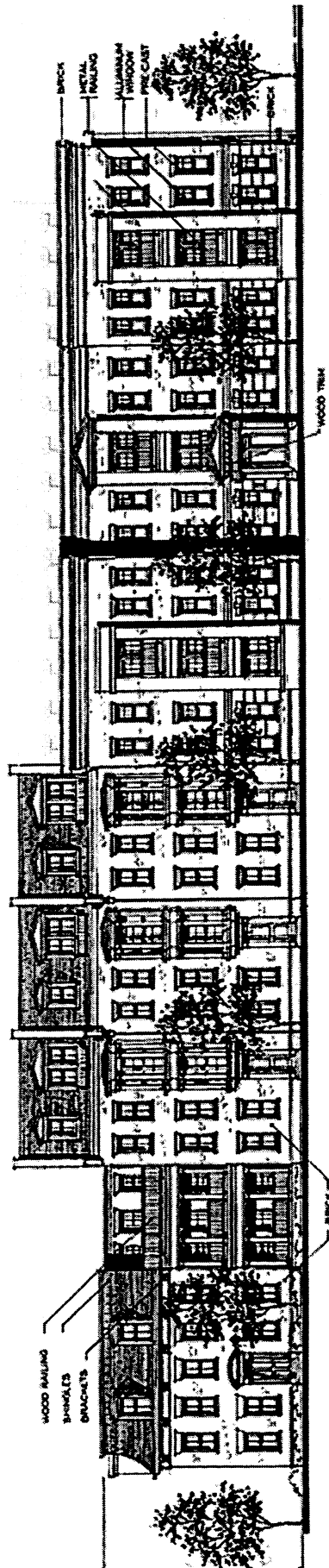
- C-10 The applicant must comply with the Article XIII of the City's zoning ordinance, which includes requirements for stormwater pollutant load reductions, treatment of the water quality volume default, and stormwater quantity management.
- C-11 The applicant must comply with the City of Alexandria, Erosion and Sediment Control Code, Section 5, Chapter 4. This includes naming a Responsible Land Disturber on the Erosion and Sediment Control sheets prior to engaging in land disturbing activities in accordance with Virginia Erosion and Sediment Control Law.
- C-12 All required permits from Virginia Department of Environmental Quality, Environmental Protection Agency, Army Corps of Engineers, Virginia Marine Resources must be in place for all project construction and mitigation work prior to release of the final site plan. This includes the new state requirement for a VPDES permit for all construction activities greater than 1 acre.

#### Virginia American Water Company

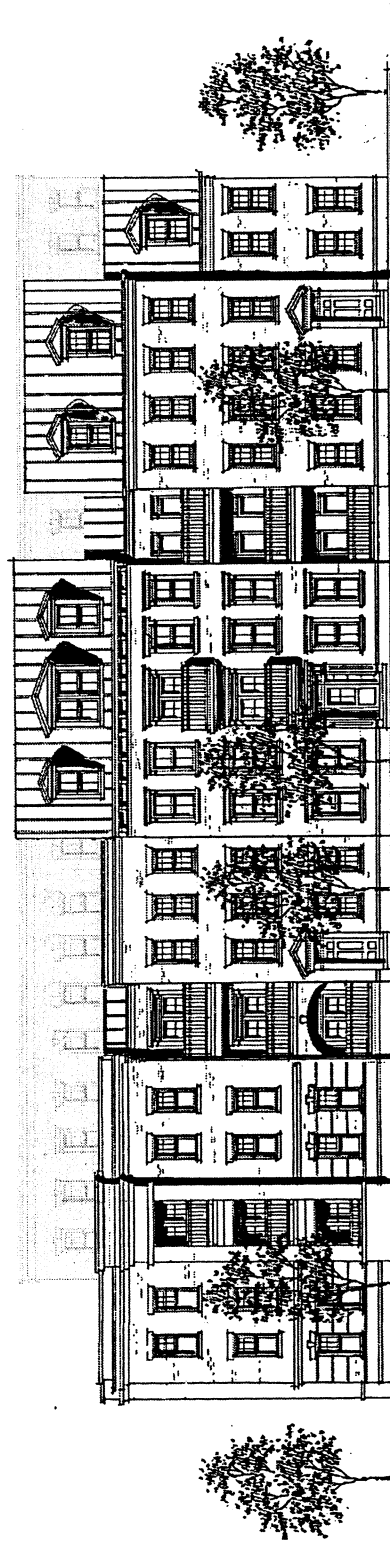
- F-1 Hydraulic calculations will be completed to verify that the required fire flow can be achieved at the proposed fire hydrants upon final submittal of the site plan. Profiles will be required for hydraulic calculations.
- F-2 The existing water main in North Fayette Street, between Cameron and Queen, is six-inch, not eight-inch as currently labeled.
- C-1 A double detector check backflow prevention device is required on all fire services. If located inside the premises, it must have a remote reading meter in a separate accessible room.

#### Archaeology

- F-1 Tax records indicate that a house may have been present on this property by 1810. There were free African American households on this street face in 1830, but the exact addresses are unknown. By the time of the Civil War, the lot had become the site of the Alexandria and Washington Railroad station. A warehouse and service station currently occupy the property. While the more recent construction has probably destroyed most of the surface evidence of 19<sup>th</sup>-Century activities on this lot, it is possible that some more deeply buried archaeological resources may remain intact.
- C-1 Call Alexandria Archaeology immediately (703-838-4399) if any buried structural remains (wall foundations, wells, privies, cisterns, etc.) or concentrations of artifacts are discovered during development. Work must cease in the area of the discovery until a City archaeologist comes to the site and records the finds.
- C-2 The above statement (in C-1) must appear in the General Notes of the site plan so that on-site contractors are aware of the requirement.



Cameron Street Elevation



Henry Street Elevation

**From:** J. Howard Middleton  
Direct Phone: 703-641-4225  
Email: jmiddleton@reedsmith.com

*PC Docket Item #13  
DSUP 2004-0001*

Reed Smith LLP  
3110 Fairview Park Drive  
Suite 1400  
Falls Church, VA 22042-4503  
Phone: 703.641.4200  
Fax: 703.641.4340

**TO:** THE HONORABLE CHAIRMAN AND MEMBERS OF THE PLANNING COMMISSION

**DATE:** September 8, 2004

**RE:** Planning Commission Meeting, September 9, 2004; Docket Item # 13, Development Special Use Permit No. 2004-0001; The Prescott; Proposed Amendments to Conditions

On behalf of the applicant, Cameron Street LLC (CarrHomes, Inc.), I am writing this memorandum to request amendments to the conditions in the staff report for DSUP # 2004-0001. Following the issuance of the staff report, we have met with members of the City staff to discuss changes to the conditions.

A. The following amendments are requested by agreement with the City staff:

2. The final design for the building shall be consistent in massing, articulation, general design and the architectural styles and character as generally depicted in the architectural elevations dated August 23, 2004, and also provide additional refinements to the satisfaction of the Director of P&Z that shall include:

\* \* \* \* \*

b. With the exception of the architectural detailing and screening for the mechanical equipment, the materials of the entire building shall be entirely masonry (brick, precast, stone), except the three townhouses on Fayette as required below.

\* \* \* \* \*

3. ~~The building shall incorporate the use of green building technology and sustainable techniques for building systems design and efficiency to the satisfaction of the Director of P&Z. (P&Z)~~  
The building shall reflect the use of sustainable techniques for building systems design and efficiency



through the use of sustainable materials such as Hardi plank, lower emission paint, energy efficient appliances, mechanical and window systems.

9. The applicant shall provide pedestrian streetscape improvements that at a minimum shall provide the level of improvements depicted on the preliminary plan and shall also provide the following to the satisfaction of the Director of P&Z:

\* \* \* \* \*

h. ~~The stamped asphalt crosswalks at the intersection of Henry and Cameron and Fayette and Cameron shall be eliminated.~~

\* \* \* \* \*

47. Applicant to provide the capital cost to install countdown pedestrian signals and Prisma accessible pedestrian signals at the intersection of Henry and Cameron Streets, not to exceed \$5,000.

53. Note 7 on Sheet 3A states that the “applicant requests a waiver of water quality due to the elimination of discharge into the combined system.” The City will not waive the requirements of Article XIII of the AZO because a) the infrastructure required to make the connection to the separate sanitary sewer is not unreasonable, and b) a written request was not submitted in accordance with Memorandum to Industry No. 2002-0001, dated January 4, 2002. Remove Note #7 from Sheet 3A and references to this note throughout the plans. (T&ES). This condition shall not preclude appropriate credits if deemed proper by the Director of T&ES.

54. Plan must demonstrate to the satisfaction of the Director of T&ES that adequate stormwater outfall is available to the site. Or else If adequate outfall is unavailable, the developer is to design and build any on- or off-site improvements to discharge to an adequate outfall to the satisfaction of the Director of T&ES.

75. The sidewalk along Route 1 shall remain open during construction to the satisfaction of the Director of T&ES.

B. The following amendments are requested by the applicant without City staff agreement:

1. The building at the corner of Henry and Cameron Street shall be revised as generally depicted in Attachment #1 to the satisfaction of the Director of P&Z. The revisions to this portion of the building shall at a minimum include the following:

a. The fourth floor shall provide a setback of approximately ~~15~~ 10 ft. as generally depicted on the sight line study prepared by The Lessard Architectural Group, Inc. and shall not be visible from the adjoining sidewalks. A corner element, along with a vision clearance modification if necessary, may be permitted if approved by the BAR, upon consideration of the recommendation of the Director of P&Z.

b. A portion of the building shall be converted to appear as two townhouses on Henry Street.

c. The height of the northern portion of the building on Henry Street shall be reduced in height to appear as a ~~two~~ three-story element with a roof and dormers. (P&Z)

2. The final design for the building shall be consistent in massing, articulation, general design and the architectural styles and character as generally depicted in the architectural elevations dated August 23, 2004, and also provide additional refinements to the satisfaction of the Director of P&Z that shall include:

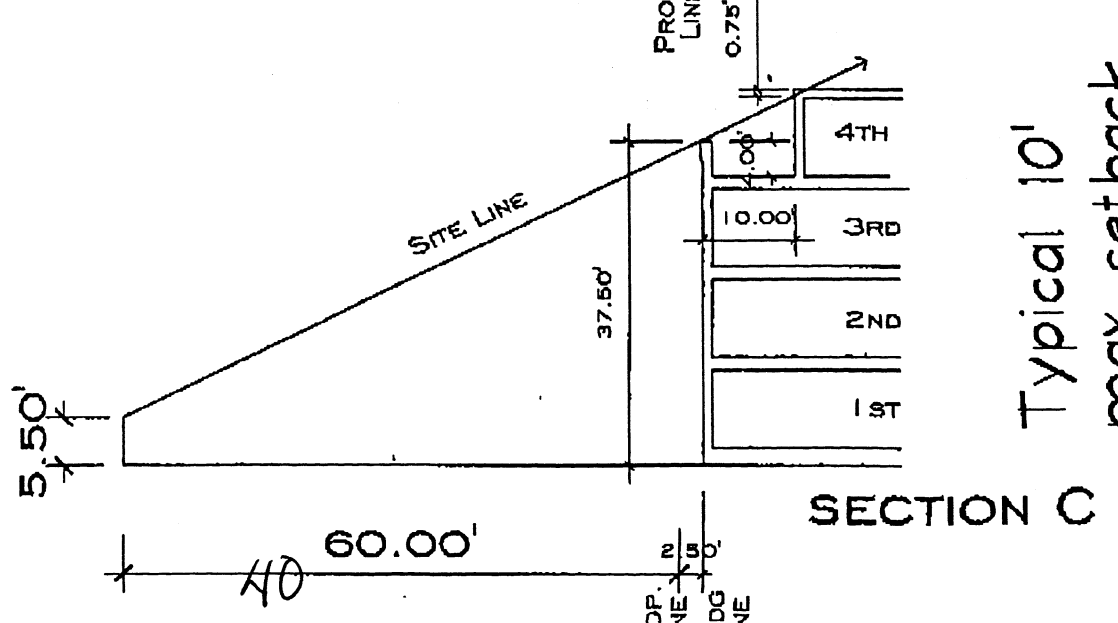
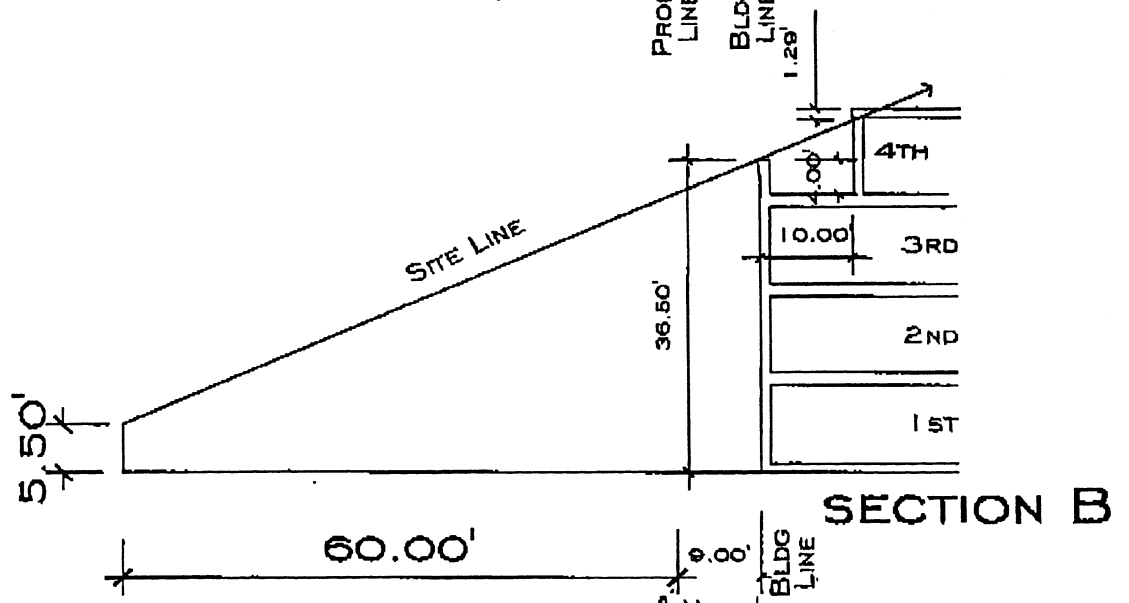
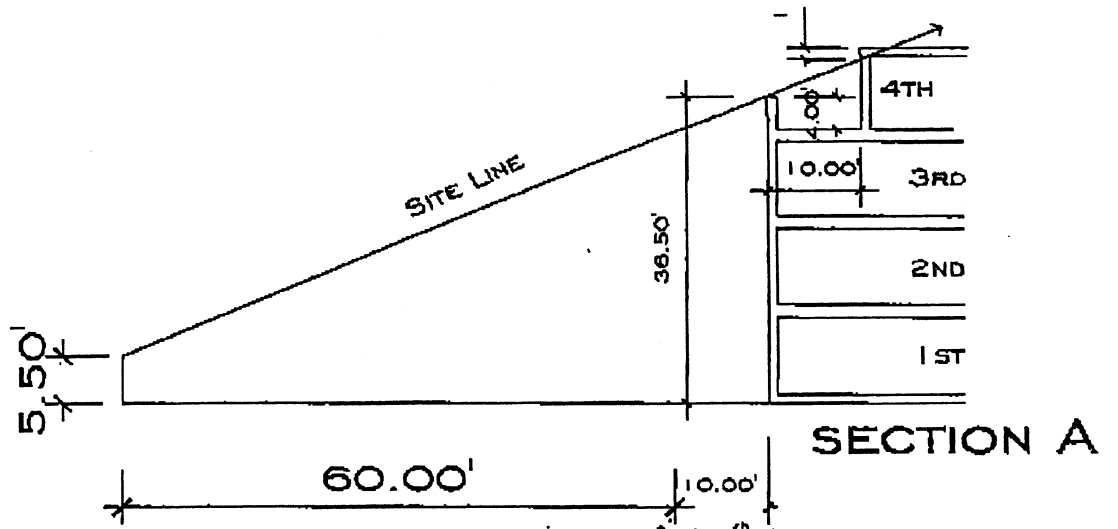
\* \* \* \* \*

Cameron Street

p. The fourth level balconies for the three central townhomes shall be converted to dormers, except that such recommendation shall be subject to the decision of the BAR.

q. The two story balconies for the three central townhomes shall be converted to enclosed balconies consistent with the architectural style of the units, except that such recommendation shall be subject to the decision of the BAR.

\* \* \* \* \*



DP. 2  
VE 15'  
DG 50'  
VE

REVISED: June 28, 2004  
APPLICATION for  
DEVELOPMENT SPECIAL USE PERMIT with SITE PLAN  
DSUP # 2004-0001

PROJECT NAME: The Prescott

PROPERTY LOCATION: 1115 Cameron Street, 200 North Henry Street

TAX MAP REFERENCE: 64.03-04-21 & 21.L1 ZONE: CD Commercial Downtown

APPLICANT Name: Cameron Street LLC c/o CARRHOMES, INC.

Address: 7535 Little River Turnpike, Annandale, VA 22003

PROPERTY OWNER Name: Cameron Street, LLC

Address: 7535 Little River Turnpike, Annandale, VA 22003

SUMMARY OF PROPOSAL: Cameron Street LLC proposes to construct a residential condominium building consisting of 64 condominium units with underground parking including 4 moderate income housing units together with an increase in density in accordance with Sec. 7-700 of the Zoning Ordinance.

MODIFICATIONS REQUESTED: (1) Sideyard setback reduction, Sec. 4-506(A)(2)(b)(2); (2) Compact car parking space increase, Sec. 8-200(E)(1); (3) Increase the pavement in a sideyard to 53%, Sec. 7-1005

SUP's REQUESTED: (1) Increase in number of dwelling units per acre (DUAC) to 54.45, Section 4-506(B)(2) and Sec. 4-505(B)(1); (2) Increase in density (FAR) and (DUAC) and increase number of compact parking spaces and inclusion of moderate income housing units, Section 7-700.

THE UNDERSIGNED hereby applies for Development Site Plan, with Special Use Permit, approval in accordance with the provisions of the Zoning Ordinance of the City of Alexandria, Virginia.

THE UNDERSIGNED, having obtained permission from the property owner, hereby grants permission to the City of Alexandria to post placard notice on the property for which this application is requested, pursuant to Article XI, Section 11-301(B) of the 1992 Zoning Ordinance of the City of Alexandria, Virginia.

THE UNDERSIGNED also attests that all of the information herein provided and specifically including all surveys, drawings, etc., required of the applicant are true, correct and accurate to the best of his knowledge and belief.

J. Howard Middleton, Jr., Reed Smith LLP  
Print Name of Applicant or Agent

  
Signature

3110 Fairview Park Drive, Suite 1400  
Mailing/Street Address

(703) 641-4225 (703) 641-4340  
Telephone # Fax #

Falls Church, VA 22042  
City and State Zip Code

\_\_\_\_\_  
Date

DO NOT WRITE BELOW THIS LINE - OFFICE USE ONLY

Application Received: \_\_\_\_\_  
Fee Paid & Date: \$ \_\_\_\_\_

Received Plans for Completeness: \_\_\_\_\_  
Received Plans for Preliminary: \_\_\_\_\_

ACTION - PLANNING COMMISSION: \_\_\_\_\_

ACTION CITY COUNCIL: \_\_\_\_\_

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Development Special Use Permit with Site Plan (DSUP) # 2004-0001

All applicants must complete this form.

Supplemental forms are required for child care facilities, restaurants, automobile oriented uses and freestanding signs requiring special use permit approval.

1. The applicant is the (check one):

- Owner                       Contract Purchaser  
 Lessee                       Other: Developer

State the name, address and percent of ownership of any person or entity owning an interest in the applicant, unless the entity is a corporation or partnership in which case identify each owner of more than ten percent.

CarrHomes LLC (Thomas E. Jordan, Christopher Rupp)

Thomas E. Jordan

Heather Rupp

Tracy Rupp

If property owner or applicant is being represented by an authorized agent such as an attorney, realtor, or other person for which there is some form of compensation, does this agent or the business in which the agent is employed have a business license to operate in the City of Alexandria, Virginia?

- Yes.      Provide proof of current City business license  
 No.      The agent shall obtain a business license prior to filing application, if required by the City Code.

## NARRATIVE DESCRIPTION

2. The applicant shall describe below the nature of the request in detail so that the Planning Commission and City Counsel can understand the nature of the operation and the use, including such items as the nature of the activity, the number and type of patrons, the number of employees, the hours, how parking is to be provided for employees and patrons, and whether the use will generate any noise. If not appropriate to the request, delete pages 4-7.  
*(Attach additional sheets if necessary)*

Cameron Street LLC proposes to construct a residential condominium consisting of 64 dwelling units to be named The Prescott. The building will be located along Cameron Street between North Henry Street and North Fayette Street with vehicular access from North Fayette Street into an underground garage and a surface parking area for visitors. The development will be located on approximately 1 acre of land and will appear along the street frontage like several buildings, with an articulated architectural design. The primary pedestrian access into the building will be from Cameron Street and from the parking area at the rear of the building midway between North Henry Street and North Fayette Street.

### Project design

The proposed condominium building will be compatible with the neighboring properties. Although the project consists of one building with residential unit access from interior hallways, the design will appear to be several structures. Along North Henry Street, the building will be four stories in height with the height diminishing to three stories between North Henry and North Fayette Street. The fourth level will be set back so that along parts of Cameron Street and North Fayette Street, the building will appear to be three stories in height. The architectural design will appear to be a series of buildings. Along North Fayette Street, which is closer to the residential area in the Parker-Gray District, the articulation of the building will give the appearance of small townhouses which are common within the area to the north and west of the property.

The total number of off-street parking spaces provided will be 124 spaces with 113 of these spaces in an underground parking structure and the remaining 11 spaces on the surface to the rear of the building off North Fayette Street. This total parking exceeds the number of required spaces by 13 spaces, which are available for visitor use.

Along Cameron Street a substantial portion of the building is located on the property line which is common in the area in which the building is located. In fact, a provision in the CD Zone (Sec. 4-566(A)(2)) encourages the front building line to be the same as the front lot line to create an urban environment. Along North Henry Street the building setback is approximately 8.7 feet from the property line, and along North Fayette Street the setback from the property line is approximately 14.5 feet. Each of the setbacks conforms to the expectation of front yard setbacks in this area. The sideyard setback, that is the setback from the rear of the building nearest North Henry Street to the property line to the north, is 14.58 feet. This is less than the required 25 feet and therefore a modification is requested from the strict application of the zoning ordinance. This property line abuts commercial/industrial use and therefore will not have a negative impact upon a neighboring property.

The additional modifications are requested for the number of compact car parking spaces and the use of a portion of the side yard for surface parking. We are providing 113 underground parking spaces and 7 surface spaces, 8.5 ft. X 18 ft. in size, smaller than standard but larger than compact size. This is the type of parking space, known as "universal spaces," that are commonly accepted. Also, the plan reflects using 53% of the side yard for surface parking, a slight increase over the allowable 50%, which will have no noticeable effect.

#### Special Use Permits:

The Applicant is requesting special use permits for the proposed development. First, in accordance with Section 4-506(B)(2) and 4-505(B)(1) of the Zoning Ordinance the number of

dwelling units per acre may rise to 54.45 with a special use permit with an accompanying reduction of the land area per dwelling unit. The Applicant is requesting a special use permit to increase the number of units in accordance with these provisions. In addition, the Applicant proposes to provide on-site moderate income housing units and, in accordance with Section 7-700 of the Zoning Ordinance, requests an increase in density as a result of providing the affordable housing. It was agreed in discussions with officials in the City's Office of Housing that the Applicant will provide 1/3 of bonus density units for affordable dwelling units, and, in return, there will be an increase in density with floor area ratio up to 1.5, the number of dwelling units up to 64, which results in one dwelling unit per 681 square feet of land area or 64 DUAC, and an increase in the number of compact parking spaces. At its meeting on June 3, 2004, the Alexandria Affordable Housing Advisory Committee approved the staff recommendation in conformity with the affordable housing policy.

The Prescott will be located in an area near the King Street Metro Station as well as the Upper King Street commercial corridor. It is ideally located for residential condominiums and for moderate income housing. Transportation and shopping needs are within walking distance, providing a convenient location for urban living.

Along with this Application, CarrHomes, Inc. is also submitting an application for review by the Parker-Gray District Board of Architectural Review. The site is on the southern edge of the Parker-Gray District, and is clearly in a transition area from the commercial uses along King Street and toward the residential uses further to the north and west. In addition, it is across the street from the Tycon building, a 7-story building extending from King Street to Cameron Street between North Henry and North Fayette streets. The Prescott will represent a transitional architectural design and density from the King Street area to the north into the Parker-Gray District.



Development Special Use Permit with Site Plan (DSUP) #2004-0001

3. How many patrons, clients, pupils and other such users do you expect?  
Specify time period (i.e., day, hour or shift).

Residential Use – 4 one-bedroom units; 60 two-bedroom units

4. How many employees, staff and other personnel do you expect?  
Specify time period (i.e., day, hour or shift).

Not Applicable.

5. Describe the proposed hours and days of operation of the proposed use:

Day	Hours	Day	Hours
<u>Residential Use</u>			

6. Describe any potential noise emanating from the proposed use:

A. Describe the noise levels anticipated from all mechanical equipment and patrons.  
Ordinary heating and air conditioning equipment for residential use

B. How will the noise from patrons be controlled?  
Not Applicable

7. Describe any potential odors emanating from the proposed use and plans to control them:

Not Applicable

Development Special Use Permit with Site Plan (DSUP) #2004-0001

8. Provide information regarding trash and litter generated by the use:

A. What type of trash and garbage will be generated by the use?

Household trash and garbage

B. How much trash and garbage will be generated by the use?

Normal trash and garbage from 64 condominium units

C. How often will trash be collected?

City Collection

D. How will you prevent littering on the property, streets and nearby properties?

Not Applicable

9. Will any hazardous materials, as defined by the state or federal government, be handled, stored, or generated on the property?

Yes.  No.

If yes, provide the name, monthly quantity, and specific disposal method below:

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10. Will any organic compounds, for example paint, ink, lacquer thinner, or cleaning or degreasing solvent, be handled, stored or generated on the property?

Yes.  No.

If yes, provide the name, monthly quantity, and specific disposal method below:

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11. What methods are proposed to ensure the safety of residents, employees and patrons?

City police protection available  
\_\_\_\_\_  
\_\_\_\_\_

**ALCOHOL SALES**

12. Will the proposed use include the sale of beer, wine, or mixed drinks?

Yes.       No.

If yes, describe alcohol sales below, including if the ABC license will include on-premises and/or off-premises sales. Existing uses much describe their existing alcohol sales and/or service and identify any proposed changes in that aspect of the operation.

\_\_\_\_\_  
\_\_\_\_\_  
\_\_\_\_\_

**PARKING AND ACCESS REQUIREMENTS**

13. Provide information regarding the availability of off-street parking:

A. How many parking spaces are required for the proposed use pursuant to section 8-200(A) of the zoning ordinance?

111 parking spaces

B. How many parking spaces of each type are provided for the proposed use:

4 Standard spaces

120 Compact spaces (universal spaces, 8.5 ft. X 18 ft.)

3 Handicapped accessible spaces(included within the total Standard)

13 Other – Visitor(included within the 124 total number of spaces)

Development Special Use Permit with Site Plan (DSUP) # 2004-0001

- C. Where is the required parking located? (check one)  on-site  off-site.

If the required parking will be located off-site, where will it be located:

\_\_\_\_\_

Pursuant to section 8-200(C) of the zoning ordinance, commercial and industrial uses may provide off-site parking within 500 feet of the proposed use, provided that the off-site parking is located on land zoned for commercial or industrial uses. All other uses must provide parking on-site, except that off-street parking may be provided within 300 feet of the use with a special use permit.

- D. If a reduction in the required parking is requested, pursuant to section 8-100(A)(4) or (5) of the zoning ordinance, complete the PARKING REDUCTION SUPPLEMENTAL APPLICATION.

14. Provide information regarding loading and unloading facilities for the use:

- A. How many loading spaces are required for the use, per section 8-200 (B) of the

zoning ordinance? Not Applicable

- B. How many loading spaces are available for the use? 1, with trash pickup

- C. Where are off-street loading facilities located? \_\_\_\_\_

- D. During what hours of the day do you expect loading/unloading operations to occur?

Normal trash collection

- E. How frequently are loading/unloading operations expected to occur, per day or per week, as appropriate?

15. Is street access to the subject property adequate or are any street improvements, such as a new turning lane, necessary to minimize impacts on traffic flow?

Across from North Fayette Street into the site is adequate.

Eileen Fogarty, Director of Planning and Zoning  
301 King Street  
Room 2100, Box 178  
Alexandria, VA 22313

12 August 2004

Dear Ms. Fogarty:

As residents in the Parker-Gray Historic District, we are writing to formally express our strong opposition to the proposed development at 1115 Cameron Street. The proposal envisions construction of 64 apartments at the intersection of Cameron and Fayette Streets. This represents a radical departure from the current residential density of the neighborhood.

The proposal would in one fell swoop, increase the population of our neighborhood by a factor of several hundred percent. It would place a multi-family dwelling in the midst of what is now an area exclusively made up of single family homes. The proposal therefore abrogates the guiding tenets inherent in the Parker-Gray Historic District. Equally important, it represents a gross violation of OBJECTIVE #1 of GOAL #1 of the *City of Alexandria 2004-2015 Strategic Plan*, which states:

GOAL 1: Quality development and redevelopment that is well planned and consistent with Alexandria's vision

OBJECTIVE 1: ensure that new development, redevelopment, and infill development are compatible with the character and scale of Alexandria neighborhoods and the natural environment

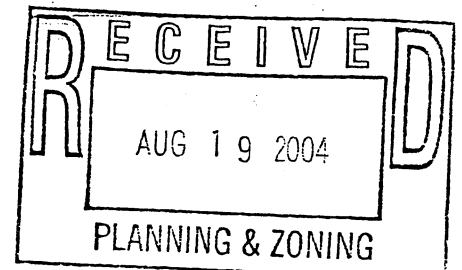
As the proposal is wholly and completely incongruent with both the character and the scale of our neighborhood, we strongly oppose it.

Finally, with respect to parking, we are well aware that based on 6 one bedroom units and 58 two bedroom units, the *Alexandria Zoning Ordinance* requires the project include 109.3 parking spaces for the dwelling. We vehemently oppose any effort that seeks to waive all or any part of that requirement.

In summary, we undersigned residents of the Parker-Gray Historic District petition the City of Alexandria to disapprove this project on the grounds that it clearly violates the guiding tenets of the Historic District, and represents a gross violation of the City's own Strategic Plan. The only people who will accrue anything positive from the project will be the profiteer developers, who will reap their bounty and leave, having no lasting stake in the neighborhood. Conversely, we residents will be left with the irrevocable and permanent harm done to the nature of our community and by extension, the quality of our lives. Please do the right thing and stop this project.

David K. Kofal 1219 Queen St. Tom Jor 1209 Queen St  
Rick J. Jhon 1225 Queen St. Wilson Thompson 1201 Queen  
Paul M. Lee 1223 Queen St. Jerry Murray 1125 Queen St  
Julie Perry 1221 Queen St  
Harold Ferguson 1207 Queen St  
Jason Harris 1303 Queen St  
Nathaniel T. T. 1213 Queen St

50





"Katy Cannady"  
<kcannady@erols.com>  
09/04/2004 10:14 PM

To "Eileen Fogarty" <eileen.fogarty@ci.alexandria.va.us>  
cc  
Subject Item 13 on the September 9th Planning Commission  
Docket

*DC Docket Item #13* September 4, 2004  
*DSUP2004-0001*

Dear Ms. Fogarty,

I will be away from Alexandria next week when the Planning Commission meets. Therefore, I request that you convey the following statement regarding Item 13 to the Chairman and Members of the Planning Commission.

Shortly before it adjourned for the summer City Council enacted an open space plan for the city. The plan focused a lot of attention, as it should have been, on providing public open space in neighborhoods which lack it now. I am pessimistic that the plan and the open space fund will truly provide more open space. A will to do all that it would take to provide any meaningful amount of new open space has not been demonstrated so far. I could be wrong in my assessment. I would be delighted if that were so.

I have read the staff report and attended meetings of the Inner City Civic Association where the proposed Prescott Condominium building on Cameron Street was discussed. When proposals such as this one come forward, they reinforce all my fears about our future. This proposal is for a three to four story building of a large mass and scale to be set down where a two story warehouse and a service station existed before. It provides no open space for the public, not even the kind of visual open space provided by the average front yard. There is very little open space for the future residents and some of that is on the roof. The alterations suggested by the staff report fall far short of correcting its glaring defects.

It is especially unfortunate to have this proposal for a part of town that has so little open space now. The remaining warehouse buildings do nothing for the street scapes. Buildings such as this one, with no meaningful set backs, will not improve the ambience in any way. The special use permit requested would allow a significant increase in density over a by right use yet there is no significant contribution to the good of the city or the neighborhood.

I know the applicant will argue that the four "affordable housing units" are a benefit. It would be more correct to think of these units as "middle income ownership opportunities." The applicant is well compensated for these units by being allowed to build six additional market rate units.

To be an acceptable addition to the city's housing stock, this building would have to be reconfigured to provide actual on site open space available for public use. At a minimum, it needs a new footprint set back from the street sufficiently to provide a feeling of openness in an area where streets are generally narrow. If this building goes up according to the plan presented to you, it will function as the urban equivalent of a medieval fortress guarding its small interior courtyard from public view. Even worse, it appears to only the first of numerous similar redevelopment projects slated for this area. The city cannot permit buildings such as this one and then at some point in the future look around for public open space.

Starting with this proposal, every would be developer in this small area plan should be required to provide actual land for open space, the best alternative, or contribute a significant amount of money for the purchase of public open space within the small area plan area.

The \$10,000 contribution to renovate an existing public park would in no way compensate the city for the all the opportunities that will be lost with this kind of development. The words "chump change" come to mind when I even think about that idea.

Please defer or deny this proposal. This site is near the Metro. Something much more worthy could be built here. It only requires an applicant with a vision of something more than his bottom line.

Katy Cannady



Frank McNelis  
<fpmcnel@yahoo.com>

09/08/2004 01:18 PM

To Donna Fossum <fossum@rand.org>, Jesse Jennings  
<jsjennings@aol.com>, John Komoroske  
<komoros@nasd.com>, Larry Robinson  
<robinsonjl@aol.com>

cc Eileen Fogarty <eileen.fogarty@alexandriava.gov>  
Subject Input on DSUP #2004-0001 - The Prescott

*PC Docket Item # 13  
DSUP 2004-0001*

To: Planning Commission Members:

As a concerned resident of the Parker Gray district, I want to communicate my input to the Commission regarding DSUP #2004-0001 - The Prescott (Hopkins project).

I support the development of The Prescott, including the Planning Commission Staff recommendations, except for the following items below;

1. I object to the use of Fayette Street for ingress and egress to the Prescott, for the following reasons:
  - a. The volume of traffic on Fayette Street has been able to be managed, over time, through such measures as the prohibition of southbound traffic during the afternoon rush hour and the limitation on truck traffic. An ingress/egress on Fayette Street would create a significant, unwanted, additional traffic load.
  - b. The original warehouse construction favors the use of Cameron Street for ingress/egress. To change this would run counter to the historical nature of the construction in this area. There were likely good reasons for the use of Cameron over Fayette.
  - c. The existence of a playground across the street from the development would pose additional safety issues as a result of the additional traffic load.
2. Although I fully support the Staff recommendation that Prescott residents should be ineligible to apply for or receive residential parking permits, I believe that this will be ineffective unless there is an increased community police presence in the area. Enforcement is critical here.
3. The existing restriction on southbound traffic on Fayette from the Oronoco Street intersection during the late afternoon time period needs to be strictly enforced. In addition, this restriction should be communicated clearly to all new Prescott residents by Prescott management.

Note to Eileen Fogarty: Would you be so kind as to forward this note to Mr. Leibach and Mr. Dunn, as I don't have their email addresses? Thanks.

Sincerely,

Frank P. McNelis  
1124 Princess Street

*Hopkins File*

**KSMET**  
**THE KING STREET METRO ENTERPRISE TEAM**  
115 SOUTH PAYNE STREET ALEXANDRIA, VA 22314  
703-549-4696

*PC Docket Item #13  
D861P 2004-0201*

September 9, 2004

Ms. Eileen Fogarty  
Planning and Zoning  
Room 2100  
City Hall  
301 King Street  
Alexandria, VA 22314

Dear Eileen:

I sent a letter to Eric Wagner, Chair of the Planning Commission, on behalf of the Board of Directors of the King Street Metro Enterprise Team (KSMET) in support of the proposed CarrHomes's project at North Henry, Cameron, and Fayette Streets after CarrHomes made their presentation to our membership. I am following up to restate that support since some time has elapsed over the summer months.

The membership of the KSMET Board of Directors had an opportunity to study the proposed plans and ask questions about any issues associated with the proposal. At the end of the discussion, the membership of the Board voted unanimously to support the proposed development.

Our commitment to supporting Second Thursday in Old Town precludes my being at the Planning Commission meeting tonight so I am sending this letter in support of the KSMET position instead.

Thank you in advance for your assistance.

Sincerely,

*Lois*  
Lois Walker  
President





## MEMORANDUM

**TO:** David Sundland City of Alexandria

**CC:** Tara Craven Carrhomes

**FROM:** Chad Baird Gorove/Slade Associates, Inc.  
Caleb Smith C.E.T. Gorove/Slade Associates, Inc.  
Christopher Tacinelli, P.E. Gorove/Slade Associates, Inc.

**DATE:** August 18, 2004

**SUBJECT:** On-Street Parking Analysis – The Prescott  
Alexandria, Virginia

### Introduction

The purpose of this memorandum is to review the on street parking within the vicinity of the proposed Prescott development located in the city of Alexandria, Virginia. The site is located along the north side of Cameron Street between N. Henry Street and N. Fayette Street. This document outlines the available on street parking within a one-block radius of the proposed development.

### Analysis

There are five basic areas of concern associated with on-street parking availability within the vicinity of the site. The following areas of concern were evaluated to show the availability of parking on the adjoining streets and how this parking will accommodate future visitor parking for the proposed development:

- Show an inventory of all on-street parking spaces within a one-block radius of the proposed site.

A comprehensive inventory of available parking spaces within the one-block area is shown on **Figure 1** and is attached to the back of this memorandum.

- Show an inventory of the available on-street parking.

The on-street parking supply within a one-block radius of the site was collected for four consecutive days from a Thursday to a Sunday. The hours of the survey are listed by date below:

- |                                      |                  |
|--------------------------------------|------------------|
| • Thursday, March 11 & April 1, 2004 | 4:00pm – 8:00pm  |
| • Friday, March 12 & April 2, 2004   | 4:00pm – 8:00pm  |
| • Saturday, March 13 & April 3, 2004 | 11:00am - 8:00pm |
| • Sunday, March 14 & April 4, 2004   | 11:00am – 3:00pm |

MEMORANDUM

On-Street Parking Analysis – The Prescott

August 18, 2004

Page 2

Tables 1 – 7 display the available parking inventory by restriction for each of the dates and times collected. All tables are attached to the back of this memorandum.

- Display all parking time availability and parking limitations within a one-block radius of the site.

The times available for on-street parking as well as the parking restrictions within a one-block area of the proposed development are shown on **Figure 1** and is attached the back of this memorandum.

- Show all residential, commercial, and church uses within a one-block radius of the propose site.

The residential, commercial, and church uses within a one-block radius of the site are shown on **Figure 2** and is attached the back of this memorandum.

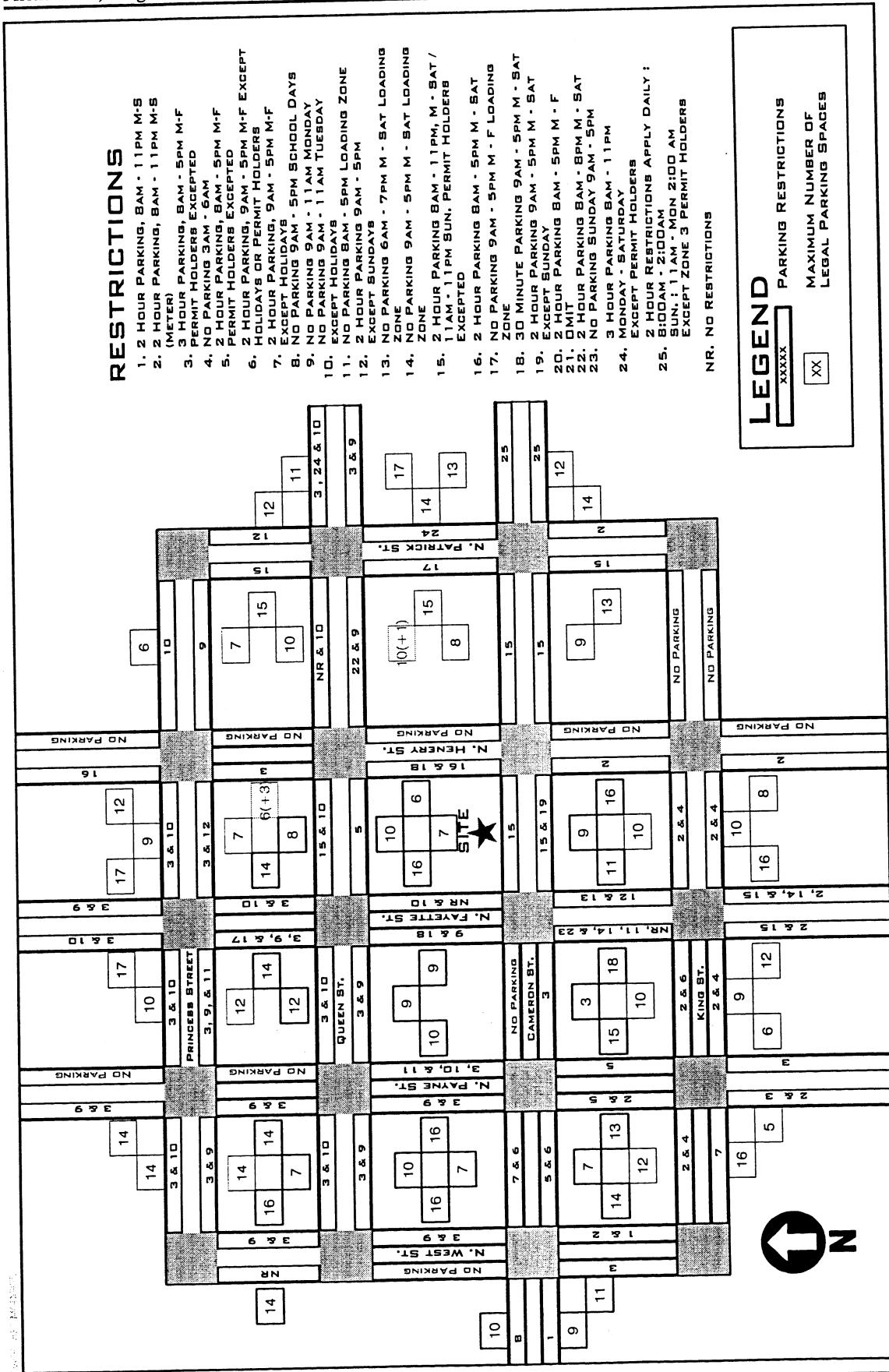
- Display circulation patterns for all streets within a one block radius of the proposed site

The roadway circulation pattern within a one-block radius of the site is shown on **Figure 3** and is attached the back of this memorandum.

**Conclusions**

This analysis has evaluated the availability of parking surrounding the Prescott site. The analysis focused on conditions during the evening hours of Thursday, March 11, & April 1, 2004, Friday, March 12, & April 2, 2004, all day Saturday, March 13, & April 3, 2004, and mid-afternoon Sunday, March 14, & April 4, 2004. The following summarizes the conclusions of our study.

1. As the analysis shows, at all times over four days (2 weekday, 2 weekend) there is significant available parking for the Prescott site.
2. The attached tables 5, 6, & 7 provide a summary of the available parking spaces within a one-block radius of the site.
3. Thursday has an average of 85 available on street parking spaces (41% available)
4. Friday has an average of 59 available on street parking spaces (29% available)
5. Saturday has an average of 66 available on street parking spaces (32% available)
6. Sunday has an average of 58 available on street parking spaces (28% available)



**RESTRICTIONS**

1. 2 HOUR PARKING, 8AM - 11PM M-S (METER)
2. 2 HOUR PARKING, 8AM - 11PM M-S
3. 3 HOUR PARKING, 8AM - 5PM M-F PERMIT HOLDERS EXCEPTED
4. NO PARKING 3AM - 6AM
5. 2 HOUR PARKING, 8AM - 5PM M-F PERMIT HOLDERS EXCEPTED
6. 2 HOUR PARKING, 9AM - 5PM M-F EXCEPT HOLIDAYS OR PERMIT HOLDERS
7. 2 HOUR PARKING, 9AM - 5PM M-F EXCEPT HOLIDAYS
8. NO PARKING 9AM - 11AM MONDAY
9. NO PARKING 9AM - 11AM TUESDAY EXCEPT HOLIDAYS
10. NO PARKING 8AM - 5PM LOADING ZONE
11. EXCEPT SUNDAYS
12. NO PARKING 6AM - 7PM M - SAT LOADING ZONE
13. NO PARKING 9AM - 5PM M - SAT LOADING ZONE
14. 2 HOUR PARKING 8AM - 11PM, M - SAT / 11AM - 1PM SUN. PERMIT HOLDERS EXCEPTED
15. 2 HOUR PARKING 8AM - 5PM M - SAT
16. 30 MINUTE PARKING 9AM - 5PM M - SAT
17. 2 HOUR PARKING 9AM - 5PM M - SAT EXCEPT SUNDAY
18. 2 HOUR PARKING 8AM - 5PM M - F OMIT
19. 2 HOUR PARKING 8AM - 5PM M - SAT
20. 3 HOUR PARKING 8AM - 11PM MONDAY - SATURDAY
21. EXCEPT PERMIT HOLDERS
22. 2 HOUR RESTRICTIONS APPLY DAILY: 8:00AM - 2:00AM SUN.; 11AM - MON 2:00 AM
23. EXCEPT ZONE 3 PERMIT HOLDERS
24. NO RESTRICTIONS

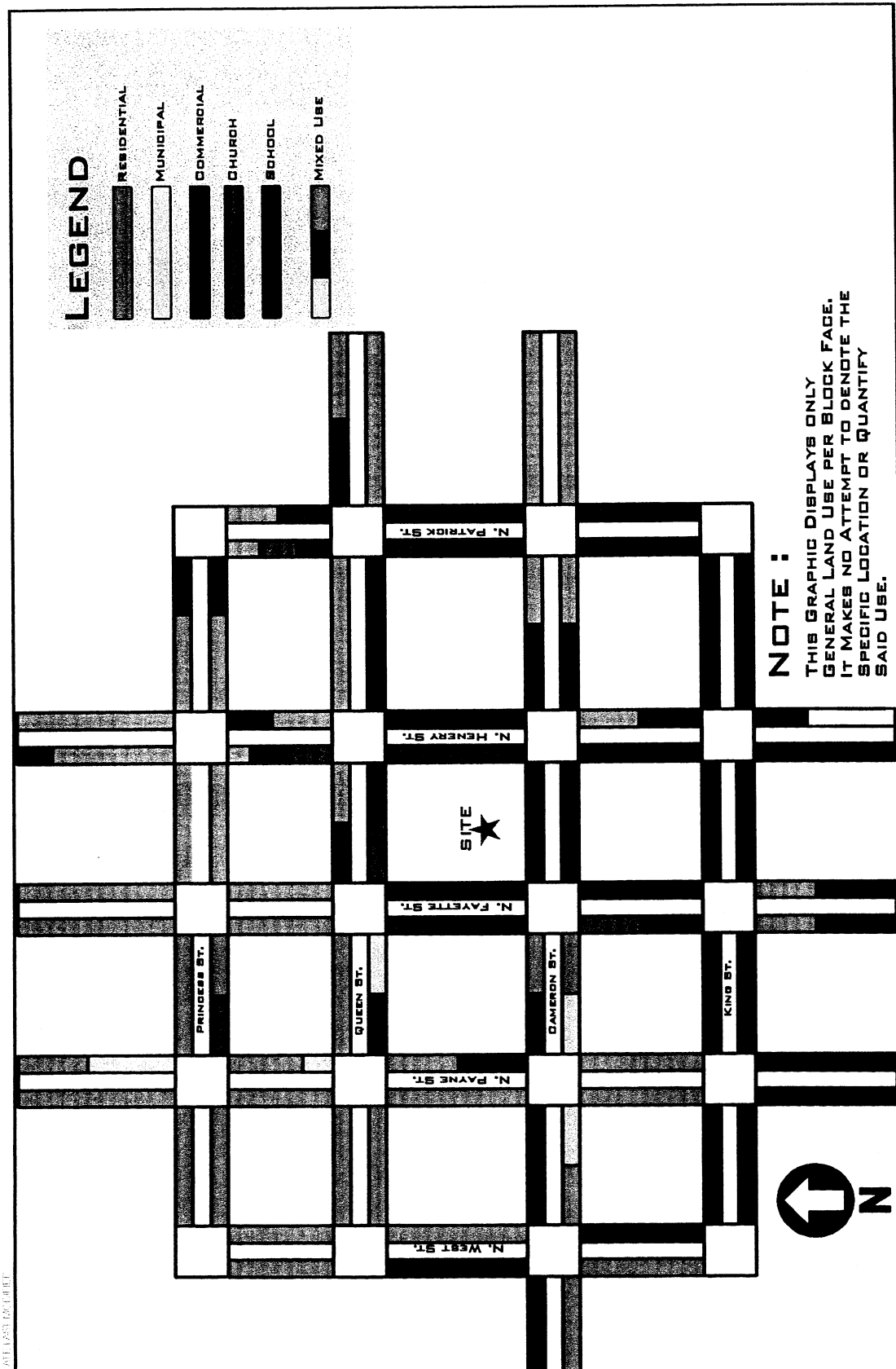
**LEGEND**

XXXXX PARKING RESTRICTIONS

XX MAXIMUM NUMBER OF LEGAL PARKING SPACES

Figure 1  
On-Street Parking Restrictions Inventory

56



10/18/04 10:00 AM

View Number

57



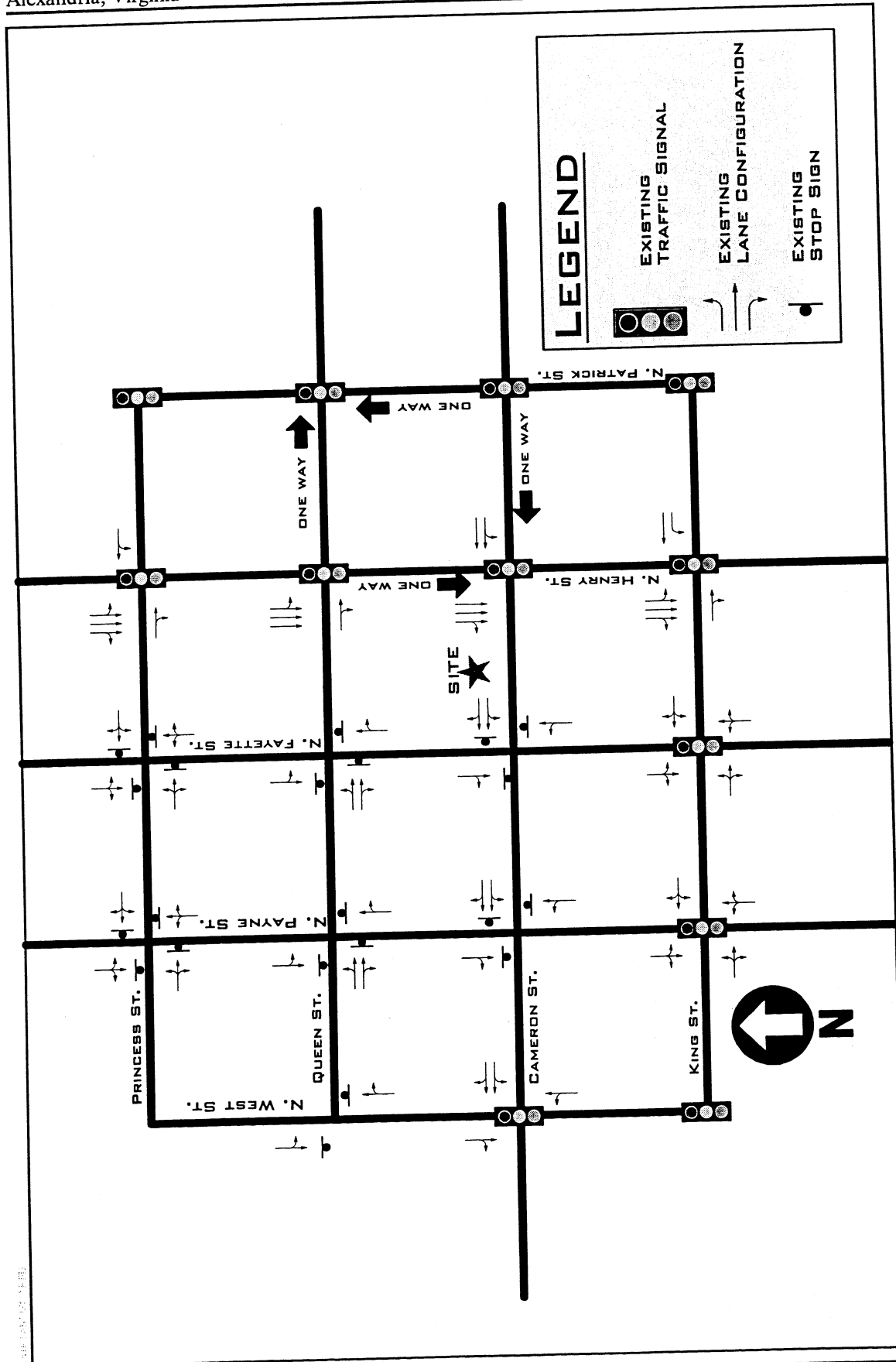


Figure 3  
Existing Traffic Controls & Circulation Patterns

**TABLE 1**

QUEEN STREET	PARKING RESTRICTION	TOTAL SPACES	PARKING OCCUPANCY										
			11:00 AM	12:00 PM	1:00 PM	2:00 PM	3:00 PM	4:00 PM	5:00 PM	6:00 PM	7:00 PM	8:00 PM	
3 HOUR PARKING BAN - 5PM M-F PERMIT HOLDERS EXCEPTED	21												
2 HOUR PARKING BAN - 5PM M - F PERMIT HOLDERS EXCEPTED	10												
2 HOUR PARKING BAN - 5PM M - SAT / 11 AM - 11 PM SUN PERMIT HOLDERS EXCEPTED & NO PARKING 9AM-11 AM TUESDAYS (EXCEPT HOLIDAYS)	10												
NO RESTRICTION EXCEPT -	10		46	48	48	53	53	51	51	39	50	48	
2 HOUR PARKING BAN - 8PM M - SAT & NO PARKING 9AM-11 AM MONDAYS (EXCEPT HOLIDAYS)	10		50	52	56	54	28						
<b>MAXIMUM # OF AVAILABLE SPACES</b>	<b>61</b>												

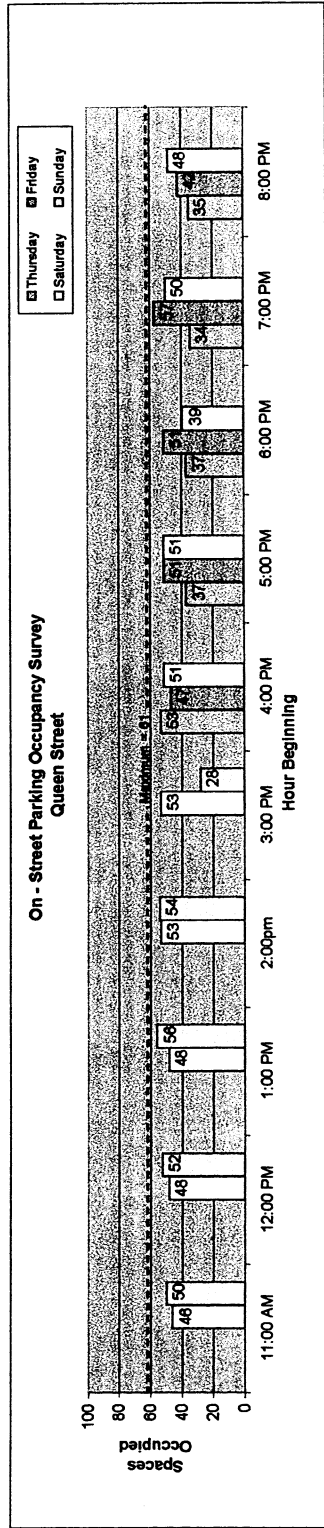
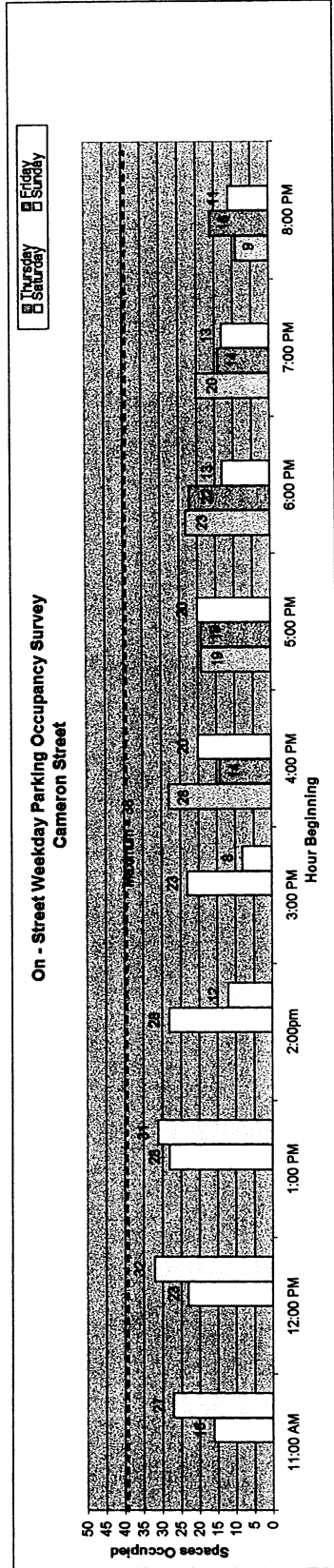


TABLE 2

CAMERON STREET		PARKING OCCUPANCY										
PARKING RESTRICTION		TOTAL SPACES										
2 HOUR PARKING 8AM - 11PM M - SAT 11AM-11PM SUNDAY	12	THURSDAY	11:00 AM	12:00 PM	1:00 PM	2:00PM	3:00 PM	4:00 PM	5:00 PM	6:00 PM	7:00 PM	8:00 PM
3 HOUR PARKING 8AM - 5PM M-F PERMIT HOLDERS EXCEPTED	3	FRIDAY	28	19	23	20	9					
2 HOUR PARKING 8AM - 5PM M - F PERMIT HOLDERS EXCEPTED	3	SATURDAY	16	23	28	28	23	14	19	22	14	16
2 HOUR PARKING 9AM - 5PM M - SAT EXCEPT SUNDAY	6	SUNDAY	27	32	31	12	8	20	13	13	13	11
2 HOUR PARKING 8AM - 11PM M - SAT 11AM-11PM SUNDAY PERMIT HOLDER EXCEPTED	7											
2 HOUR PARKING 8AM - 8PM M - SAT	8											
2 HOUR RESTRICTIONS APPLY DAILY : 8:00AM - 2AM SUNDAY 11AM - MON 2AM. ZONE 3 PERMIT HOLDERS EXCEPTED												
MAXIMUM # OF AVAILABLE SPACES	39											

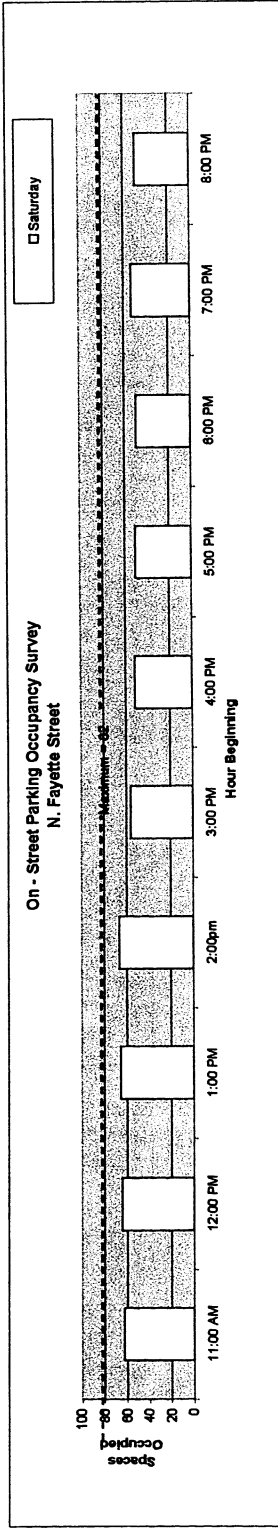
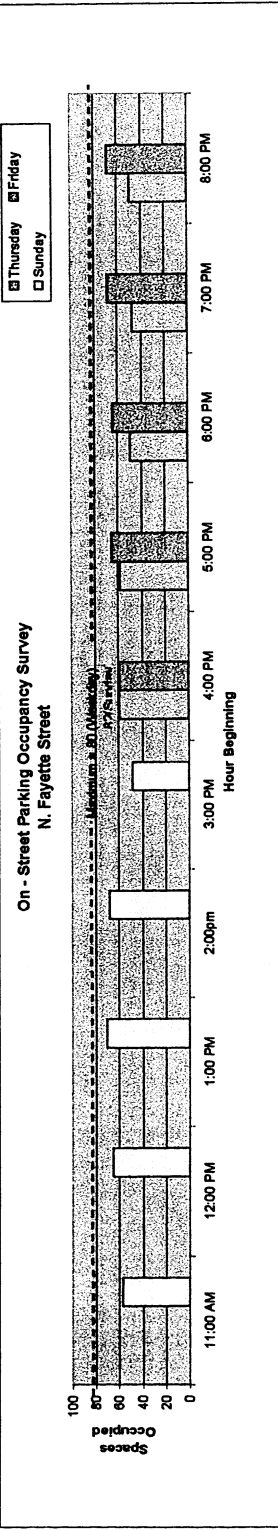


60

**N. FAYETTE STREET**

**TABLE 3**

PARKING RESTRICTION	TOTAL SPACES	PARKING OCCUPANCY												
		11:00 AM	12:00 PM	1:00 PM	2:00 PM	3:00 PM	4:00 PM	5:00 PM	6:00 PM	7:00 PM	8:00 PM			
3 HOUR PARKING 8AM - 5PM M-F PERMIT HOLDERS EXCEPTED	27													
NO PARKING 8AM - 5PM LOADING ZONE	1													
2 HOUR PARKING 9AM - 5PM EXCEPT SUNDAYS	9													
NO PARKING 6AM - 7PM M - SAT LOADING ZONE	2													
NO PARKING 9AM - 5PM M - SAT LOADING ZONE	2													
2 HOUR PARKING 8AM - 5PM M - SAT / 11AM - 11PM SUN PERMIT HOLDERS EXCEPTED														
NO PARKING 9AM - 5PM M - F LOADING ZONE	1													
30 MINUTE PARKING 9AM - 5PM M - SAT & NO PARKING 9AM-11AM MONDAYS (EXCEPT HOLIDAYS)	9													
NO PARKING SUNDAY 9AM - 5PM	2													
NO RESTRICTION														
NO RESTRICTION EXCEPT -	29													
NO PARKING 9AM-11AM TUESDAYS (EXCEPT HOLIDAYS)	82													
<b>MAXIMUM # OF AVAILABLE SPACES</b>														

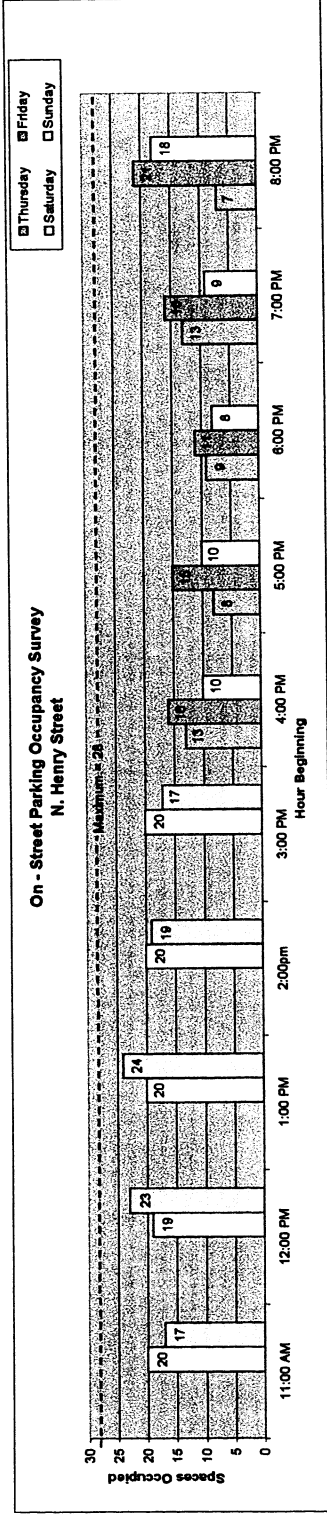


61



TABLE 4

PARKING RESTRICTION	TOTAL SPACES	PARKING OCCUPANDY													
		11:00 AM	12:00 PM	1:00 PM	2:00 PM	3:00 PM	4:00 PM	5:00 PM	6:00 PM	7:00 PM	8:00 PM	9:00 PM			
2 HOUR PARKING BAN - 11 PM M - SAT (METER)	16														
3 HOUR PARKING BAN - 5 PM M-F PERMIT HOLDERS EXCEPTED	6														
2 HOUR PARKING BAN - 5 PM M - SAT	2														
30 MINUTE PARKING BAN - 5 PM M - SAT	4														
<b>MAXIMUM # OF AVAILABLE SPACES</b>	<b>28</b>	20	19	20	20	20	19	24	19	17					
		THURSDAY	FRIDAY	SATURDAY	SUNDAY										



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TABLE 5  
PARKING OCCUPANCY SUMMARY

	11:00 AM		12:00 PM		1:00 PM		2:00PM		3:00 PM	
	SPACES OCCUPIED	TOTAL # SPACES	SPACES OCCUPIED	TOTAL # SPACES	SPACES OCCUPIED	TOTAL # SPACES	SPACES OCCUPIED	TOTAL # SPACES	SPACES OCCUPIED	TOTAL # SPACES
SATURDAY, 3/13 & 4/3, 2004	144	206	154	206	161	206	167	206	152	206
SUNDAY, 3/14 & 4/4, 2004	151	210	172	210	181	210	153	210	101	210
	4:00 PM		5:00 PM		6:00 PM		7:00 PM		8:00 PM	
	OCCUPIED	TOTAL	OCCUPIED	TOTAL	OCCUPIED	TOTAL	OCCUPIED	TOTAL	OCCUPIED	TOTAL
THURSDAY, 3/11 & 4/1, 2004	153	206	122	206	118	206	114	206	100	206
FRIDAY, 3/12 & 4/2, 2004	136	206	150	206	148	206	155	206	147	206
SATURDAY, 3/13 & 4/3, 2004	132	206	131	206	109	206	125	206	126	206

TABLE 6  
PARKING OCCUPANCY SUMMARY

	11:00 AM		12:00 PM		1:00 PM		2:00PM		3:00 PM	
	OCCUPIED	AVAILABLE	OCCUPIED	AVAILABLE	OCCUPIED	AVAILABLE	OCCUPIED	AVAILABLE	OCCUPIED	AVAILABLE
SATURDAY, 3/13 & 4/3, 2004	144	62	154	52	161	45	167	39	152	54
SUNDAY, 3/14 & 4/4, 2004	151	59	172	38	181	29	153	57	101	109
	4:00 PM		5:00 PM		6:00 PM		7:00 PM		8:00 PM	
	OCCUPIED	AVAILABLE	OCCUPIED	AVAILABLE	OCCUPIED	AVAILABLE	OCCUPIED	AVAILABLE	OCCUPIED	AVAILABLE
THURSDAY, 3/11 & 4/1, 2004	153	53	122	84	118	88	114	92	100	106
FRIDAY, 3/12 & 4/2, 2004	136	70	150	56	148	58	155	51	147	59
SATURDAY, 3/13 & 4/3, 2004	132	74	131	75	109	97	125	81	126	80

TABLE 7  
PARKING OCCUPANCY SUMMARY

	11:00 AM		12:00 PM		1:00 PM		2:00PM		3:00 PM	
	% OCCUPIED	% AVAILABLE	% OCCUPIED	% AVAILABLE	% OCCUPIED	% AVAILABLE	% OCCUPIED	% AVAILABLE	% OCCUPIED	% AVAILABLE
SATURDAY, 3/13 & 4/3, 2004	69.9%	30.1%	74.8%	25.2%	78.2%	21.8%	81.1%	18.9%	73.8%	26.2%
SUNDAY, 3/14 & 4/4, 2004	71.9%	28.1%	81.9%	18.1%	86.2%	13.8%	72.9%	27.1%	48.1%	51.9%
	4:00 PM		5:00 PM		6:00 PM		7:00 PM		8:00 PM	
	OCCUPIED	AVAILABLE	OCCUPIED	AVAILABLE	OCCUPIED	AVAILABLE	OCCUPIED	AVAILABLE	OCCUPIED	AVAILABLE
THURSDAY, 3/11 & 4/1, 2004	74.3%	25.7%	59.2%	40.8%	57.3%	42.7%	55.3%	44.7%	48.5%	51.5%
FRIDAY, 3/12 & 4/2, 2004	66.0%	34.0%	72.8%	27.2%	71.8%	28.2%	75.2%	24.8%	71.4%	28.6%
SATURDAY, 3/13 & 4/3, 2004	64.1%	35.9%	63.6%	36.4%	52.9%	47.1%	60.7%	39.3%	61.2%	38.8%

AVERAGE DAILY PARKING AVAILABILITY

THURSDAY, 3/11 & 4/1, 2004	85	Spaces Available	41%	Available
FRIDAY, 3/12 & 4/2, 2004	59	Spaces Available	29%	Available
SATURDAY, 3/13 & 4/3, 2004	66	Spaces Available	32%	Available
SUNDAY, 3/14 & 4/4, 2004	58	Spaces Available	28%	Available

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## MEMORANDUM

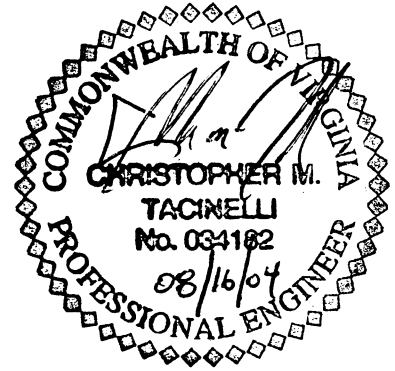
**TO:** Tom Culpepper City of Alexandria

**CC:** Howard Middleton Reed Smith

**FROM:** Christopher M. Tacinelli, P.E.  
Chad A. Baird  
Cheryl L. Franks

**DATE:** August 12, 2004

**SUBJECT:** The Prescott Traffic Assessment



This memorandum presents the findings of a traffic impact assessment for the redevelopment of The Prescott with 64 condominium units in Alexandria, Virginia. The project is scheduled to be complete by the year 2006.

The following tasks were undertaken as part of this study:

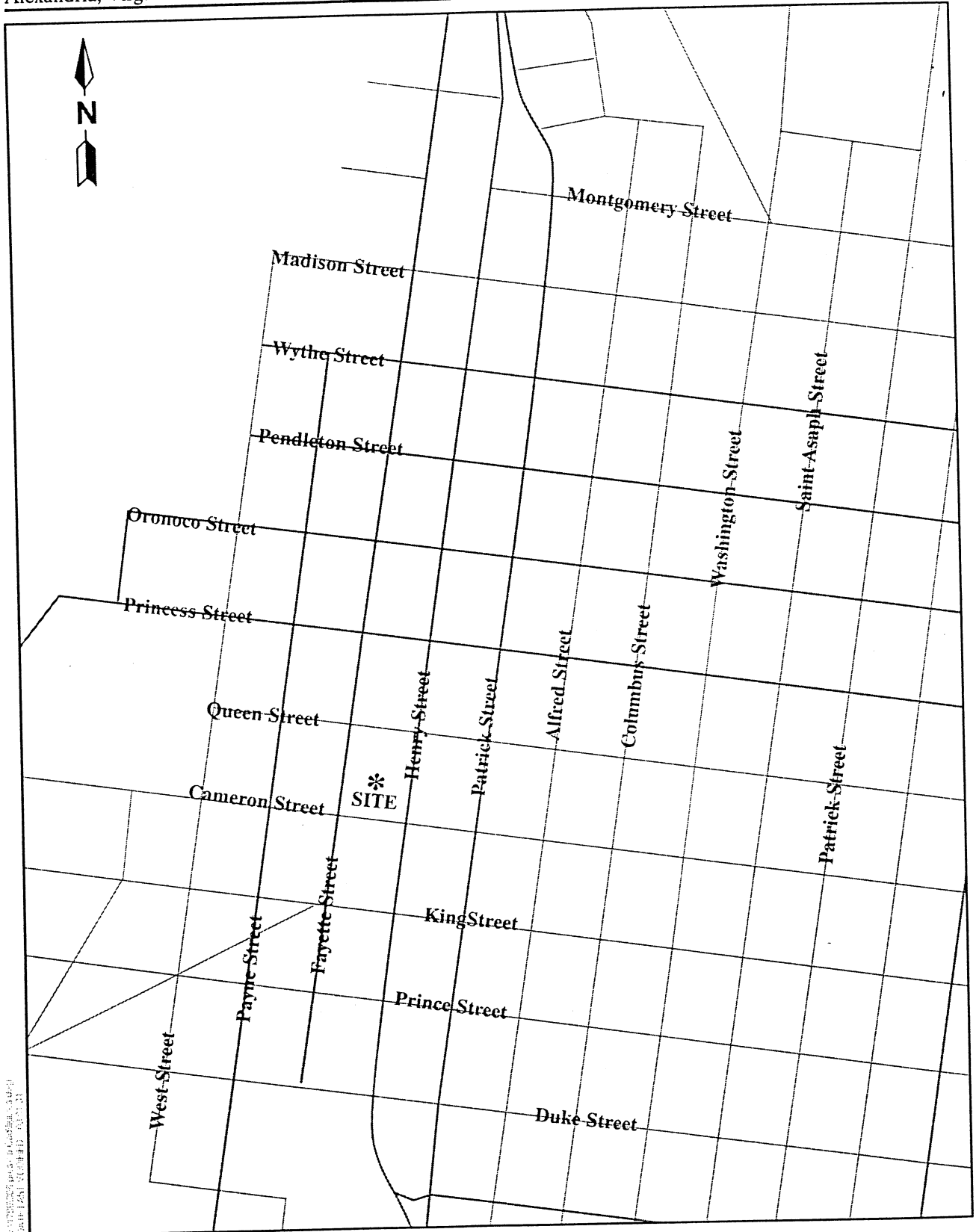
- ♦ Field reconnaissance in the vicinity of the site was performed to collect information related to existing traffic controls, roadway geometry, and traffic flow characteristics;
- ♦ Manual vehicle traffic turning movement counts were conducted at two intersections near the site during the morning and evening peak hours;
- ♦ Future traffic conditions were projected based on global growth in the vicinity of the site and the development's number of condominium units;
- ♦ Intersection capacity analyses were performed for existing, build year with development (2006), and build year with development +10 years additional background growth (2016) peak hour traffic conditions at the intersections contained within the study area.

Sources of data for this study include the City of Alexandria, the Virginia Department of Transportation (VDOT), and the office files and field reconnaissance efforts of Gorove/Slade Associates.

### Scope of Study

The following intersections were identified for inclusion in this study:

- ♦ Fayette Street and Queen Street;
- ♦ Fayette Street and Cameron Street



DATE: 12/15/04  
BY: [illegible]

Figure 1  
Site Location Map

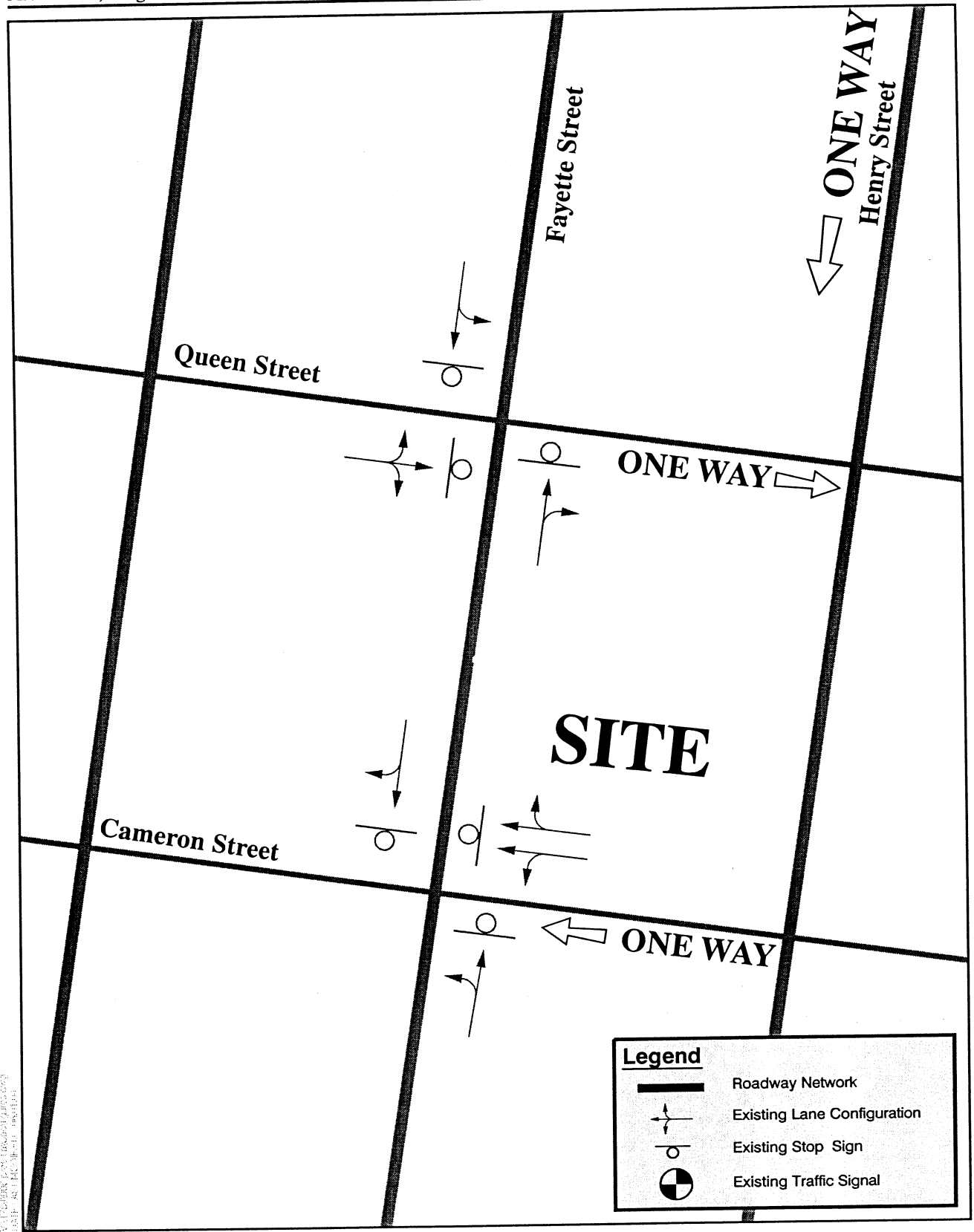


Figure 2  
Local Roadway Network



## EXISTING (2004) TRAFFIC CONDITIONS

### Existing (2004) Traffic Volumes

Existing weekday peak hour traffic turning movement counts were conducted at the intersections of Fayette Street with Queen Street and Fayette Street with Cameron Street on May 18, 2004 from 6:00 A.M. to 9:00 A.M. and 4:00 P.M. to 7:00 P.M. The A.M. commuter peak hour occurs from 8:00 A.M. to 9:00 A.M., the P.M. commuter peak hour occur from 5:15 P.M. to 6:15 P.M. The volumes during these commuter peak hours were considered in the analysis. The existing traffic volumes are illustrated in Figure 3.

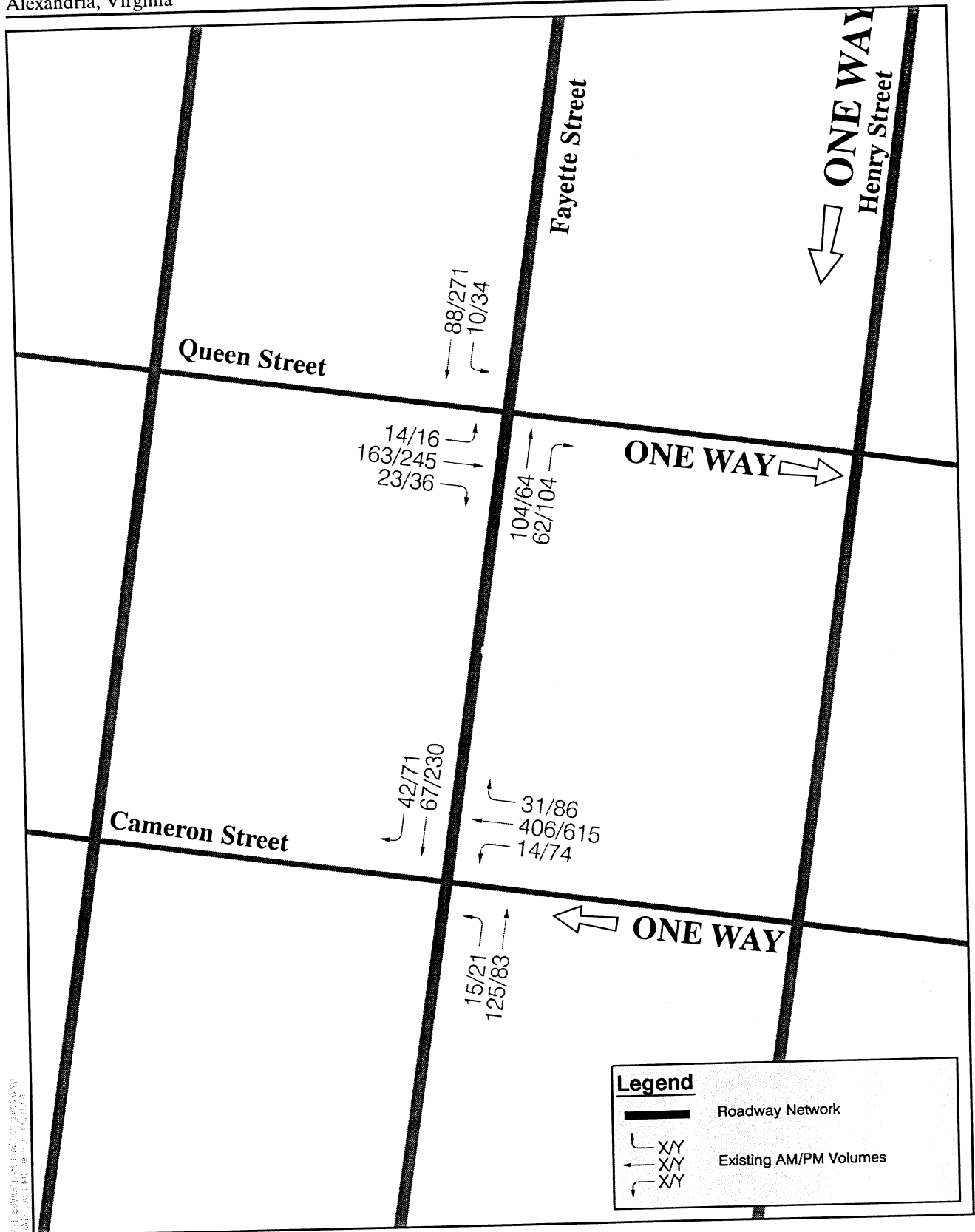
### Existing (2004) Capacity Analysis

Capacity analyses for the intersections within the study area were performed for the critical A.M. and P.M. commuter peak hours, as well as the Saturday midday peak hour. They were analyzed with Highway Capacity Manual methodology using the Highway Capacity Software 2000 (HCS 4.1d). The results of the capacity analyses can be found in Table 1, and are expressed in levels of service (LOS) and delay (in seconds).

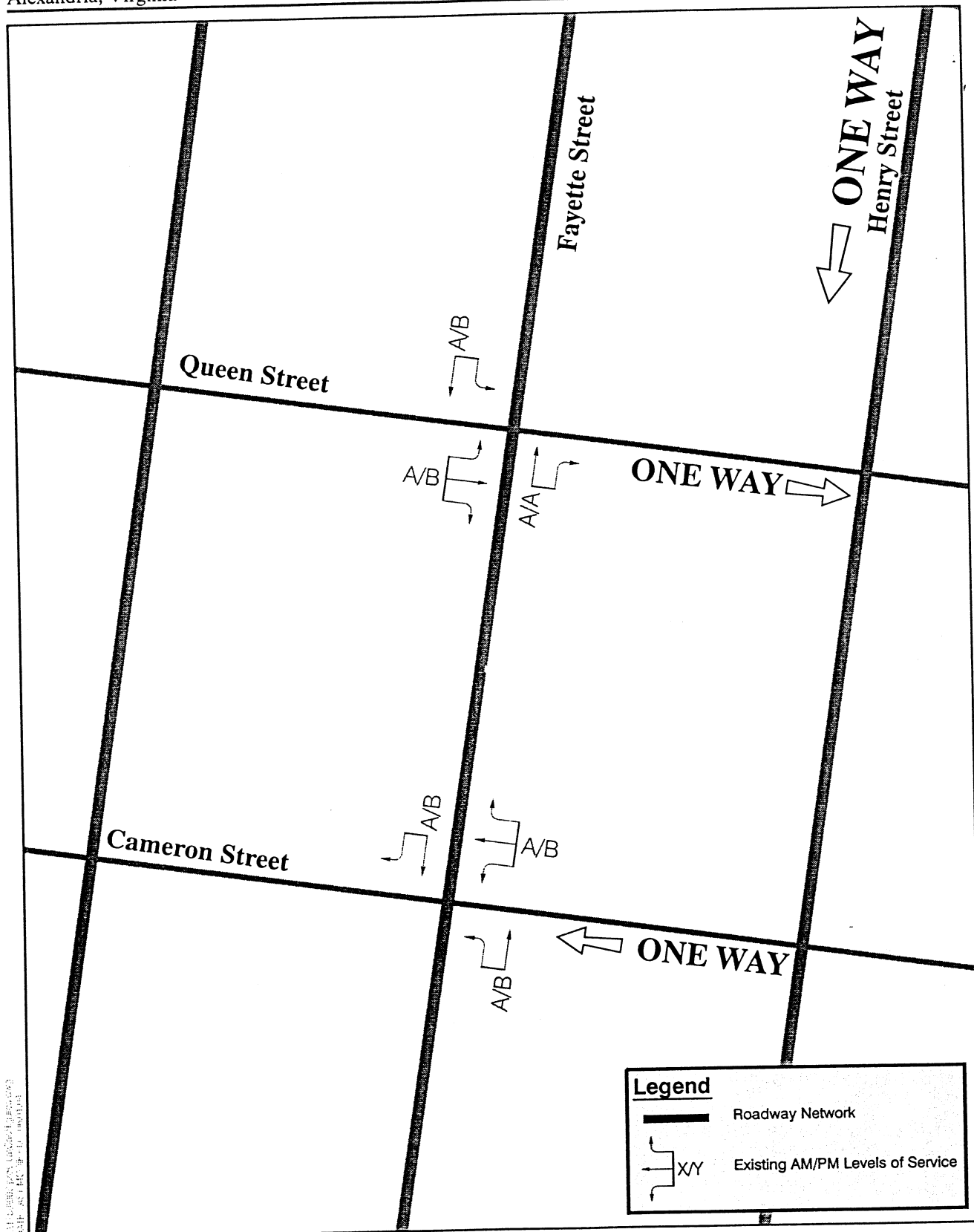
**Table 1**  
**Existing (2004) Capacity Analysis Results**

Intersection	Movement	LOS (Delay)	
		AM	PM
Fayette Street and Queen Street <i>Unsignalized</i>	Overall Intersection	A (8.56)	B (10.98)
	Eastbound Approach	A (8.90)	B (11.50)
	Northbound Approach	A (8.32)	A (9.11)
	Southbound Approach	A (8.24)	B (11.50)
Fayette Street and Cameron Street <i>Unsignalized</i>	Overall Intersection	A (9.14)	B (13.24)
	Westbound Approach	A (9.26)	B (13.73)
	Northbound Approach	A (9.20)	B (10.25)
	Southbound Approach	A (8.61)	B (12.99)

The results of the existing capacity analysis indicate that all of the intersections operate at acceptable levels of service during the A.M. and P.M. commuter peak hours. The results of the capacity analyses are shown in Figure 4. The detailed analysis worksheets are contained in the Technical Appendix.



**Figure 3**  
Existing (2004) Peak Hour Volumes



**Figure 4**  
Existing (2004) Levels of Service



## BUILD YEAR WITH DEVELOPMENT (2006) CONDITIONS

### Site Access

The proposed development will have one driveway that will serve the residents of the property. The driveway access point will be off Fayette Street between Queen Street and Cameron Street.

### Site Generated Traffic Volumes

In order to determine the appropriate trip generation for this new development, the Institute of Transportation Engineers (ITE) *Trip Generation Manual, 7<sup>th</sup> Edition* was used to determine the trips into and out of the proposed site. When using ITE's trip generation manual, the land use used in the calculations was #230 (Condominium/Townhouse). The summary of the trips generated for the peak hours as well as the weekday daily traffic volume is presented in Table 3.

**Table 2**  
**Trip Generation**

Land Use	ITE Code	Size	AM Peak Hour			PM Peak Hour			Average Daily Traffic		
			TOTAL	In	Out	TOTAL	In	Out	TOTAL	In	Out
<b>PROPOSED</b>											
Townhouse/ Condominium	230	64 Dwelling Units	37	7	30	42	29	13	440	220	220
		Directional Distribution		17%	83%		67%	33%		50%	50%
<b>TOTAL PROPOSED TRIPS</b>			<b>37</b>	<b>7</b>	<b>30</b>	<b>42</b>	<b>29</b>	<b>13</b>	<b>440</b>	<b>220</b>	<b>220</b>

### Directional Distribution and Trip Assignment

Based on the location of the subject site in the City of Alexandria and existing data, the assumption was made that the majority of the approaching traffic to the site will generally travel along Henry Street and Patrick Street. The trips estimated for the A.M. and P.M. peak hours were routed on the roadway network to the site based on the direction of approach assumptions. The site traffic assignment for the A.M. and P.M. peak hours is illustrated in Figure 5.

### Total Future (2006) Traffic Volumes

The construction of the proposed development is anticipated to be complete in 2006. Based on historical data provided by VDOT and other studies performed in the vicinity of the site, an inherent growth rate of 3 percent per year over a two-year period for a 6.09% total increase to existing traffic volumes was taken into consideration to account for regional increases in traffic due to background growth and development. The inherent traffic growth was added to the existing volumes and the proposed development site trips in order to establish total future 2006 traffic volumes. The total future traffic volumes are shown in Figure 6.

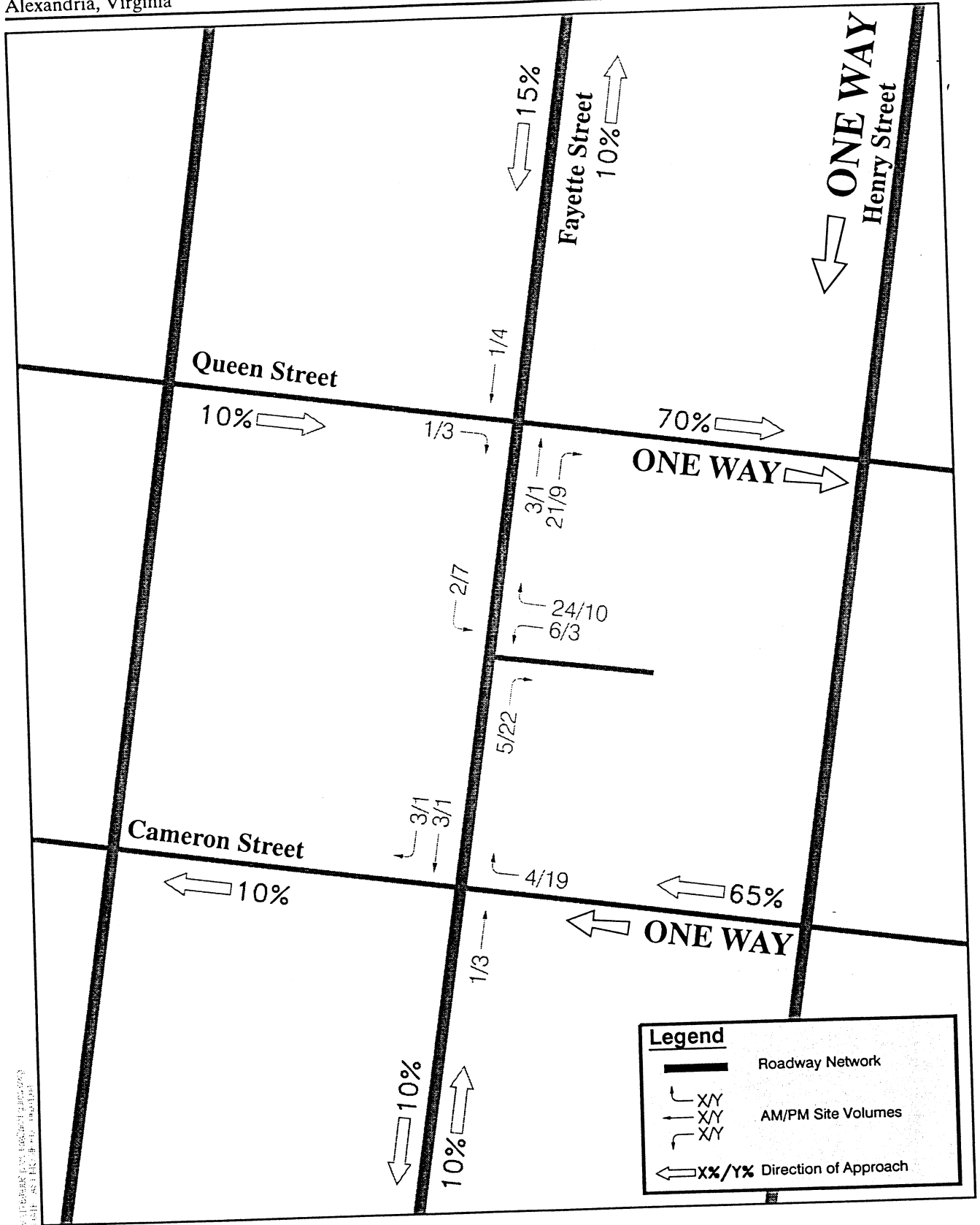


Figure 5  
Site Generated Volumes and Direction of Approach

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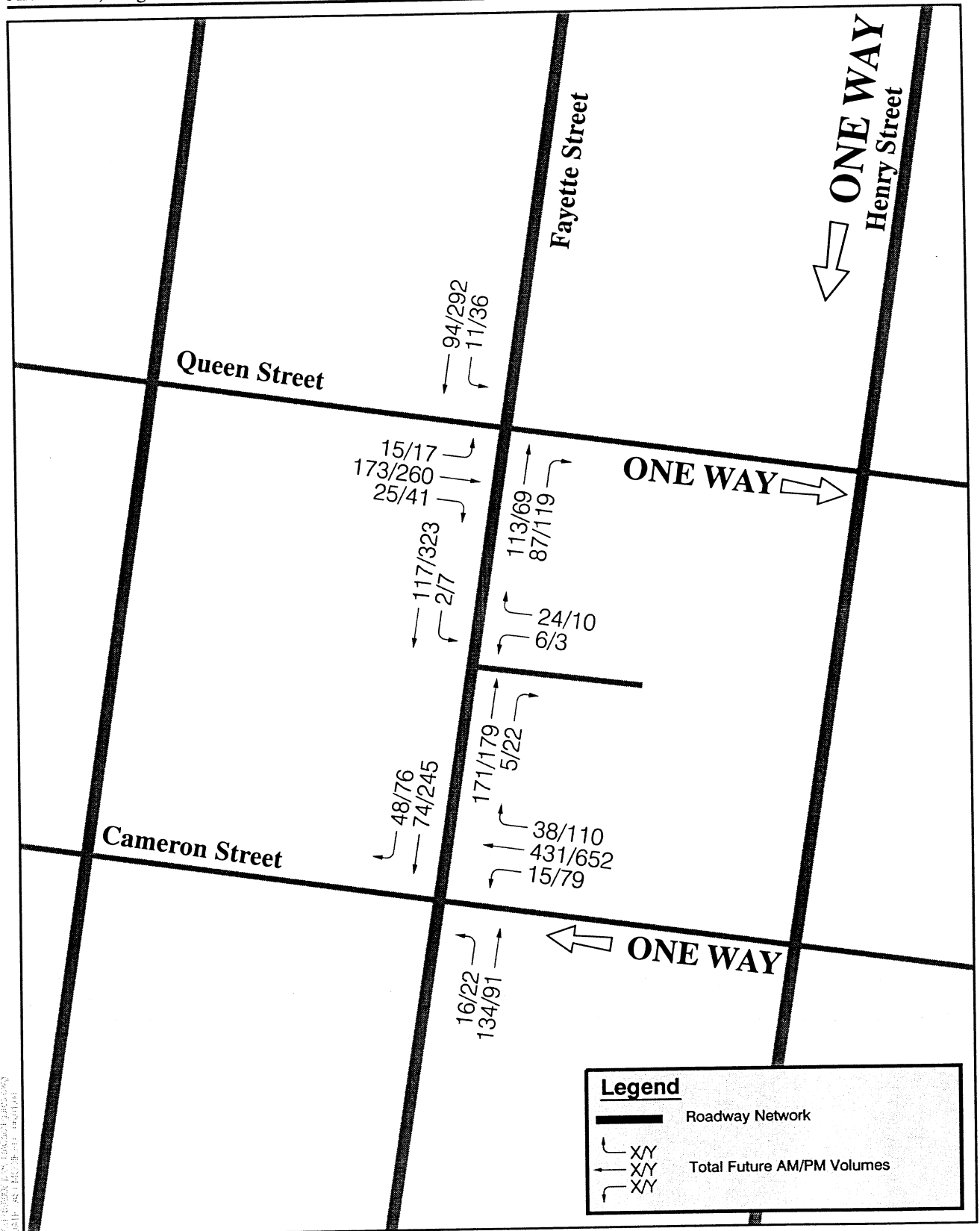


Figure 6  
Total Future (2006) Peak Hour Volumes

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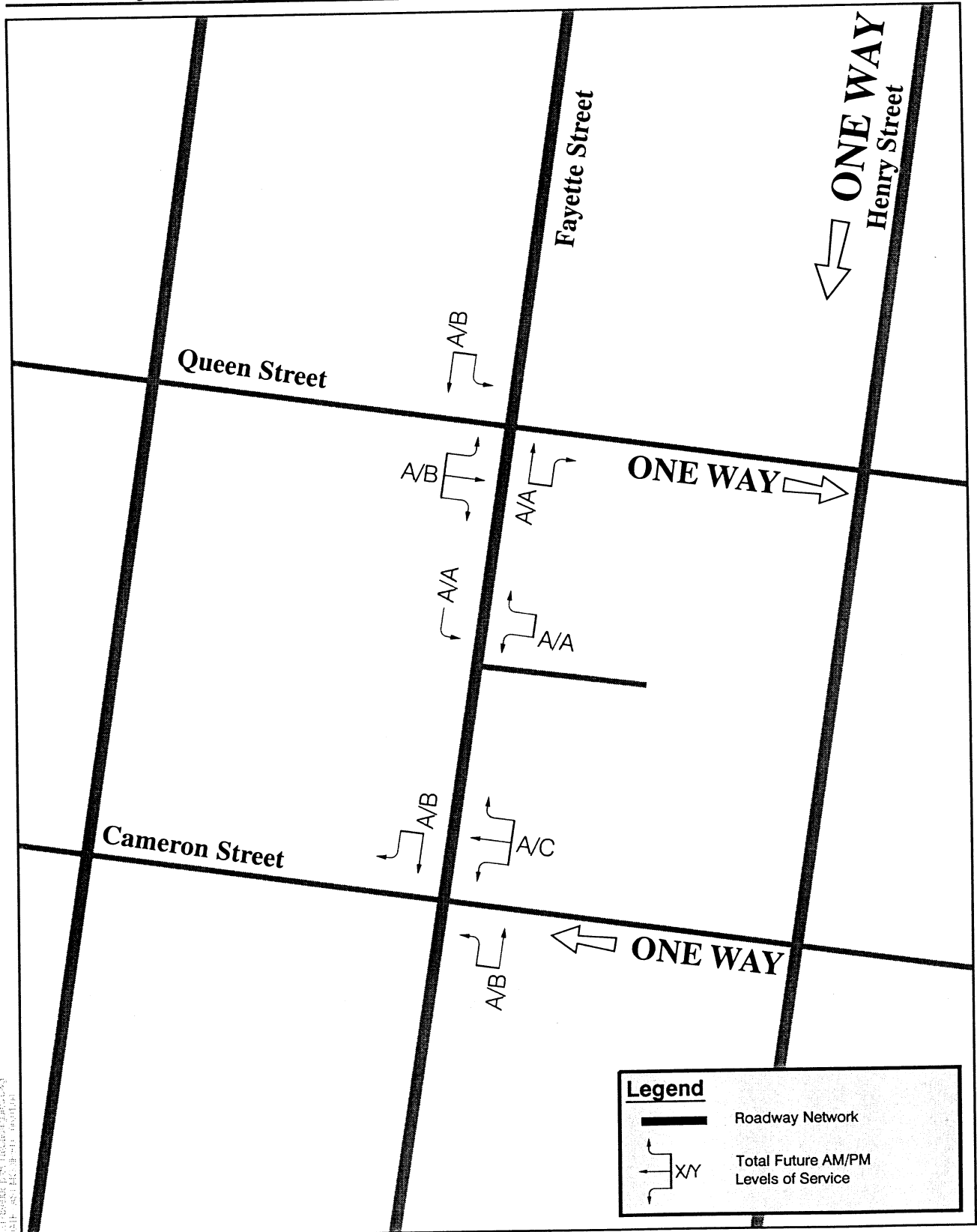
**Total Future (2006) Capacity Analysis**

Capacity analyses were performed to determine the operational levels of service of the studied intersections for the build year with development (total future) traffic conditions. The results of the level of service analysis for total future traffic conditions are presented in Table 3.

**Table 3  
 Total Future (2006) Capacity Analysis Results**

Intersection	Movement	LOS (Delay)	
		AM	PM
Fayette Street and Queen Street <i>Unsignalized</i>	Overall Intersection	A (8.82)	B (11.71)
	Eastbound Approach	A (9.20)	B (12.33)
	Northbound Approach	A (8.63)	A (9.54)
	Southbound Approach	A (8.41)	B (12.36)
Fayette Street and Cameron Street <i>Unsignalized</i>	Overall Intersection	A (9.43)	B (14.62)
	Westbound Approach	A (9.57)	C (15.38)
	Northbound Approach	A (9.45)	B (10.62)
	Southbound Approach	A (8.84)	B (14.02)
Fayette Street and Site Entrance <i>Unsignalized</i>	Westbound Approach	A (9.5)	A (9.9)
	Southbound Left Turn	A (7.6)	A (7.6)

The results of the total future capacity analysis indicate that all of the intersections operate at acceptable overall levels of service "B" or better during the A.M. and P.M. peak hours. The traffic generated by the proposed site will have negligible impact to the traffic conditions on Fayette Street as well as Queen and Cameron Streets. The results of the capacity analyses are shown in Figure 7. The detailed analysis worksheets are contained in the Technical Appendix.



**Figure 7**  
Total Future (2006) Levels of Service

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**BUILD YEAR +10 YEARS WITH DEVELOPMENT (2016) CONDITIONS**

**Total Future +10 Years (2016) Traffic Volumes**

An analysis of a future scenario estimating traffic conditions for ten years beyond the build-out of the site was performed. The inherent growth rate of 3 percent per year over a twelve-year period for a 42.6% total increase to existing traffic volumes was taken into consideration to account for regional increases in traffic due to background growth and development. The inherent traffic growth was added to the existing volumes and the proposed development site trips in order to establish total future 2016 traffic volumes. The total future +10 years traffic volumes are shown in Figure 8.

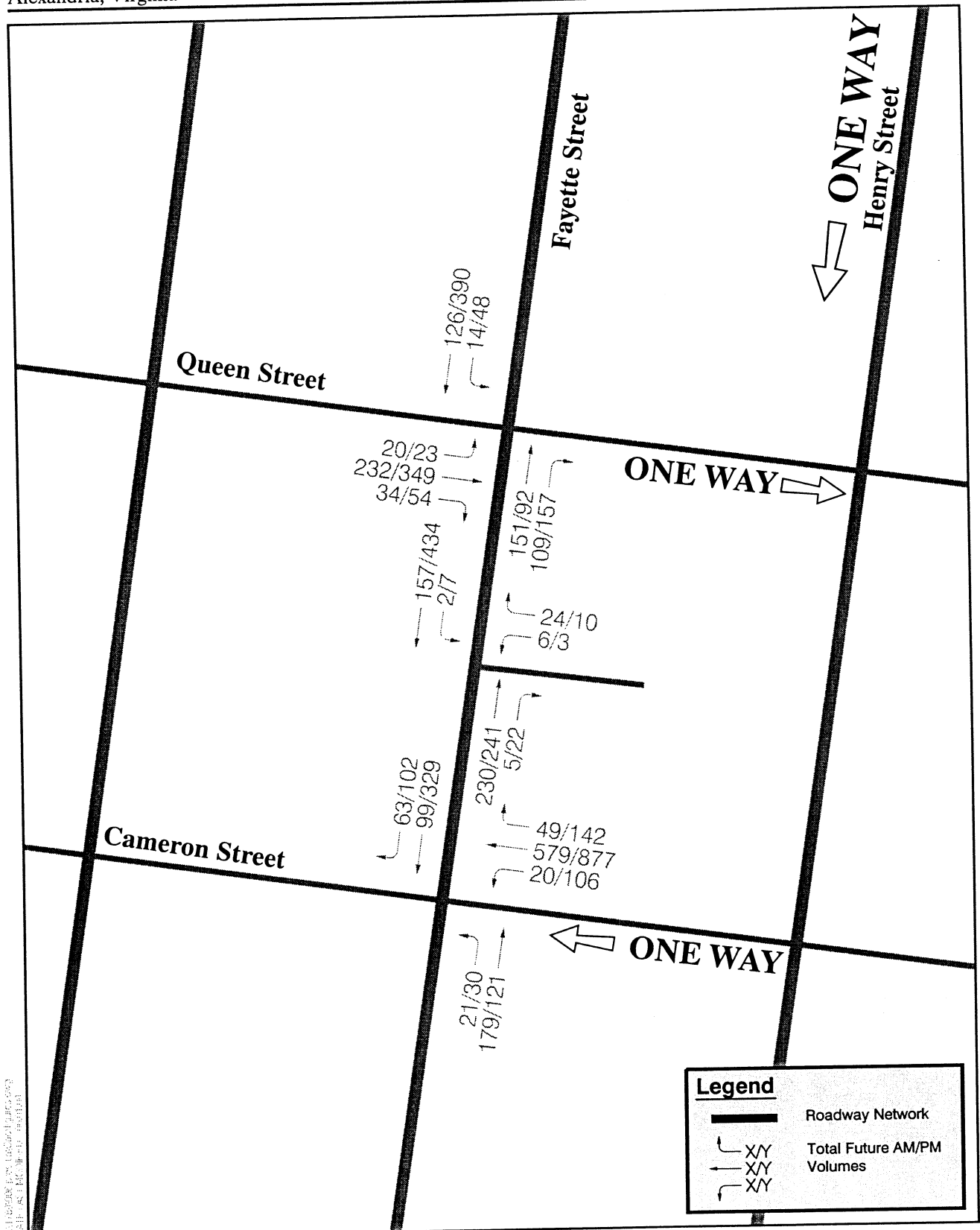
**Total Future +10 Years (2016) Capacity Analysis**

Capacity analyses were performed to determine the operational levels of service of the studied intersections for the build year with development plus ten years traffic conditions. The results of the level of service analysis for total future traffic conditions are presented in Table 3.

**Table 4  
 Total Future +10 Years (2016) Capacity Analysis Results**

Intersection	Movement	LOS (Delay)	
		AM	PM
<b>Fayette Street and Queen Street</b> <i>Unsignalized</i>	<b>Overall Intersection</b>	<b>B (10.10)</b>	<b>C (18.48)</b>
	Eastbound Approach	B (10.76)	C (20.04)
	Northbound Approach	A (9.85)	B (12.18)
	Southbound Approach	A (9.20)	C (20.54)
<b>Fayette Street and Cameron Street</b> <i>Unsignalized</i>	<b>Overall Intersection</b>	<b>B (11.21)</b>	<b>D (30.86)</b>
	Westbound Approach	B (11.60)	E (36.39)
	Northbound Approach	B (10.91)	B (12.43)
	Southbound Approach	A (9.99)	C (22.89)
<b>Fayette Street and Site Entrance</b> <i>Unsignalized</i>	Westbound Approach	A (9.9)	B (10.7)
	Southbound Left Turn	A (7.7)	A (7.8)

The results of the total future plus 10 years capacity analysis indicate that all of the intersections operate at acceptable overall intersection levels of service during the A.M. and P.M. peak hours. The results of the capacity analyses are shown in Figure 9. The detailed analysis worksheets are contained in the Technical Appendix.



**Figure 8**  
Total Future +10 Years (2016) Peak Hour Volumes

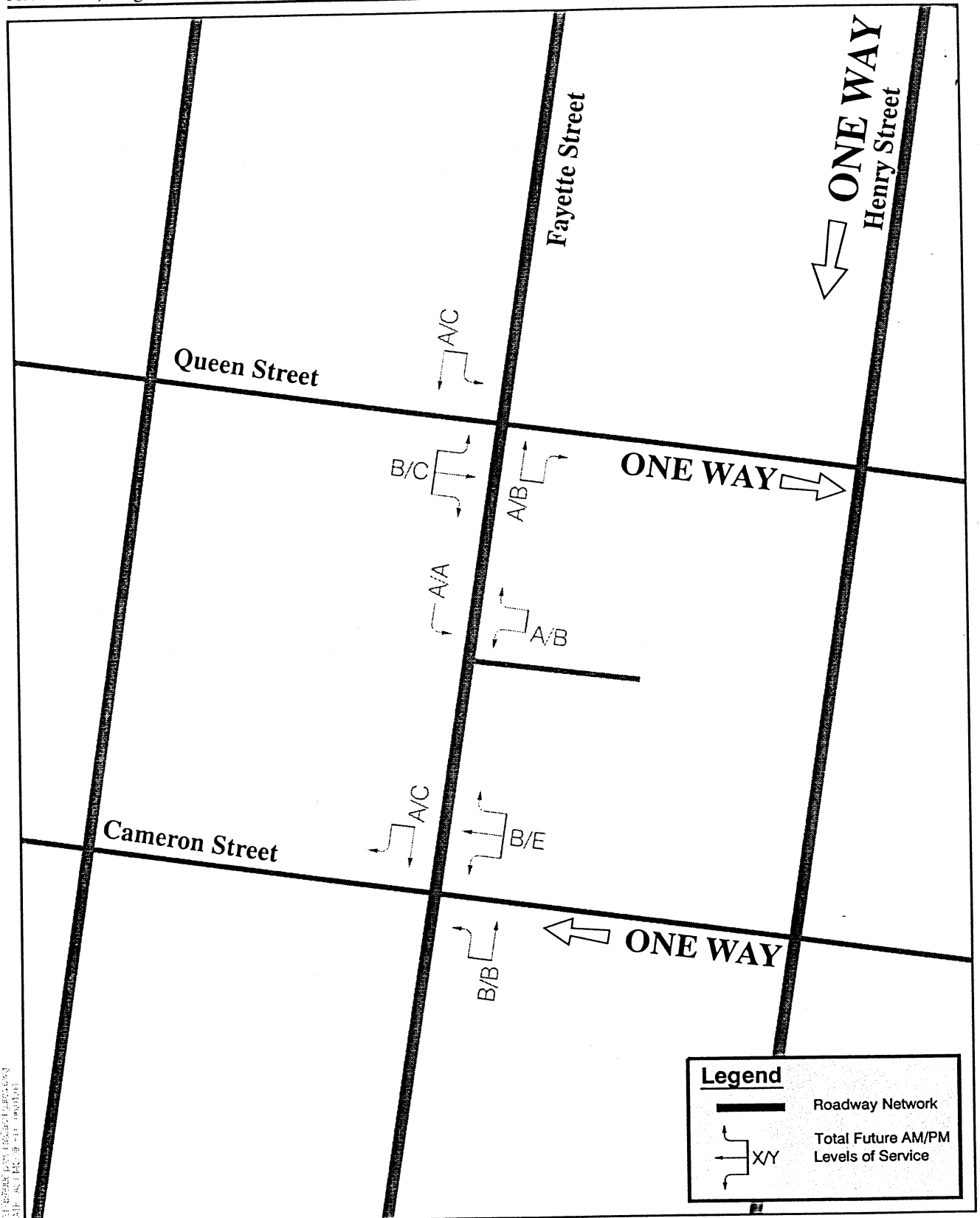


Figure 9  
Total Future +10 Years (2016) Levels of Service



## **CONCLUSIONS**

This memorandum presents the findings of a traffic impact assessment for the redevelopment of The Prescott with 64 condominium units in Alexandria, Virginia. The project is scheduled to be complete by the year 2006.

Based on analysis, the following major conclusions were determined:

### **Existing Conditions (2004)**

The results of the existing intersection capacity analyses show that the intersections studied will operate at acceptable overall levels of service "B" or better during the A.M. and P.M. peak hours.

### **Build Year (2006) with Development (Total Future) Conditions**

The results of the total future capacity analysis indicate that all of the intersections operate at acceptable overall levels of service "B" or better during the A.M. and P.M. peak hours. The traffic generated by the proposed site will have a negligible impact to the traffic conditions on Fayette, Queen and Cameron Streets; therefore, no mitigation measures are necessary.

### **Build Year with Development +10 Years Additional Growth (2016) Conditions**

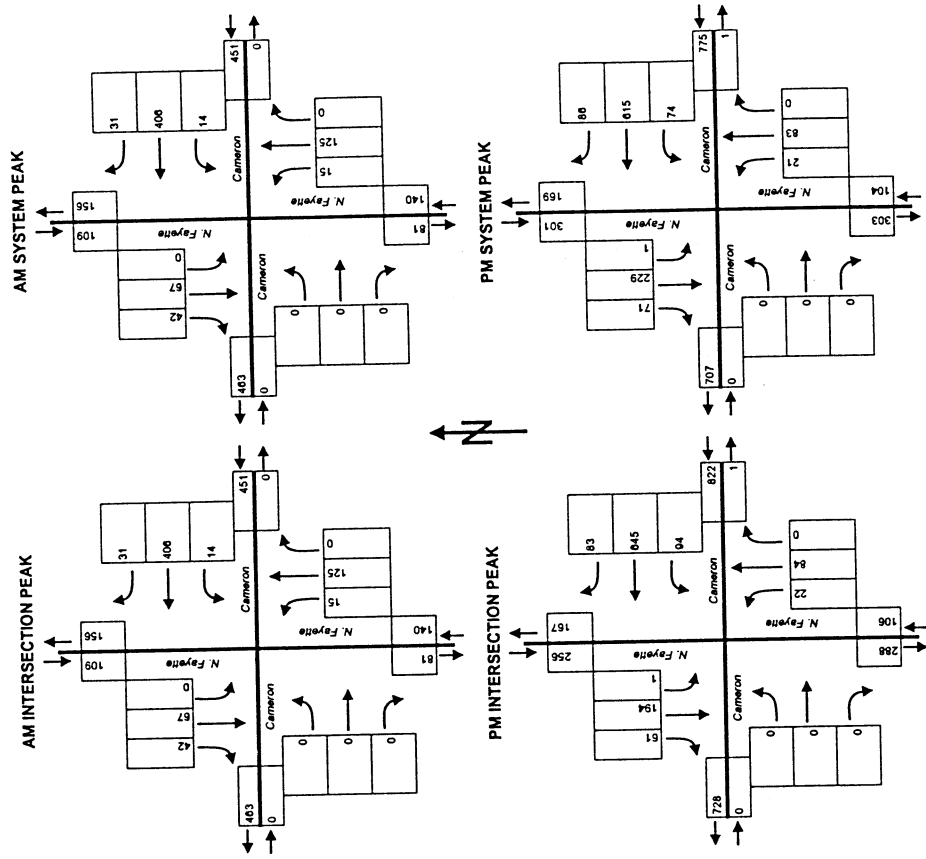
The results of the total future plus ten years capacity analysis indicate that despite the additional ten years of background growth, all of the intersections will continue to operate at acceptable overall intersection levels of service during the A.M. and P.M. peak hours.

The Prescott  
August 12, 2004  
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## **APPENDIX**

**APPENDIX A**  
**Existing 2004 Traffic Counts**

Date of Counts: May 18, 2004  
Weather Conditions: Fair

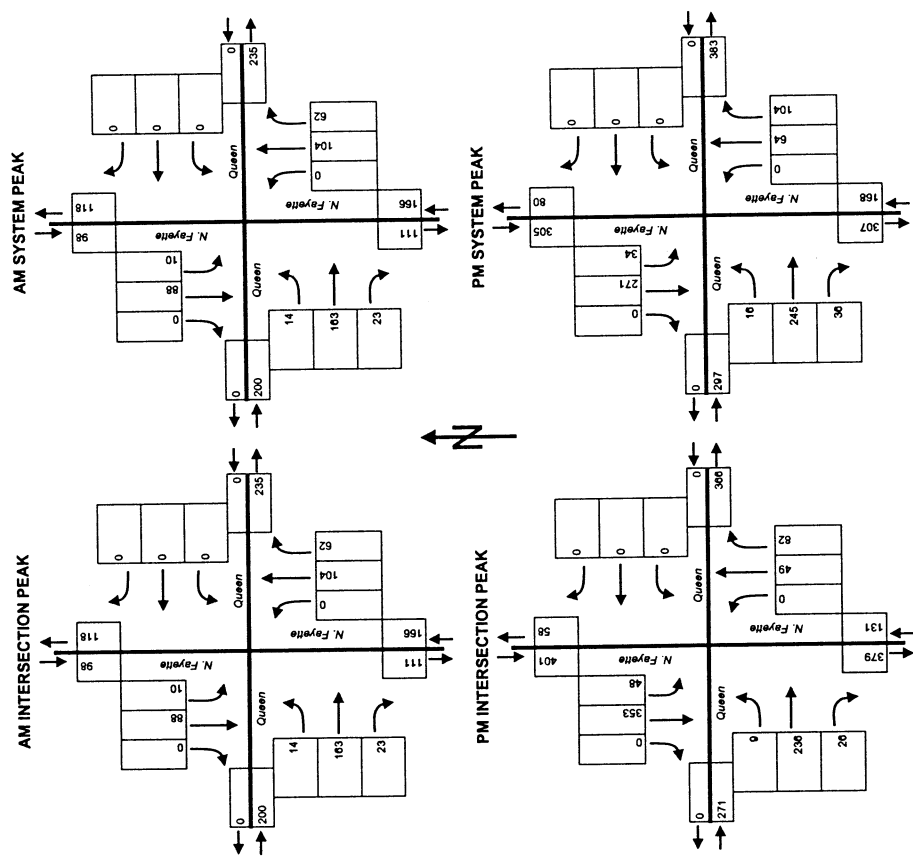


Intersection:		N. Fayette at Cameron																	
AM PEAK	Direction: Roadway: Movement:	Southbound			Westbound			Northbound			Eastbound								
		Right	Thru	Left	Right	Thru	Left	Right	Thru	Left	Right	Thru	Left						
6:00 AM to 6:15 AM		2	1	0	0	2	16	1	0	0	4	0	0	0	0	0	0	0	
6:15 AM to 6:30 AM		1	3	0	0	0	20	3	1	0	8	2	0	0	0	0	0	0	
6:30 AM to 6:45 AM		1	2	0	0	1	0	22	0	1	10	2	3	0	0	0	0	0	
6:45 AM to 7:00 AM		2	2	0	3	2	26	1	1	0	24	2	1	0	0	0	0	0	
7:00 AM to 7:15 AM		1	9	0	1	7	38	2	1	0	20	1	2	0	0	0	2	1	
7:15 AM to 7:30 AM		3	12	0	1	6	60	1	1	0	18	0	1	0	0	0	0	2	
7:30 AM to 7:45 AM		9	6	0	0	2	86	3	1	0	19	0	1	0	0	0	0	2	
7:45 AM to 8:00 AM		11	14	0	0	8	105	2	0	0	26	0	1	0	0	0	0	2	
8:00 AM to 8:15 AM		9	18	0	0	6	110	5	0	0	23	3	0	0	0	0	0	2	
8:15 AM to 8:30 AM		13	18	0	0	8	113	5	1	0	35	4	1	0	0	0	0	2	
8:30 AM to 8:45 AM		10	12	0	1	8	92	3	3	0	33	2	3	0	0	0	0	1	
8:45 AM to 9:00 AM		10	19	0	1	9	91	1	1	0	34	6	5	0	0	0	0	4	
PM PEAK																			
PM PEAK	Direction: Roadway: Movement:	Southbound			Westbound			Northbound			Eastbound								
		Right	Thru	Left	Right	Thru	Left	Right	Thru	Left	Right	Thru	Left						
4:00 PM to 4:15 PM		6	31	0	2	13	101	28	1	0	7	0	7	0	0	0	0	8	
4:15 PM to 4:30 PM		6	26	0	7	9	98	21	3	0	11	1	3	0	0	0	0	7	
4:30 PM to 4:45 PM		14	23	0	9	16	134	27	3	0	22	1	2	0	0	0	0	4	
4:45 PM to 5:00 PM		12	35	0	8	15	135	33	2	0	14	3	3	0	0	0	0	1	
5:00 PM to 5:15 PM		14	37	0	6	20	180	30	2	0	17	3	6	0	0	0	0	2	
5:15 PM to 5:30 PM		10	53	0	5	21	172	31	0	0	26	7	7	0	0	0	0	3	
5:30 PM to 5:45 PM		17	41	1	1	24	148	18	5	0	19	4	2	0	0	0	0	2	
5:45 PM to 6:00 PM		20	63	0	5	18	145	15	1	0	22	8	1	0	0	0	0	1	
6:00 PM to 6:15 PM		24	72	0	1	23	150	10	4	0	16	2	3	0	0	0	0	1	
6:15 PM to 6:30 PM		20	70	0	5	12	137	21	0	0	26	1	3	0	0	0	0	1	
6:30 PM to 6:45 PM		26	80	0	1	7	104	17	0	0	19	6	1	0	0	0	0	1	
6:45 PM to 7:00 PM		22	61	1	1	6	91	21	3	0	18	6	2	0	0	0	0	3	
PEAK HOURS																			
AM INTERSECTION PEAK HOUR	Direction: Roadway: Movement:	Southbound			Westbound			Northbound			Eastbound								
		Right	Thru	Left	Right	Thru	Left	Right	Thru	Left	Right	Thru	Left						
6:00 AM to 6:00 AM		42	87	0	2	31	406	14	5	0	123	15	9	0	0	0	0	9	
6:00 PM to 6:00 PM		61	194	1	17	83	645	94	8	0	94	22	16	0	0	0	0	8	
AM SYSTEM PEAK HOUR																			
8:00 AM to 9:00 AM		42	87	0	2	31	406	14	5	0	123	15	9	0	0	0	0	9	
PM SYSTEM PEAK HOUR		71	229	1	12	68	816	74	10	0	83	21	13	0	0	0	0	6	
PEAK HOUR FACTORS																			
AM PEAK HOUR		0.83	0.88	n/a	0.80	0.86	0.83	0.75	0.42	n/a	0.80	0.63	0.48	n/a	n/a	n/a	n/a	0.58	
PM PEAK HOUR		0.88	0.89	0.26	0.83	0.90	0.80	0.82	0.60	n/a	0.81	0.60	1.18	n/a	n/a	n/a	n/a	0.63	
Overall AM PEAK HOUR FACTOR		= .804/0.01																	
Overall PM PEAK HOUR FACTOR		= 0.82																	

Notes:

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Date of Counts: May 18, 2004  
Weather Conditions: Fair



Intersection:		N. Fayette at Queen																
AM PEAK	Direction: Roadway: Movement:	Southbound N. Fayette			Westbound Queen			Northbound N. Fayette			Eastbound Queen							
		Right	Thru	Left	Right	Thru	Left	Right	Thru	Left	Right	Thru	Left					
8:00 AM to 8:15 AM		5	0	0	0	0	0	1	4	0	0	0	0	3	0	0		
8:15 AM to 8:30 AM		3	3	0	0	0	0	2	8	0	0	0	1	2	0	0		
8:30 AM to 8:45 AM		0	3	1	0	0	0	7	8	0	0	0	0	12	4	0		
8:45 AM to 9:00 AM		0	8	3	0	0	0	7	17	0	0	0	0	13	2	0		
7:00 AM to 7:15 AM		0	10	1	0	0	0	7	20	0	0	0	0	11	3	0		
7:15 AM to 7:30 AM		1	14	4	0	0	0	5	16	0	0	0	1	16	4	0		
7:30 AM to 7:45 AM		0	16	4	0	0	0	4	21	0	0	0	0	23	4	0		
7:45 AM to 8:00 AM		0	23	2	0	0	0	4	21	0	0	0	3	38	2	0		
8:00 AM to 8:15 AM		0	26	5	0	0	0	8	28	0	0	0	9	36	3	0		
8:15 AM to 8:30 AM		0	22	2	0	0	0	20	20	0	0	0	5	45	6	0		
8:30 AM to 8:45 AM		0	15	2	0	0	0	16	22	0	0	0	4	39	4	0		
8:45 AM to 9:00 AM		0	25	1	0	0	0	18	34	0	0	0	5	43	1	0		
PM PEAK																		
4:00 PM to 4:15 PM		0	28	4	0	0	0	8	10	0	0	0	7	34	0	0		
4:15 PM to 4:30 PM		0	34	1	0	0	0	7	15	0	0	0	5	28	3	0		
4:30 PM to 4:45 PM		0	31	6	0	0	0	18	21	0	0	0	7	21	5	0		
4:45 PM to 5:00 PM		0	36	4	0	0	0	14	13	0	0	0	11	38	3	0		
5:00 PM to 5:15 PM		0	48	7	0	0	0	22	17	0	0	0	7	37	6	0		
5:15 PM to 5:30 PM		0	57	2	0	0	0	28	20	0	0	0	14	58	6	0		
5:30 PM to 5:45 PM		0	50	5	0	0	0	29	16	0	0	0	5	77	7	0		
5:45 PM to 6:00 PM		0	76	12	0	0	0	26	14	0	0	0	11	59	2	0		
6:00 PM to 6:15 PM		0	88	15	0	0	0	21	14	0	0	0	6	51	1	0		
6:15 PM to 6:30 PM		0	89	12	0	0	0	19	12	0	0	0	5	60	5	0		
6:30 PM to 6:45 PM		0	100	9	0	0	0	16	9	0	0	0	4	66	1	0		
6:45 PM to 7:00 PM		0	78	7	0	0	0	9	8	0	0	0	10	48	2	0		
PEAK HOURS																		
Direction: Roadway: Movement:		Southbound N. Fayette	Westbound Queen	Northbound N. Fayette	Eastbound Queen	Southbound N. Fayette	Westbound Queen	Northbound N. Fayette	Eastbound Queen	Southbound N. Fayette	Westbound Queen	Northbound N. Fayette	Eastbound Queen	Southbound N. Fayette	Westbound Queen	Northbound N. Fayette	Eastbound Queen	
8:00 AM to 9:00 AM		86	10	0	0	82	104	0	0	23	183	14	0	0	0	0	0	
PM INTERSECTION PEAK HOUR		353	48	401	58	48	353	48	401	58	48	353	48	401	58	48	353	48
AM SYSTEM PEAK HOUR		307	80	305	80	34	305	80	307	80	34	305	80	307	80	34	305	80
PM SYSTEM PEAK HOUR		271	0	271	0	271	0	0	271	0	271	0	0	271	0	0	271	0
AM INTERSECTION PEAK HOUR		235	0	235	0	235	0	0	235	0	235	0	0	235	0	0	235	0
PM INTERSECTION PEAK HOUR		383	0	383	0	383	0	0	383	0	383	0	0	383	0	0	383	0
AM SYSTEM PEAK HOUR		200	0	200	0	200	0	0	200	0	200	0	0	200	0	0	200	0
PM SYSTEM PEAK HOUR		297	0	297	0	297	0	0	297	0	297	0	0	297	0	0	297	0
PEAK HOURS																		
Direction: Roadway: Movement:		Southbound N. Fayette	Westbound Queen	Northbound N. Fayette	Eastbound Queen	Southbound N. Fayette	Westbound Queen	Northbound N. Fayette	Eastbound Queen	Southbound N. Fayette	Westbound Queen	Northbound N. Fayette	Eastbound Queen	Southbound N. Fayette	Westbound Queen	Northbound N. Fayette	Eastbound Queen	
AM PEAK HOUR		0.26	0.05	0.76	n/a	0.76	n/a	0.76	n/a	0.84	0.91	0.83	n/a	0.83	n/a	0.83	n/a	n/a
PM PEAK HOUR		0.80	0.80	0.80	n/a	0.80	n/a	0.80	n/a	0.70	0.80	0.70	n/a	0.70	n/a	0.70	n/a	n/a
Overall AM PEAK HOUR FACTOR																		
Overall PM PEAK HOUR FACTOR																		

Notes:

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GOROVE/SLADE ASSOCIATES INC.  
 1175 HERNDON PARKWAY STE. 600  
 HERNDON, VIRGINIA 20170

CARRHOMES, PRESCOTT  
 ALEXANDRIA, VIRGINIA

Start Time	19-May-04		Northbound		Southbound		Combined		20-May-04		Northbound		Southbound		Combined	
	Wed		A.M.	P.M.	A.M.	P.M.	A.M.	P.M.	Thu	A.M.	P.M.	A.M.	P.M.	A.M.	P.M.	
12:00			*	30	*	18	*	48		2	32	0	14	2	46	
12:15			*	28	*	15	*	43		0	29	1	27	1	56	
12:30			*	28	*	19	*	47		0	22	2	27	2	49	
12:45			*	37	*	23	*	60		2	26	0	24	2	50	
01:00			*	23	*	22	*	45		2	32	1	23	3	55	
01:15			*	34	*	22	*	56		2	42	2	22	4	64	
01:30			*	30	*	13	*	43		2	30	0	27	2	57	
01:45			*	28	*	12	*	40		2	28	1	20	3	48	
02:00			*	28	*	18	*	46		1	34	1	25	2	59	
02:15			*	26	*	26	*	52		2	19	0	21	2	40	
02:30			*	22	*	19	*	41		2	35	2	15	4	50	
02:45			*	26	*	28	*	54		1	24	2	14	3	38	
03:00			*	26	*	13	*	39		0	19	1	36	1	55	
03:15			*	38	*	21	*	59		0	25	0	26	0	51	
03:30			*	24	*	19	*	43		0	17	0	30	0	47	
03:45			*	21	*	23	*	44		1	25	1	33	2	58	
04:00			*	18	*	38	*	56		3	25	2	32	5	57	
04:15			*	21	*	30	*	51		1	25	0	47	1	72	
04:30			*	17	*	30	*	47		4	32	3	25	7	57	
04:45			*	60	*	7	*	67		0	33	1	25	1	58	
05:00			*	30	*	35	*	65		3	44	1	28	4	72	
05:15			*	47	*	49	*	96		3	39	4	23	7	62	
05:30			*	39	*	53	*	92		5	36	6	20	11	56	
05:45			*	46	*	44	*	90		2	47	2	23	4	70	
06:00			*	28	*	65	*	93		10	26	10	21	20	47	
06:15			*	35	*	46	*	81		12	30	6	31	18	61	
06:30			*	23	*	34	*	57		10	26	1	26	11	52	
06:45			*	23	*	31	*	54		12	25	2	22	14	47	
07:00			*	32	*	22	*	54		20	23	5	21	25	44	
07:15			*	19	*	19	*	38		32	23	25	21	57	44	
07:30			*	16	*	22	*	38		25	29	14	16	39	45	
07:45			*	17	*	10	*	27		18	20	15	4	33	24	
08:00			*	14	*	15	*	29		30	23	16	14	46	37	
08:15		13		18	6	6	19	24		31	27	17	6	48	33	
08:30		41	14	26	5	67	19	19		26	18	21	9	47	27	
08:45		45	11	16	6	61	17	17		31	15	26	7	57	22	
09:00		35	11	23	10	58	21	21		40	15	24	13	64	28	
09:15		26	11	29	8	55	19	19		44	21	31	10	75	31	
09:30		32	8	17	7	49	15	15		37	10	32	7	69	17	
09:45		17	6	20	5	37	11	11		25	8	24	4	49	12	
10:00		32	9	17	4	49	13	13		22	12	27	7	49	19	
10:15		23	7	15	3	38	10	10		27	14	23	6	50	20	
10:30		22	5	22	6	44	11	11		27	5	11	12	38	17	
10:45		19	2	22	2	41	4	4		35	4	21	4	56	8	
11:00		18	1	15	3	33	4	4		29	7	17	4	46	11	
11:15		24	5	9	2	33	7	7		20	6	18	4	38	10	
11:30		28	5	11	1	39	6	6		40	4	22	1	62	5	
11:45		23	2	25	6	48	8	8		34	1	17	2	51	3	
Total			398	1049	273	935	671	1984		677	1112	458	879	1135	1991	
Day Total			1447		1208		2655			1789		1337		3126		
% Total			15.0%	39.5%	10.3%	35.2%				21.7%	35.6%	14.7%	28.1%			
Peak			08:30	04:45	08:30	05:15	08:30	05:15		08:45	05:00	09:15	03:30	08:45	05:00	
Vol.			147	176	94	211	241	371		152	166	114	142	265	260	
P.H.F.			0.817	0.733	0.810	0.812	0.899	0.966		0.864	0.883	0.891	0.755	0.883	0.903	

**GOROVE/SLADE ASSOCIATES INC.**  
**1175 HERNDON PARKWAY STE. 600**  
**HERNDON, VIRGINIA 20170**

**CARRHOMES, PRESCOTT**  
**ALEXANDRIA, VIRGINIA**

Start Time	21-May-04		Northbound		Southbound		Combined		22-May-04		Northbound		Southbound		Combined	
	Fri		A.M.	P.M.	A.M.	P.M.	A.M.	P.M.	Sat		A.M.	P.M.	A.M.	P.M.	A.M.	P.M.
12:00			3	31	5	21	8	52			3	21	6	23	9	44
12:15			3	37	0	24	3	61			7	30	5	25	12	55
12:30			2	33	2	31	4	64			12	24	3	16	15	40
12:45			1	34	1	27	2	61			10	23	1	12	11	35
01:00			0	37	2	13	2	50			2	35	4	18	6	53
01:15			4	33	0	13	4	46			0	18	4	31	4	49
01:30			2	17	0	12	2	29			4	37	6	20	10	51
01:45			0	32	0	16	0	48			1	38	2	18	3	56
02:00			2	39	1	20	3	59			4	33	4	24	8	57
02:15			0	28	1	23	1	51			3	23	3	21	6	44
02:30			1	30	2	23	3	53			1	24	1	8	2	32
02:45			2	24	0	20	2	44			0	25	2	15	2	40
03:00			0	22	1	19	1	41			3	23	1	14	4	37
03:15			1	30	1	37	2	67			0	21	1	9	1	30
03:30			0	26	1	32	1	58			2	28	1	12	3	40
03:45			2	34	0	39	2	73			0	29	2	16	2	45
04:00			2	23	0	37	2	60			3	15	2	20	5	35
04:15			3	36	0	27	3	63			0	25	2	16	2	41
04:30			5	28	2	36	7	64			2	23	2	17	4	40
04:45			3	31	1	29	4	60			4	17	3	14	7	31
05:00			6	25	3	29	9	54			1	18	2	19	3	37
05:15			1	40	4	36	5	76			1	12	0	14	1	26
05:30			0	37	2	32	2	69			0	24	1	13	1	37
05:45			8	30	5	30	13	60			4	21	7	10	11	31
06:00			6	29	8	16	14	45			9	18	4	17	13	35
06:15			5	32	2	38	7	70			3	15	4	10	7	25
06:30			8	30	4	30	12	60			5	15	2	9	7	24
06:45			12	30	12	28	24	58			4	24	2	9	6	33
07:00			14	23	6	23	20	46			13	19	6	13	19	32
07:15			29	27	13	15	42	42			7	13	3	8	10	21
07:30			20	19	13	9	33	28			7	13	2	12	9	25
07:45			27	25	8	13	35	38			7	16	10	14	17	30
08:00			39	26	18	12	57	38			6	22	7	11	13	33
08:15			32	34	13	14	45	48			6	19	11	6	17	25
08:30			48	32	20	13	68	45			9	20	11	9	20	29
08:45			42	15	23	13	65	28			20	13	15	7	35	20
09:00			36	20	28	10	64	30			16	17	13	5	29	22
09:15			23	16	22	12	45	28			17	22	15	6	32	28
09:30			20	19	13	5	33	24			19	12	11	3	30	15
09:45			41	10	14	11	55	21			15	13	20	8	35	21
10:00			8	14	39	11	47	25			18	21	14	10	32	31
10:15			1	16	43	8	44	24			19	10	14	4	33	14
10:30			26	12	12	3	38	15			21	10	13	3	34	13
10:45			16	11	26	4	42	15			24	14	22	1	46	15
11:00			27	11	15	5	42	16			25	13	24	5	49	18
11:15			21	5	18	3	39	8			6	13	32	7	38	20
11:30			30	6	23	2	53	8			0	7	46	5	46	12
11:45			36	9	18	3	54	12			3	14	39	8	42	22
Total Day			618	1208	445	927	1063	2135			346	954	405	595	751	1549
Total			1826		1372		3198				1300		1000		2300	
% Total			19.3%	37.8%	13.9%	29.0%					15.0%	41.5%	17.6%	25.9%		
Peak			08:00	00:15	10:00	03:15	08:15	03:45			10:15	01:30	11:00	01:15	10:45	01:15
Vol.			161	141	120	145	242	260			89	125	141	93	179	213
P.H.F.			0.839	0.881	0.698	0.929	0.890	0.855			0.890	0.822	0.766	0.750	0.913	0.934

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GOROVE/SLADE ASSOCIATES INC.  
 1175 HERNDON PARKWAY STE. 600  
 HERNDON, VIRGINIA 20170

CARRHOMES, PRESCOTT  
 ALEXANDRIA, VIRGINIA

Start Time	23-May-04 Sun				24-May-04 Mon							
	Northbound		Southbound		Northbound		Southbound		Combined			
	A.M.	P.M.	A.M.	P.M.	A.M.	P.M.	A.M.	P.M.	A.M.	P.M.		
12:00	9	16	16	5	25	21	3	26	1	10	4	36
12:15	11	14	5	15	16	29	1	25	1	19	2	44
12:30	3	18	6	13	9	31	1	22	0	19	1	41
12:45	9	19	6	13	15	32	2	34	2	31	4	65
01:00	4	16	4	17	8	33	1	33	0	18	1	51
01:15	13	17	3	22	16	39	3	28	1	15	4	43
01:30	4	19	2	10	6	29	3	32	1	20	4	52
01:45	8	12	1	17	9	29	1	32	0	22	1	54
02:00	5	12	1	33	6	45	7	24	1	15	8	39
02:15	6	10	4	14	10	24	0	18	3	31	3	49
02:30	1	27	0	11	1	38	0	15	0	18	0	33
02:45	2	18	0	19	2	37	0	22	0	7	0	29
03:00	2	0	2	35	4	35	0	20	2	12	2	32
03:15	0	0	0	35	0	35	0	15	1	27	1	42
03:30	1	0	1	21	2	21	0	24	0	21	0	45
03:45	1	0	0	21	1	21	0	22	0	24	0	46
04:00	0	0	0	26	0	26	3	27	0	29	3	56
04:15	4	0	0	20	4	20	1	23	0	36	1	59
04:30	0	0	2	26	2	26	7	21	2	26	9	47
04:45	3	0	1	29	4	29	0	28	1	21	1	49
05:00	4	0	4	23	8	23	1	42	0	38	1	80
05:15	0	0	1	27	1	27	3	44	1	51	4	95
05:30	4	0	1	22	5	22	2	45	3	45	5	90
05:45	1	0	2	19	3	19	6	44	5	40	11	84
06:00	4	0	4	25	8	25	3	25	3	20	6	45
06:15	1	0	0	25	1	25	9	11	6	13	15	24
06:30	2	0	1	22	3	22	13	25	4	30	17	55
06:45	2	14	4	10	6	24	15	16	5	13	20	29
07:00	3	12	4	16	7	28	24	14	5	20	29	34
07:15	1	9	1	4	2	13	23	19	22	15	45	34
07:30	3	12	5	6	8	18	21	23	11	12	32	35
07:45	5	9	1	8	6	17	36	21	24	16	60	37
08:00	5	14	3	10	8	24	24	12	12	11	36	23
08:15	5	7	3	13	8	20	31	14	25	13	56	27
08:30	10	9	3	4	13	13	39	12	24	9	63	21
08:45	7	7	6	4	13	11	28	22	25	12	53	34
09:00	3	9	2	3	5	12	36	14	29	4	65	18
09:15	7	11	7	10	14	21	21	12	23	5	44	17
09:30	7	8	7	4	14	12	24	6	19	5	43	11
09:45	11	7	13	4	24	11	21	6	19	2	40	8
10:00	16	7	15	1	31	8	22	4	20	4	42	8
10:15	14	6	2	5	16	11	19	4	17	4	36	8
10:30	13	4	5	3	18	7	18	8	14	2	32	10
10:45	21	4	17	1	38	5	18	1	16	1	34	2
11:00	22	3	12	1	34	4	20	5	15	1	35	6
11:15	20	3	12	1	32	4	27	4	13	5	40	9
11:30	23	2	11	3	34	5	26	3	25	2	51	5
11:45	9	5	16	3	25	8	20	0	17	2	37	2
Total	309	360	216	679	525	1039	583	947	418	816	1001	1763
Day Total	669		895		1564		1530		1234		2764	
% Total	19.8%	23.0%	13.8%	43.4%			21.1%	34.3%	15.1%	29.5%		
Peak	10:45	00:45	10:45	03:00	10:45	02:30	08:15	05:00	08:15	05:00	08:15	05:00
Vol.	86	71	52	112	138	145	134	175	103	174	237	349
P.H.F.	0.935	0.657	0.765	0.800	0.908	0.806	0.859	0.972	0.888	0.853	0.912	0.918

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**GOROVE/SLADE ASSOCIATES INC.**  
**1175 HERNDON PARKWAY STE. 600**  
**HERNDON, VIRGINIA 20170**

**CARRHOMES, PRESCOTT**  
**ALEXANDRIA, VIRGINIA**

Start Time	25-May-04 Tue		26-May-04 Wed		26-May-04 Wed		26-May-04 Wed		26-May-04 Wed		26-May-04 Wed		26-May-04 Wed	
	Northbound		Southbound		Combined		Northbound		Southbound		Combined		Combined	
	A.M.	P.M.	A.M.	P.M.	A.M.	P.M.	A.M.	P.M.	A.M.	P.M.	A.M.	P.M.	A.M.	P.M.
12:00	4	37	0	25	4	62	*	*	*	*	*	*	*	*
12:15	4	36	2	23	6	59	*	*	*	*	*	*	*	*
12:30	0	40	1	27	1	67	*	*	*	*	*	*	*	*
12:45	1	40	0	20	1	60	*	*	*	*	*	*	*	*
01:00	3	46	0	19	3	65	*	*	*	*	*	*	*	*
01:15	1	32	1	18	2	50	*	*	*	*	*	*	*	*
01:30	1	38	0	24	1	62	*	*	*	*	*	*	*	*
01:45	2	31	1	19	3	50	*	*	*	*	*	*	*	*
02:00	6	41	2	18	8	59	*	*	*	*	*	*	*	*
02:15	2	*	1	*	3	*	*	*	*	*	*	*	*	*
02:30	1	*	0	*	1	*	*	*	*	*	*	*	*	*
02:45	0	*	0	*	0	*	*	*	*	*	*	*	*	*
03:00	1	*	0	*	1	*	*	*	*	*	*	*	*	*
03:15	0	*	0	*	0	*	*	*	*	*	*	*	*	*
03:30	0	*	1	*	1	*	*	*	*	*	*	*	*	*
03:45	0	*	1	*	1	*	*	*	*	*	*	*	*	*
04:00	2	*	1	*	3	*	*	*	*	*	*	*	*	*
04:15	1	*	0	*	1	*	*	*	*	*	*	*	*	*
04:30	5	*	1	*	6	*	*	*	*	*	*	*	*	*
04:45	2	*	4	*	6	*	*	*	*	*	*	*	*	*
05:00	2	*	1	*	3	*	*	*	*	*	*	*	*	*
05:15	3	*	0	*	3	*	*	*	*	*	*	*	*	*
05:30	4	*	4	*	8	*	*	*	*	*	*	*	*	*
05:45	1	*	3	*	4	*	*	*	*	*	*	*	*	*
06:00	7	*	7	*	14	*	*	*	*	*	*	*	*	*
06:15	7	*	6	*	13	*	*	*	*	*	*	*	*	*
06:30	9	*	4	*	13	*	*	*	*	*	*	*	*	*
06:45	13	*	5	*	18	*	*	*	*	*	*	*	*	*
07:00	23	*	6	*	29	*	*	*	*	*	*	*	*	*
07:15	22	*	8	*	30	*	*	*	*	*	*	*	*	*
07:30	26	*	8	*	34	*	*	*	*	*	*	*	*	*
07:45	27	*	15	*	42	*	*	*	*	*	*	*	*	*
08:00	28	*	28	*	56	*	*	*	*	*	*	*	*	*
08:15	24	*	19	*	43	*	*	*	*	*	*	*	*	*
08:30	26	*	40	*	66	*	*	*	*	*	*	*	*	*
08:45	40	*	20	*	60	*	*	*	*	*	*	*	*	*
09:00	45	*	20	*	65	*	*	*	*	*	*	*	*	*
09:15	20	*	26	*	46	*	*	*	*	*	*	*	*	*
09:30	28	*	35	*	63	*	*	*	*	*	*	*	*	*
09:45	24	*	16	*	40	*	*	*	*	*	*	*	*	*
10:00	34	*	16	*	50	*	*	*	*	*	*	*	*	*
10:15	25	*	11	*	36	*	*	*	*	*	*	*	*	*
10:30	27	*	16	*	43	*	*	*	*	*	*	*	*	*
10:45	24	*	14	*	38	*	*	*	*	*	*	*	*	*
11:00	15	*	16	*	31	*	*	*	*	*	*	*	*	*
11:15	24	*	13	*	37	*	*	*	*	*	*	*	*	*
11:30	33	*	17	*	50	*	*	*	*	*	*	*	*	*
11:45	36	*	16	*	52	*	*	*	*	*	*	*	*	*
Total	633	341	406	193	1039	534	0	0	0	0	0	0	0	0
Day Total	974		599		1573		0	0	0	0	0	0	0	0
% Total	40.2%	21.7%	25.8%	12.3%			0.0%	0.0%	0.0%	0.0%				
Peak	08:15	00:15	08:00	12:00	08:30	00:15								
Vol.	135	162	107	95	237	251								
P.H.F.	0.750	0.880	0.669	0.880	0.898	0.937								
ADT	Not Calculated													

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The Prescott  
August 12, 2004  
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**APPENDIX B**  
**Intersection Capacity Analysis –**  
**Existing Condition**

ALL-WAY STOP CONTROL ANALYSIS									
General Information					Site Information				
Analyst	CLF				Intersection	North Fayette St. & Cameron St			
Agency/Co.	Gorove/Slade Associates				Jurisdiction	City of Alexandria			
Date Performed	5/26/2004				Analysis Year	Existing 2004			
Analysis Time Period	AMPH								
Project ID 1789-006 Carrhomes Prescott TIA									
East/West Street: Cameron Street					North/South Street: North Fayette St.				
Volume Adjustments and Site Characteristics									
Approach	Eastbound					Westbound			
Movement	L	T	R	L	T	R			
Volume	0	0	0	14	406	31			
%Thrus Left Lane	50			50					
Approach	Northbound					Southbound			
Movement	L	T	R	L	T	R			
Volume	15	125	0	0	67	42			
%Thrus Left Lane	50			50					
	Eastbound		Westbound		Northbound		Southbound		
	L1	L2	L1	L2	L1	L2	L1	L2	
Configuration			LT	TR	LT		TR		
PHF			1.00	1.00	1.00		1.00		
Flow Rate			217	234	140		109		
% Heavy Vehicles									
No. Lanes	0		2		1		1		
Geometry Group			1		2		2		
Duration, T	0.25								
Saturation Headway Adjustment Worksheet									
Prop. Left-Turns			0.1	0.0	0.1		0.0		
Prop. Right-Turns			0.0	0.1	0.0		0.4		
Prop. Heavy Vehicle									
hLT-adj			0.2	0.2	0.2	0.2	0.2	0.2	0.2
hRT-adj			-0.6	-0.6	-0.6	-0.6	-0.6	-0.6	-0.6
hHV-adj			1.7	1.7	1.7	1.7	1.7	1.7	1.7
hadj, computed			0.00	0.00	0.00		0.00		
Departure Headway and Service Time									
hd, initial value			3.20	3.20	3.20		3.20		
x, initial			0.19	0.21	0.12		0.10		
hd, final value			0.00	0.00	0.00		0.00		
x, final value			0.27	0.29	0.19		0.15		
Move-up time, m			2.0		2.0		2.0		
Service Time									
Capacity and Level of Service									
	Eastbound		Westbound		Northbound		Southbound		
	L1	L2	L1	L2	L1	L2	L1	L2	
Capacity			467	484	390		359		
Delay			9.25	9.26	9.20		8.61		
LOS			A	A	A		A		
Approach: Delay			9.26		9.20		8.61		
LOS			A		A		A		
Intersection Delay	9.14								
Intersection LOS	A								

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ALL-WAY STOP CONTROL ANALYSIS								
General Information				Site Information				
Analyst	CLF			Intersection	North Fayette St. & Cameron St.			
Agency/Co.	Gorove/Slade Associates			Jurisdiction	City of Alexandria			
Date Performed	5/26/2004			Analysis Year	Existing 2004			
Analysis Time Period	PMPH							
Project ID 1789-006 Carrhomes Prescott TIA								
East/West Street: Cameron Street				North/South Street: North Fayette St.				
Volume Adjustments and Site Characteristics								
Approach	Eastbound			Westbound				
	L	T	R	L	T	R		
Movement								
Volume	0	0	0	74	615	86		
%Thrus Left Lane	50			50				
Approach	Northbound			Southbound				
	L	T	R	L	T	R		
Movement								
Volume	21	83	0	0	230	71		
%Thrus Left Lane	50			50				
	Eastbound		Westbound		Northbound		Southbound	
	L1	L2	L1	L2	L1	L2	L1	L2
Configuration			LT	TR	LT		TR	
PHF			1.00	1.00	1.00		1.00	
Flow Rate			381	394	104		301	
% Heavy Vehicles								
No. Lanes	0		2		1		1	
Geometry Group			1		2		2	
Duration, T	0.25							
Saturation Headway Adjustment Worksheet								
Prop. Left-Turns			0.2	0.0	0.2		0.0	
Prop. Right-Turns			0.0	0.2	0.0		0.2	
Prop. Heavy Vehicle								
hLT-adj			0.2	0.2	0.2	0.2	0.2	0.2
hRT-adj			-0.6	-0.6	-0.6	-0.6	-0.6	-0.6
hHV-adj			1.7	1.7	1.7	1.7	1.7	1.7
hadj, computed			0.00	0.00	0.00		0.00	
Departure Headway and Service Time								
hd, initial value			3.20	3.20	3.20		3.20	
x, initial			0.34	0.35	0.09		0.27	
hd, final value			0.00	0.00	0.00		0.00	
x, final value			0.54	0.54	0.17		0.46	
Move-up time, m			2.0		2.0		2.0	
Service Time								
Capacity and Level of Service								
	Eastbound		Westbound		Northbound		Southbound	
	L1	L2	L1	L2	L1	L2	L1	L2
Capacity			631	644	354		551	
Delay			13.92	13.55	10.25		12.99	
LOS			B	B	B		B	
Approach: Delay			13.73		10.25		12.99	
LOS			B		B		B	
Intersection Delay	13.24							
Intersection LOS	B							

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ALL-WAY STOP CONTROL ANALYSIS										
General Information					Site Information					
Analyst	CLF				Intersection	North Fayette St. & Queen St.				
Agency/Co.	Gorove/Slade Associates				Jurisdiction	City of Alexandria				
Date Performed	5/26/2004				Analysis Year	Existing 2004				
Analysis Time Period	AMPH									
Project ID 1789-006 Carrhomes Prescott TIA										
East/West Street: Queen Street					North/South Street: North Fayette St.					
Volume Adjustments and Site Characteristics										
Approach	Eastbound			Westbound			Southbound			
	L	T	R	L	T	R	L	T	R	
Movement										
Volume	14	163	23	0	0	0				
%Thrus Left Lane	50			50						
Approach	Northbound			Southbound			Westbound			
	L	T	R	L	T	R	L	T	R	
Movement										
Volume	0	104	62	10	88	0				
%Thrus Left Lane	50			50						
	Eastbound		Westbound		Northbound		Southbound			
	L1	L2	L1	L2	L1	L2	L1	L2		
Configuration	LTR				TR		LT			
PHF	1.00				1.00		1.00			
Flow Rate	200				166		98			
% Heavy Vehicles										
No. Lanes	1		0		1		1			
Geometry Group	1				1		1			
Duration, T					0.25					
Saturation Headway Adjustment Worksheet										
Prop. Left-Turns	0.1				0.0		0.1			
Prop. Right-Turns	0.1				0.4		0.0			
Prop. Heavy Vehicle										
hLT-adj	0.2	0.2			0.2	0.2	0.2	0.2		
hRT-adj	-0.6	-0.6			-0.6	-0.6	-0.6	-0.6		
hHV-adj	1.7	1.7			1.7	1.7	1.7	1.7		
hadj, computed	4.45				4.45		4.45			
Departure Headway and Service Time										
hd, initial value	3.20				3.20		3.20			
x, initial	0.18				0.15		0.09			
hd, final value	4.45				4.45		4.45			
x, final value	0.25				0.20		0.12			
Move-up time, m	2.0				2.0		2.0			
Service Time	2.4		2.4		2.4		2.4			
Capacity and Level of Service										
	Eastbound		Westbound		Northbound		Southbound			
	L1	L2	L1	L2	L1	L2	L1	L2		
Capacity	450				416		348			
Delay	8.90				8.32		8.24			
LOS	A				A		A			
Approach: Delay	8.90				8.32		8.24			
LOS	A				A		A			
Intersection Delay					8.56					
Intersection LOS					A					

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ALL-WAY STOP CONTROL ANALYSIS									
<b>General Information</b>					<b>Site Information</b>				
Analyst	CLF				Intersection	North Fayette St. & Queen St.			
Agency/Co.	Gorove/Slade Associates				Jurisdiction	City of Alexandria			
Date Performed	5/26/2004				Analysis Year	Existing 2004			
Analysis Time Period	AMPH								
Project ID 1789-006 Carrhomes Prescott TIA									
East/West Street: Queen Street					North/South Street: North Fayette St.				
<b>Volume Adjustments and Site Characteristics</b>									
Approach	Eastbound					Westbound			
Movement	L	T	R	L	T	R			
Volume	16	245	36	0	0	0			
%Thrus Left Lane	50			50					
Approach	Northbound					Southbound			
Movement	L	T	R	L	T	R			
Volume	0	64	104	34	271	0			
%Thrus Left Lane	50			50					
	Eastbound		Westbound		Northbound		Southbound		
	L1	L2	L1	L2	L1	L2	L1	L2	
Configuration	LTR				TR		LT		
PHF	1.00				1.00		1.00		
Flow Rate	297				168		305		
% Heavy Vehicles									
No. Lanes	1		0		1		1		
Geometry Group	1				1		1		
Duration, T					0.25				
<b>Saturation Headway Adjustment Worksheet</b>									
Prop. Left-Turns	0.1				0.0		0.1		
Prop. Right-Turns	0.1				0.6		0.0		
Prop. Heavy Vehicle									
hLT-adj	0.2	0.2			0.2	0.2	0.2	0.2	
hRT-adj	-0.6	-0.6			-0.6	-0.6	-0.6	-0.6	
hHV-adj	1.7	1.7			1.7	1.7	1.7	1.7	
hadj, computed	5.01				5.01		5.01		
<b>Departure Headway and Service Time</b>									
hd, initial value	3.20				3.20		3.20		
x, initial	0.26				0.15		0.27		
hd, final value	5.01				5.01		5.01		
x, final value	0.41				0.22		0.42		
Move-up time, m	2.0				2.0		2.0		
Service Time	3.0		3.0		3.0		3.0		
<b>Capacity and Level of Service</b>									
	Eastbound		Westbound		Northbound		Southbound		
	L1	L2	L1	L2	L1	L2	L1	L2	
Capacity	547				418		555		
Delay	11.50				9.11		11.50		
LOS	B				A		B		
Approach: Delay	11.50				9.11		11.50		
LOS	B				A		B		
Intersection Delay					10.98				
Intersection LOS					B				

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**APPENDIX C**  
**Intersection Capacity Analysis –**  
**Total Future Condition**

ALL-WAY STOP CONTROL ANALYSIS										
General Information					Site Information					
Analyst	CLF				Intersection	North Fayette St. & Cameron St				
Agency/Co.	Gorove/Slade Associates				Jurisdiction	City of Alexandria				
Date Performed	5/26/2004				Analysis Year	Total Future 2006				
Analysis Time Period	AMPH									
Project ID 1789-006 Carrhomes Prescott TIA										
East/West Street: Cameron Street					North/South Street: North Fayette St.					
Volume Adjustments and Site Characteristics										
Approach	Eastbound					Westbound				
Movement	L	T	R	L	T	R				
Volume	0	0	0	15	431	38				
%Thrus Left Lane	50			50						
Approach	Northbound					Southbound				
Movement	L	T	R	L	T	R				
Volume	16	134	0	0	74	48				
%Thrus Left Lane	50			50						
	Eastbound		Westbound		Northbound		Southbound			
	L1	L2	L1	L2	L1	L2	L1	L2		
Configuration			LT	TR	LT		TR			
PHF			1.00	1.00	1.00		1.00			
Flow Rate			230	254	150		122			
% Heavy Vehicles										
No. Lanes	0		2		1		1			
Geometry Group			1		2		2			
Duration, T	0.25									
Saturation Headway Adjustment Worksheet										
Prop. Left-Turns			0.1	0.0	0.1		0.0			
Prop. Right-Turns			0.0	0.1	0.0		0.4			
Prop. Heavy Vehicle										
hLT-adj			0.2	0.2	0.2	0.2	0.2	0.2	0.2	
hRT-adj			-0.6	-0.6	-0.6	-0.6	-0.6	-0.6	-0.6	
hHV-adj			1.7	1.7	1.7	1.7	1.7	1.7	1.7	
hadj, computed			0.00	0.00	0.00		0.00			
Departure Headway and Service Time										
hd, initial value			3.20	3.20	3.20		3.20			
x, initial			0.20	0.23	0.13		0.11			
hd, final value			0.00	0.00	0.00		0.00			
x, final value			0.29	0.32	0.21		0.17			
Move-up time, m			2.0		2.0		2.0			
Service Time										
Capacity and Level of Service										
	Eastbound		Westbound		Northbound		Southbound			
	L1	L2	L1	L2	L1	L2	L1	L2		
Capacity			480	504	400		372			
Delay			9.53	9.60	9.45		8.84			
LOS			A	A	A		A			
Approach: Delay			9.57		9.45		8.84			
LOS			A		A		A			
Intersection Delay	9.43									
Intersection LOS	A									

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ALL-WAY STOP CONTROL ANALYSIS										
General Information					Site Information					
Analyst	CLF				Intersection	North Fayette St. & Cameron St				
Agency/Co.	Gorove/Slade Associates				Jurisdiction	City of Alexandria				
Date Performed	5/26/2004				Analysis Year	Total Future 2006				
Analysis Time Period	PMPH									
Project ID 1789-006 Carrhomes Prescott TIA										
East/West Street: Cameron Street					North/South Street: North Fayette St.					
Volume Adjustments and Site Characteristics										
Approach	Eastbound					Westbound				
	L	T	R	L	T	R				
Movement										
Volume	0	0	0	79	652	110				
%Thrus Left Lane	50			50						
Approach	Northbound					Southbound				
	L	T	R	L	T	R				
Movement										
Volume	22	91	0	0	245	76				
%Thrus Left Lane	50			50						
	Eastbound		Westbound		Northbound		Southbound			
	L1	L2	L1	L2	L1	L2	L1	L2		
Configuration			LT	TR	LT		TR			
PHF			1.00	1.00	1.00		1.00			
Flow Rate			405	436	113		321			
% Heavy Vehicles										
No. Lanes	0		2		1		1			
Geometry Group			1		2		2			
Duration, T	0.25									
Saturation Headway Adjustment Worksheet										
Prop. Left-Turns			0.2	0.0	0.2		0.0			
Prop. Right-Turns			0.0	0.3	0.0		0.2			
Prop. Heavy Vehicle										
hLT-adj			0.2	0.2	0.2	0.2	0.2	0.2	0.2	
hRT-adj			-0.6	-0.6	-0.6	-0.6	-0.6	-0.6	-0.6	
hHV-adj			1.7	1.7	1.7	1.7	1.7	1.7	1.7	
hadj, computed			0.00	0.00	0.00		0.00			
Departure Headway and Service Time										
hd, initial value			3.20	3.20	3.20		3.20			
x, initial			0.36	0.39	0.10		0.29			
hd, final value			0.00	0.00	0.00		0.00			
x, final value			0.59	0.61	0.19		0.50			
Move-up time, m			2.0		2.0		2.0			
Service Time										
Capacity and Level of Service										
	Eastbound		Westbound		Northbound		Southbound			
	L1	L2	L1	L2	L1	L2	L1	L2		
Capacity			655	686	363		571			
Delay			15.30	15.46	10.62		14.02			
LOS			C	C	B		B			
Approach: Delay			15.38		10.62		14.02			
LOS			C		B		B			
Intersection Delay	14.62									
Intersection LOS	B									

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ALL-WAY STOP CONTROL ANALYSIS									
General Information					Site Information				
Analyst	CLF				Intersection	North Fayette St. & Queen St.			
Agency/Co.	Gorove/Slade Associates				Jurisdiction	City of Alexandria			
Date Performed	5/26/2004				Analysis Year	Total Future 2006			
Analysis Time Period	AMPH								
Project ID 1789-006 Carrhomes Prescott TIA									
East/West Street: Queen Street					North/South Street: North Fayette St.				
Volume Adjustments and Site Characteristics									
Approach	Eastbound					Westbound			
	L	T	R	L	T	R			
	15	173	25	0	0	0			
Volume									
%Thrus Left Lane	50					50			
Approach	Northbound					Southbound			
	L	T	R	L	T	R			
	0	113	87	11	94	0			
Volume									
%Thrus Left Lane	50					50			
Configuration	Eastbound		Westbound		Northbound		Southbound		
	L1	L2	L1	L2	L1	L2	L1	L2	
	LTR				TR		LT		
	1.00				1.00		1.00		
	213				200		105		
	1		0		1		1		
	1				1		1		
					0.25				
Saturation Headway Adjustment Worksheet									
Prop. Left-Turns	0.1				0.0			0.1	
Prop. Right-Turns	0.1				0.4			0.0	
Prop. Heavy Vehicle									
hLT-adj	0.2	0.2			0.2	0.2		0.2	0.2
hRT-adj	-0.6	-0.6			-0.6	-0.6		-0.6	-0.6
hHV-adj	1.7	1.7			1.7	1.7		1.7	1.7
hadj, computed	4.54				4.54			4.54	
Departure Headway and Service Time									
hd, initial value	3.20				3.20			3.20	
xs, initial	0.19				0.18			0.09	
hd, final value	4.54				4.54			4.54	
xs, final value	0.27				0.24			0.14	
Move-up time, m	2.0				2.0		2.0		
Service Time	2.5		2.5		2.5		2.5		
Capacity and Level of Service									
	Eastbound		Westbound		Northbound		Southbound		
	L1	L2	L1	L2	L1	L2	L1	L2	
Capacity	463				450		355		
Delay	9.20				8.63		8.41		
LOS	A				A		A		
Approach: Delay	9.20				8.63		8.41		
LOS	A				A		A		
Intersection Delay					8.82				
Intersection LOS					A				

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ALL-WAY STOP CONTROL ANALYSIS								
General Information				Site Information				
Analyst	CLF			Intersection	North Fayette St. & Queen St.			
Agency/Co.	Gorove/Slade Associates			Jurisdiction	City of Alexandria			
Date Performed	5/26/2004			Analysis Year	Total Future 2006			
Analysis Time Period	AMPH							
Project ID 1789-006 Carrhomes Prescott TIA								
East/West Street: Queen Street				North/South Street: North Fayette St.				
Volume Adjustments and Site Characteristics								
Approach	Eastbound			Westbound				
	L	T	R	L	T	R		
Movement								
Volume	17	260	41	0	0	0		
%Thrus Left Lane	50			50				
Approach	Northbound			Southbound				
	L	T	R	L	T	R		
Movement								
Volume	0	69	119	36	292	0		
%Thrus Left Lane	50			50				
	Eastbound		Westbound		Northbound		Southbound	
	L1	L2	L1	L2	L1	L2	L1	L2
Configuration	LTR				TR		LT	
PHF	1.00				1.00		1.00	
Flow Rate	318				188		328	
% Heavy Vehicles								
No. Lanes	1		0		1		1	
Geometry Group	1				1		1	
Duration, T					0.25			
Saturation Headway Adjustment Worksheet								
Prop. Left-Turns	0.1				0.0		0.1	
Prop. Right-Turns	0.1				0.6		0.0	
Prop. Heavy Vehicle								
hLT-adj	0.2	0.2			0.2	0.2	0.2	0.2
hRT-adj	-0.6	-0.6			-0.6	-0.6	-0.6	-0.6
hHV-adj	1.7	1.7			1.7	1.7	1.7	1.7
hadj, computed	5.14				5.14		5.14	
Departure Headway and Service Time								
hd, initial value	3.20				3.20		3.20	
xs, initial	0.28				0.17		0.29	
hd, final value	5.14				5.14		5.14	
xs, final value	0.45				0.25		0.46	
Move-up time, m	2.0				2.0		2.0	
Service Time	3.1		3.1		3.1		3.1	
Capacity and Level of Service								
	Eastbound		Westbound		Northbound		Southbound	
	L1	L2	L1	L2	L1	L2	L1	L2
Capacity	568				438		578	
Delay	12.33				9.54		12.36	
LOS	B				A		B	
Approach: Delay	12.33				9.54		12.36	
LOS	B				A		B	
Intersection Delay					11.71			
Intersection LOS					B			

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TWO-WAY STOP CONTROL SUMMARY								
<b>General Information</b>				<b>Site Information</b>				
Analyst	CLF			Intersection	N Fayette and Site Entrance			
Agency/Co.	Gorove/Slade Associates			Jurisdiction	Alexandria, VA			
Date Performed	6/1/2004			Analysis Year	Total Future 2006			
Analysis Time Period	AMPH							
Project Description 1789-006 Prescott								
East/West Street: Site Entrance				North/South Street: N Fayette St				
Intersection Orientation: North-South				Study Period (hrs): 0.25				
Vehicle Volumes and Adjustments								
Major Street	Northbound			Southbound				
Movement	1	2	3	4	5	6		
	L	T	R	L	T	R		
Volume	0	171	5	2	117	0		
Peak-Hour Factor, PHF	1.00	1.00	1.00	1.00	1.00	1.00		
Hourly Flow Rate, HFR	0	171	5	2	117	0		
Percent Heavy Vehicles	0	--	--	0	--	--		
Median Type	Undivided							
RT Channelized			0			0		
Lanes	0	1	0	0	1	0		
Configuration			TR	LT				
Upstream Signal		0			0			
Minor Street	Westbound			Eastbound				
Movement	7	8	9	10	11	12		
	L	T	R	L	T	R		
Volume	6	0	24	0	0	0		
Peak-Hour Factor, PHF	1.00	1.00	1.00	1.00	1.00	1.00		
Hourly Flow Rate, HFR	6	0	24	0	0	0		
Percent Heavy Vehicles	0	0	0	0	0	0		
Percent Grade (%)	0			0				
Flared Approach		N			N			
Storage		0			0			
RT Channelized			0			0		
Lanes	0	0	0	0	0	0		
Configuration		LR						
Delay, Queue Length, and Level of Service								
Approach	NB	SB	Westbound			Eastbound		
Movement	1	4	7	8	9	10	11	12
Lane Configuration		LT		LR				
v (vph)		2		30				
C (m) (vph)		1412		833				
v/c		0.00		0.04				
95% queue length		0.00		0.11				
Control Delay		7.6		9.5				
LOS		A		A				
Approach Delay	--	--	9.5					
Approach LOS	--	--	A					

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TWO-WAY STOP CONTROL SUMMARY								
<b>General Information</b>				<b>Site Information</b>				
Analyst	CLF			Intersection	N Fayette and Site Entrance			
Agency/Co.	Gorove/Slade Associates			Jurisdiction	Alexandria, VA			
Date Performed	6/1/2004			Analysis Year	Total Future 2006			
Analysis Time Period	PMPH							
Project Description 1789-006 Prescott								
East/West Street: Site Entrance				North/South Street: N Fayette St				
Intersection Orientation: North-South				Study Period (hrs): 0.25				
Vehicle Volumes and Adjustments								
Major Street	Northbound			Southbound				
Movement	1	2	3	4	5	6		
	L	T	R	L	T	R		
Volume	0	179	22	7	323	0		
Peak-Hour Factor, PHF	1.00	1.00	1.00	1.00	1.00	1.00		
Hourly Flow Rate, HFR	0	179	22	7	323	0		
Percent Heavy Vehicles	0	--	--	0	--	--		
Median Type	Undivided							
RT Channelized			0			0		
Lanes	0	1	0	0	1	0		
Configuration			TR	LT				
Upstream Signal		0			0			
Minor Street	Westbound			Eastbound				
Movement	7	8	9	10	11	12		
	L	T	R	L	T	R		
Volume	3	0	10	0	0	0		
Peak-Hour Factor, PHF	1.00	1.00	1.00	1.00	1.00	1.00		
Hourly Flow Rate, HFR	3	0	10	0	0	0		
Percent Heavy Vehicles	0	0	0	0	0	0		
Percent Grade (%)	0			0				
Flared Approach		N			N			
Storage		0			0			
RT Channelized			0			0		
Lanes	0	0	0	0	0	0		
Configuration		LR						
Delay, Queue Length, and Level of Service								
Approach	NB	SB	Westbound			Eastbound		
Movement	1	4	7	8	9	10	11	12
Lane Configuration		LT		LR				
v (vph)		7		13				
C (m) (vph)		1383		742				
v/c		0.01		0.02				
95% queue length		0.02		0.05				
Control Delay		7.6		9.9				
LOS		A		A				
Approach Delay	--	--	9.9					
Approach LOS	--	--	A					

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**APPENDIX D**  
**Intersection Capacity Analysis –**  
**Total Future +10 Years Condition**

ALL-WAY STOP CONTROL ANALYSIS									
<b>General Information</b>					<b>Site Information</b>				
Analyst	CLF				Intersection	North Fayette St. & Cameron St			
Agency/Co.	Gorove/Slade Associates				Jurisdiction	City of Alexandria			
Date Performed	5/26/2004				Analysis Year	Total Future+10 2016			
Analysis Time Period	AMPH								
Project ID 1789-006 Carrhomes Prescott TIA									
East/West Street: Cameron Street					North/South Street: North Fayette St.				
<b>Volume Adjustments and Site Characteristics</b>									
Approach			Eastbound			Westbound			
Movement	L	T	R	L	T	R			
Volume	0	0	0	20	579	49			
%Thrus Left Lane	50			50					
Approach			Northbound			Southbound			
Movement	L	T	R	L	T	R			
Volume	21	179	0	0	99	63			
%Thrus Left Lane	50			50					
		Eastbound		Westbound		Northbound		Southbound	
		L1	L2	L1	L2	L1	L2	L1	L2
Configuration				LT	TR	LT		TR	
PHF				1.00	1.00	1.00		1.00	
Flow Rate				309	339	200		162	
% Heavy Vehicles									
No. Lanes		0		2		1		1	
Geometry Group				1		2		2	
Duration, T						0.25			
<b>Saturation Headway Adjustment Worksheet</b>									
Prop. Left-Turns				0.1	0.0	0.1		0.0	
Prop. Right-Turns				0.0	0.1	0.0		0.4	
Prop. Heavy Vehicle									
hLT-adj				0.2	0.2	0.2	0.2	0.2	0.2
hRT-adj				-0.6	-0.6	-0.6	-0.6	-0.6	-0.6
hHV-adj				1.7	1.7	1.7	1.7	1.7	1.7
hadj, computed				0.00	0.00	0.00		0.00	
<b>Departure Headway and Service Time</b>									
hd, initial value				3.20	3.20	3.20		3.20	
x, initial				0.27	0.30	0.18		0.14	
hd, final value				0.00	0.00	0.00		0.00	
x, final value				0.42	0.45	0.31		0.24	
Move-up time, m				2.0		2.0		2.0	
Service Time									
<b>Capacity and Level of Service</b>									
		Eastbound		Westbound		Northbound		Southbound	
		L1	L2	L1	L2	L1	L2	L1	L2
Capacity				559	589	450		412	
Delay				11.45	11.74	10.91		9.99	
LOS				B	B	B		A	
Approach: Delay				11.60		10.91		9.99	
LOS				B		B		A	
Intersection Delay						11.21			
Intersection LOS						B			

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ALL-WAY STOP CONTROL ANALYSIS									
<b>General Information</b>					<b>Site Information</b>				
Analyst	CLF				Intersection	North Fayette St. & Cameron St			
Agency/Co.	Gorove/Slade Associates				Jurisdiction	City of Alexandria			
Date Performed	5/26/2004				Analysis Year	Total Future+10 2016			
Analysis Time Period	PMPH								
Project ID 1789-006 Carrhomes Prescott TIA									
East/West Street: Cameron Street					North/South Street: North Fayette St.				
<b>Volume Adjustments and Site Characteristics</b>									
Approach			Eastbound			Westbound			
Movement	L	T	R	L	T	R			
Volume	0	0	0	106	877	142			
%Thrus Left Lane	50			50					
Approach			Northbound			Southbound			
Movement	L	T	R	L	T	R			
Volume	30	121	0	0	329	102			
%Thrus Left Lane	50			50					
		Eastbound		Westbound		Northbound		Southbound	
		L1	L2	L1	L2	L1	L2	L1	L2
Configuration			LT	TR	LT		TR		
PHF			1.00	1.00	1.00		1.00		
Flow Rate			544	581	151		431		
% Heavy Vehicles									
No. Lanes	0		2		1		1		
Geometry Group			1		2		2		
Duration, T					0.25				
<b>Saturation Headway Adjustment Worksheet</b>									
Prop. Left-Turns			0.2	0.0	0.2		0.0		
Prop. Right-Turns			0.0	0.2	0.0		0.2		
Prop. Heavy Vehicle									
hLT-adj			0.2	0.2	0.2	0.2	0.2	0.2	
hRT-adj			-0.6	-0.6	-0.6	-0.6	-0.6	-0.6	
hHV-adj			1.7	1.7	1.7	1.7	1.7	1.7	
hadj, computed			0.00	0.00	0.00		0.00		
<b>Departure Headway and Service Time</b>									
hd, initial value			3.20	3.20	3.20		3.20		
x, initial			0.48	0.52	0.13		0.38		
hd, final value			0.00	0.00	0.00		0.00		
x, final value			0.87	0.90	0.28		0.72		
Move-up time, m			2.0		2.0		2.0		
Service Time									
<b>Capacity and Level of Service</b>									
		Eastbound		Westbound		Northbound		Southbound	
		L1	L2	L1	L2	L1	L2	L1	L2
Capacity			627	648	401			595	
Delay			34.80	37.88	12.43			22.89	
LOS			D	E	B			C	
Approach: Delay			36.39		12.43		22.89		
LOS			E		B		C		
Intersection Delay					30.86				
Intersection LOS					D				

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ALL-WAY STOP CONTROL ANALYSIS										
General Information					Site Information					
Analyst	CLF				Intersection	North Fayette St. & Queen St.				
Agency/Co.	Gorove/Slade Associates				Jurisdiction	City of Alexandria				
Date Performed	5/26/2004				Analysis Year	Total Future+10 2016				
Analysis Time Period	AMPH									
Project ID 1789-006 Carrhomes Prescott TIA										
East/West Street: Queen Street					North/South Street: North Fayette St.					
Volume Adjustments and Site Characteristics										
Approach	Eastbound					Westbound				
	L	T	R	L	T	R				
	20	232	34	0	0	0				
Volume	50			50						
%Thrus Left Lane										
Approach	Northbound					Southbound				
	L	T	R	L	T	R				
	0	151	109	14	126	0				
Volume	50			50						
%Thrus Left Lane										
	Eastbound		Westbound		Northbound		Southbound			
	L1	L2	L1	L2	L1	L2	L1	L2		
Configuration	LTR				TR		LT			
PHF	1.00				1.00		1.00			
Flow Rate	286				260		140			
% Heavy Vehicles										
No. Lanes	1		0		1		1			
Geometry Group	1				1		1			
Duration, T					0.25					
Saturation Headway Adjustment Worksheet										
Prop. Left-Turns	0.1				0.0		0.1			
Prop. Right-Turns	0.1				0.4		0.0			
Prop. Heavy Vehicle										
hLT-adj	0.2	0.2			0.2	0.2	0.2	0.2		
hRT-adj	-0.6	-0.6			-0.6	-0.6	-0.6	-0.6		
hHV-adj	1.7	1.7			1.7	1.7	1.7	1.7		
hadj, computed	4.81				4.81		4.81			
Departure Headway and Service Time										
hd, initial value	3.20				3.20		3.20			
x, initial	0.25				0.23		0.12			
hd, final value	4.81				4.81		4.81			
x, final value	0.38				0.33		0.19			
Move-up time, m	2.0				2.0		2.0			
Service Time	2.8		2.8		2.8		2.8			
Capacity and Level of Service										
	Eastbound		Westbound		Northbound		Southbound			
	L1	L2	L1	L2	L1	L2	L1	L2		
Capacity	536				510		390			
Delay	10.76				9.85		9.20			
LOS	B				A		A			
Approach: Delay	10.76				9.85		9.20			
LOS	B				A		A			
Intersection Delay					10.10					
Intersection LOS					B					

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ALL-WAY STOP CONTROL ANALYSIS								
<b>General Information</b>				<b>Site Information</b>				
Analyst	CLF			Intersection	North Fayette St. & Queen St.			
Agency/Co.	Gorove/Slade Associates			Jurisdiction	City of Alexandria			
Date Performed	5/26/2004			Analysis Year	Total Future+10 2016			
Analysis Time Period	AMPH							
Project ID 1789-006 Carrhomes Prescott TIA								
East/West Street: Queen Street				North/South Street: North Fayette St.				
<b>Volume Adjustments and Site Characteristics</b>								
Approach	Eastbound			Westbound				
	L	T	R	L	T	R		
Movement								
Volume	23	349	54	0	0	0		
%Thrus Left Lane	50			50				
Approach	Northbound			Southbound				
	L	T	R	L	T	R		
Movement								
Volume	0	92	157	48	390	0		
%Thrus Left Lane	50			50				
	Eastbound		Westbound		Northbound		Southbound	
	L1	L2	L1	L2	L1	L2	L1	L2
Configuration	LTR				TR		LT	
PHF	1.00				1.00		1.00	
Flow Rate	426				249		438	
% Heavy Vehicles								
No. Lanes	1		0		1		1	
Geometry Group	1				1		1	
Duration, T					0.25			
<b>Saturation Headway Adjustment Worksheet</b>								
Prop. Left-Turns	0.1				0.0		0.1	
Prop. Right-Turns	0.1				0.6		0.0	
Prop. Heavy Vehicle								
hLT-adj	0.2	0.2			0.2	0.2	0.2	0.2
hRT-adj	-0.6	-0.6			-0.6	-0.6	-0.6	-0.6
hHV-adj	1.7	1.7			1.7	1.7	1.7	1.7
hadj, computed	5.74				5.74		5.74	
<b>Departure Headway and Service Time</b>								
hd, initial value	3.20				3.20		3.20	
x, initial	0.38				0.22		0.39	
hd, final value	5.74				5.74		5.74	
x, final value	0.68				0.39		0.69	
Move-up time, m	2.0				2.0		2.0	
Service Time	3.7		3.7		3.7		3.7	
<b>Capacity and Level of Service</b>								
	Eastbound		Westbound		Northbound		Southbound	
	L1	L2	L1	L2	L1	L2	L1	L2
Capacity	602				499		613	
Delay	20.04				12.18		20.54	
LOS	C				B		C	
Approach: Delay	20.04				12.18		20.54	
LOS	C				B		C	
Intersection Delay					18.48			
Intersection LOS					C			

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TWO-WAY STOP CONTROL SUMMARY								
<b>General Information</b>				<b>Site Information</b>				
Analyst	CLF			Intersection	N Fayette and Site Entrance			
Agency/Co.	Gorove/Slade Associates			Jurisdiction	Alexandria, VA			
Date Performed	6/1/2004			Analysis Year	Total Future+10 2016			
Analysis Time Period	AMPH							
Project Description 1789-006 Prescott								
East/West Street: Site Entrance				North/South Street: N Fayette St				
Intersection Orientation: North-South				Study Period (hrs): 0.25				
<b>Vehicle Volumes and Adjustments</b>								
<b>Major Street</b>	Northbound			Southbound				
Movement	1	2	3	4	5	6		
	L	T	R	L	T	R		
Volume	0	230	5	2	157	0		
Peak-Hour Factor, PHF	1.00	1.00	1.00	1.00	1.00	1.00		
Hourly Flow Rate, HFR	0	230	5	2	157	0		
Percent Heavy Vehicles	0	--	--	0	--	--		
Median Type	Undivided							
RT Channelized			0			0		
Lanes	0	1	0	0	1	0		
Configuration			TR	LT				
Upstream Signal		0			0			
<b>Minor Street</b>	Westbound			Eastbound				
Movement	7	8	9	10	11	12		
	L	T	R	L	T	R		
Volume	6	0	24	0	0	0		
Peak-Hour Factor, PHF	1.00	1.00	1.00	1.00	1.00	1.00		
Hourly Flow Rate, HFR	6	0	24	0	0	0		
Percent Heavy Vehicles	0	0	0	0	0	0		
Percent Grade (%)	0			0				
Flared Approach		N			N			
Storage		0			0			
RT Channelized			0			0		
Lanes	0	0	0	0	0	0		
Configuration		LR						
<b>Delay, Queue Length, and Level of Service</b>								
Approach	NB	SB	Westbound			Eastbound		
Movement	1	4	7	8	9	10	11	12
Lane Configuration		LT	LR					
v (vph)		2	30					
C (m) (vph)		1344	763					
v/c		0.00	0.04					
95% queue length		0.00	0.12					
Control Delay		7.7	9.9					
LOS		A	A					
Approach Delay	--	--	9.9					
Approach LOS	--	--	A					

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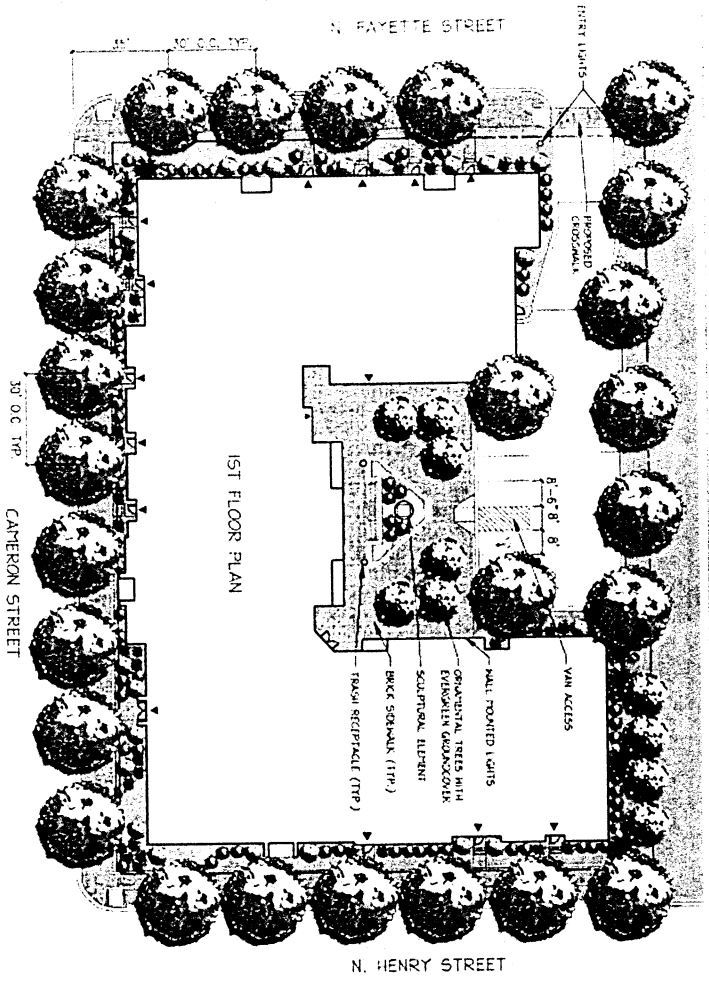
TWO-WAY STOP CONTROL SUMMARY								
<b>General Information</b>				<b>Site Information</b>				
Analyst	CLF			Intersection	N Fayette and Site Entrance			
Agency/Co.	Gorove/Slade Associates			Jurisdiction	Alexandria, VA			
Date Performed	6/1/2004			Analysis Year	Total Future+10 2016			
Analysis Time Period	PMPH							
Project Description 1789-006 Prescott								
East/West Street: Site Entrance				North/South Street: N Fayette St				
Intersection Orientation: North-South				Study Period (hrs): 0.25				
<b>Vehicle Volumes and Adjustments</b>								
<b>Major Street</b>	Northbound			Southbound				
Movement	1	2	3	4	5	6		
	L	T	R	L	T	R		
Volume	0	241	22	7	434	0		
Peak-Hour Factor, PHF	1.00	1.00	1.00	1.00	1.00	1.00		
Hourly Flow Rate, HFR	0	241	22	7	434	0		
Percent Heavy Vehicles	0	--	--	0	--	--		
Median Type	Undivided							
RT Channelized			0			0		
Lanes	0	1	0	0	1	0		
Configuration			TR	LT				
Upstream Signal		0			0			
<b>Minor Street</b>	Westbound			Eastbound				
Movement	7	8	9	10	11	12		
	L	T	R	L	T	R		
Volume	3	0	10	0	0	0		
Peak-Hour Factor, PHF	1.00	1.00	1.00	1.00	1.00	1.00		
Hourly Flow Rate, HFR	3	0	10	0	0	0		
Percent Heavy Vehicles	0	0	0	0	0	0		
Percent Grade (%)	0			0				
Flared Approach		N			N			
Storage		0			0			
RT Channelized			0			0		
Lanes	0	0	0	0	0	0		
Configuration		LR						
<b>Delay, Queue Length, and Level of Service</b>								
Approach	NB	SB	Westbound			Eastbound		
Movement	1	4	7	8	9	10	11	12
Lane Configuration		LT		LR				
v (vph)		7		13				
C (m) (vph)		1313		650				
v/c		0.01		0.02				
95% queue length		0.02		0.06				
Control Delay		7.8		10.7				
LOS		A		B				
Approach Delay	--	--	10.7					
Approach LOS	--	--	B					

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9-21-04

# OVERVIEW

HOPKINS WAREHOUSE - THE PRESCOTT  
DSUP # 2004-0001

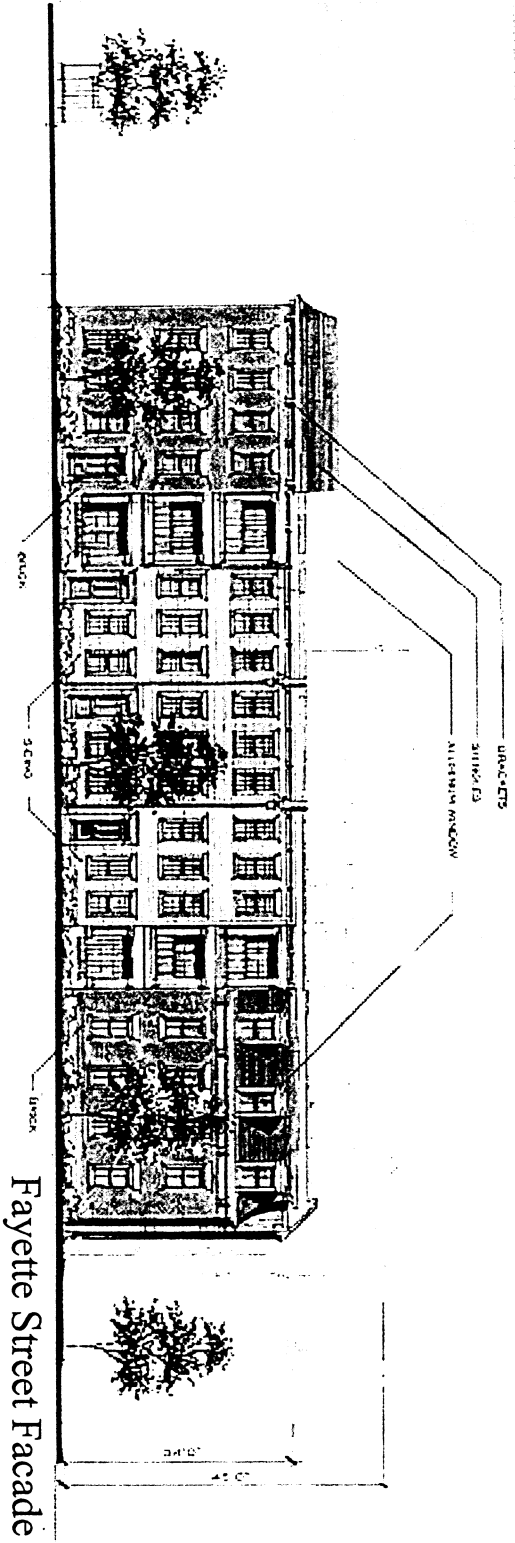


- 64 units
- 40% open space
- 3 - 4 stories
- 35 - 50 foot height

# OVERVIEW

HOPKINS WAREHOUSE - THE PRESCOTT  
DSUP # 2004-0001

- Designed to look like multiple buildings
- Underground parking
- SUP for 20% density bonus
- 4 on-site affordable units



City of Alexandria - Department of Planning & Zoning  
City Council Hearing of September 21, 2004

# ISSUES

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HOPKINS WAREHOUSE - THE PRESCOTT  
DSUP # 2004-0001

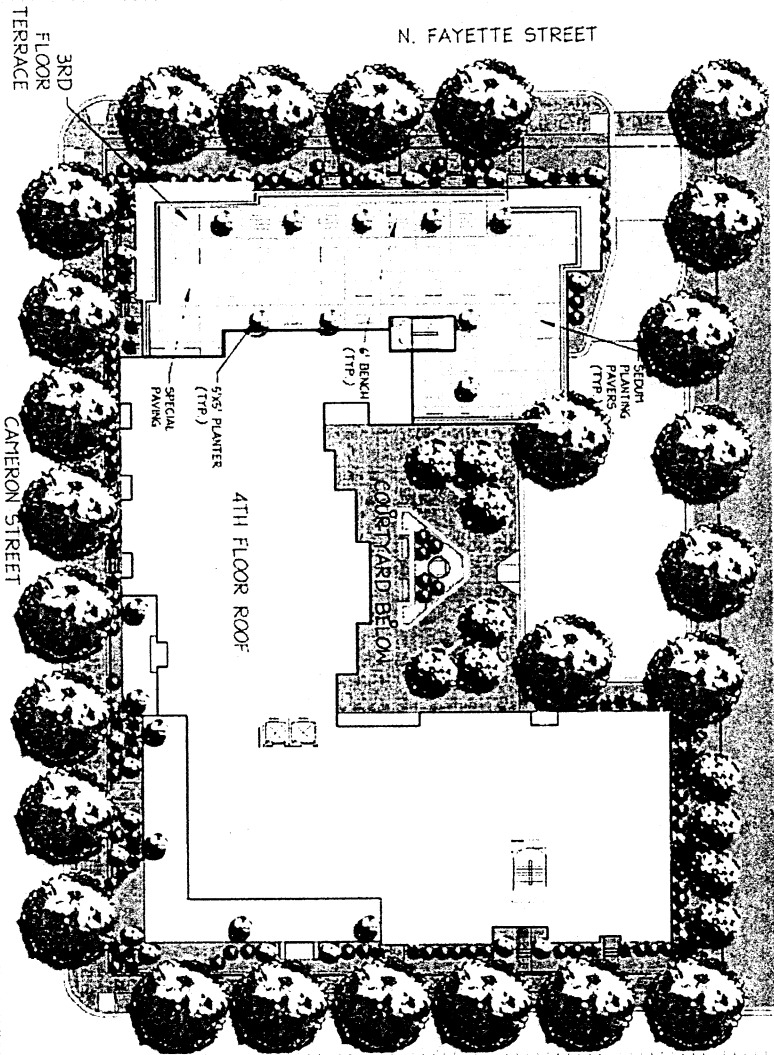
- Building Massing
  - Open Space
  - Traffic
  - Access on Fayette Street
-

# OPEN SPACE

HOPKINS WAREHOUSE - THE PRESCOTT  
DSUP # 2004-0001

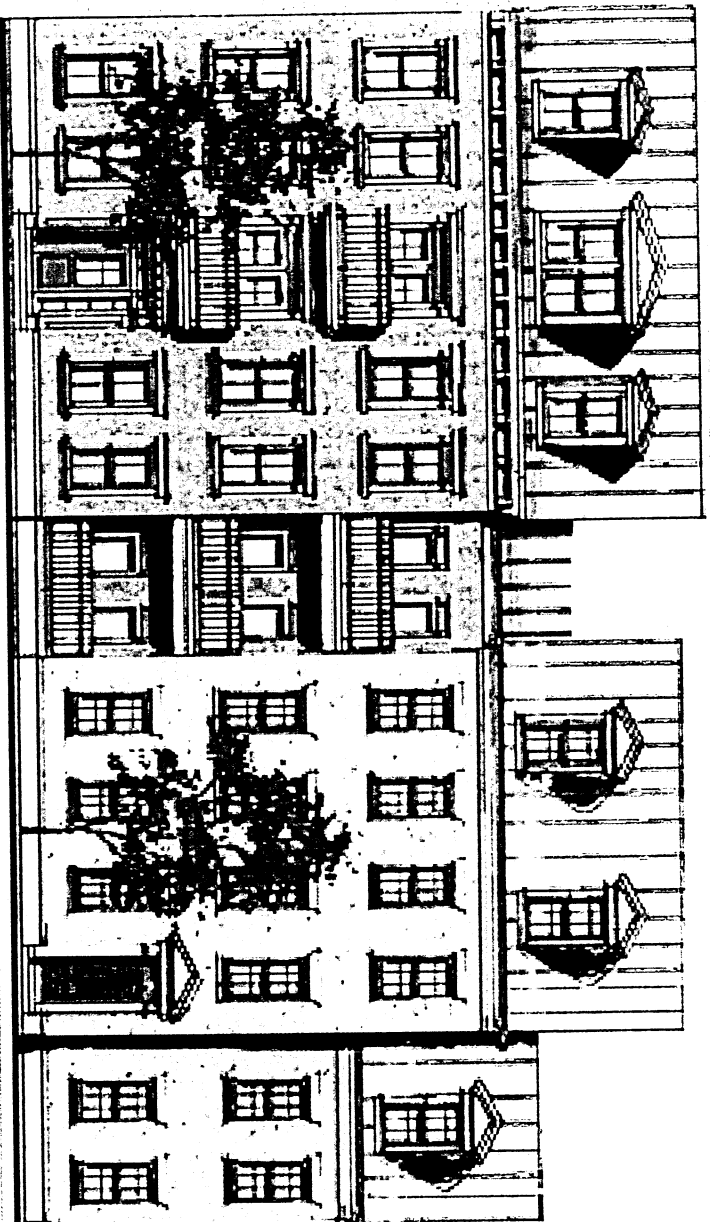


- 40% open space
- 26% ground-level open space
- 14% rooftop terrace

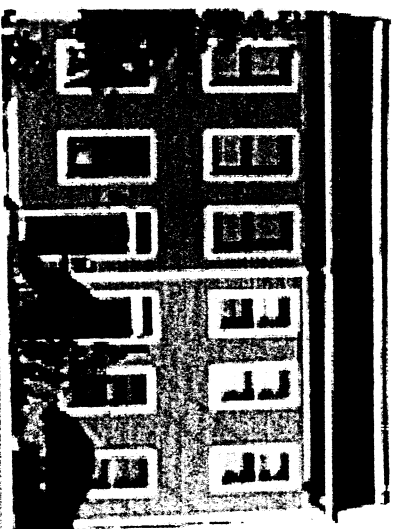


# BUILDING MASSING

HOPKINS WAREHOUSE - THE PRESCOTT  
DSUP # 2004-0001



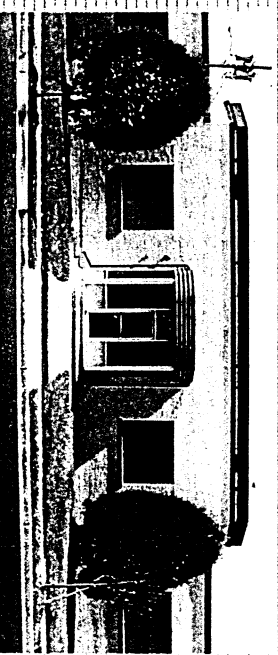
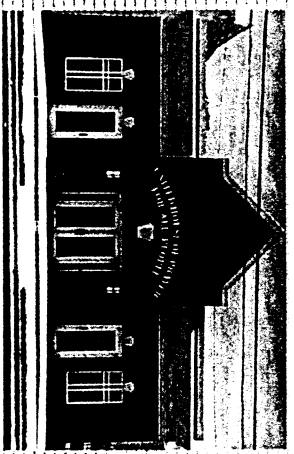
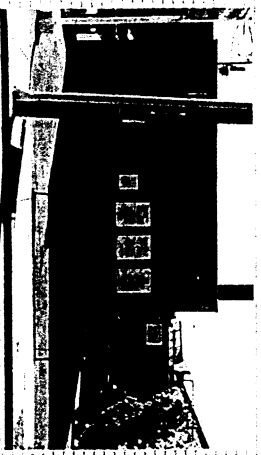
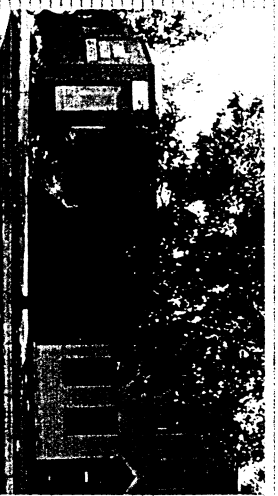
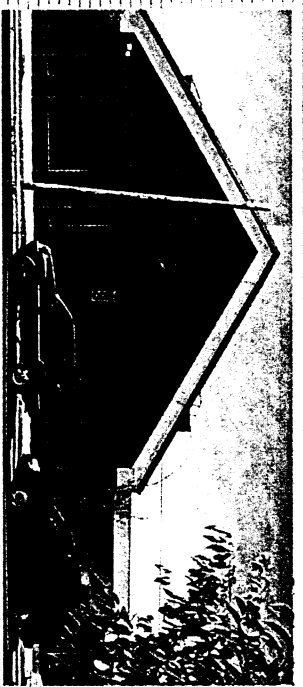
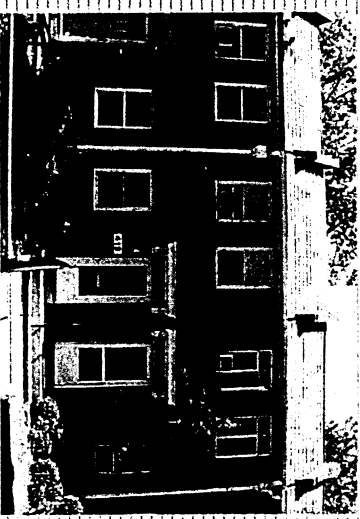
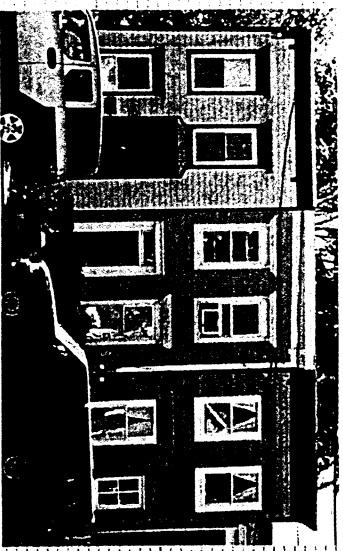
Staff Proposal - Henry Street



"Typical" Henry Street  
Townhomes

# BUILDING MASSING

HOPKINS WAREHOUSE - THE PRESCOTT  
DSUP # 2004-0001

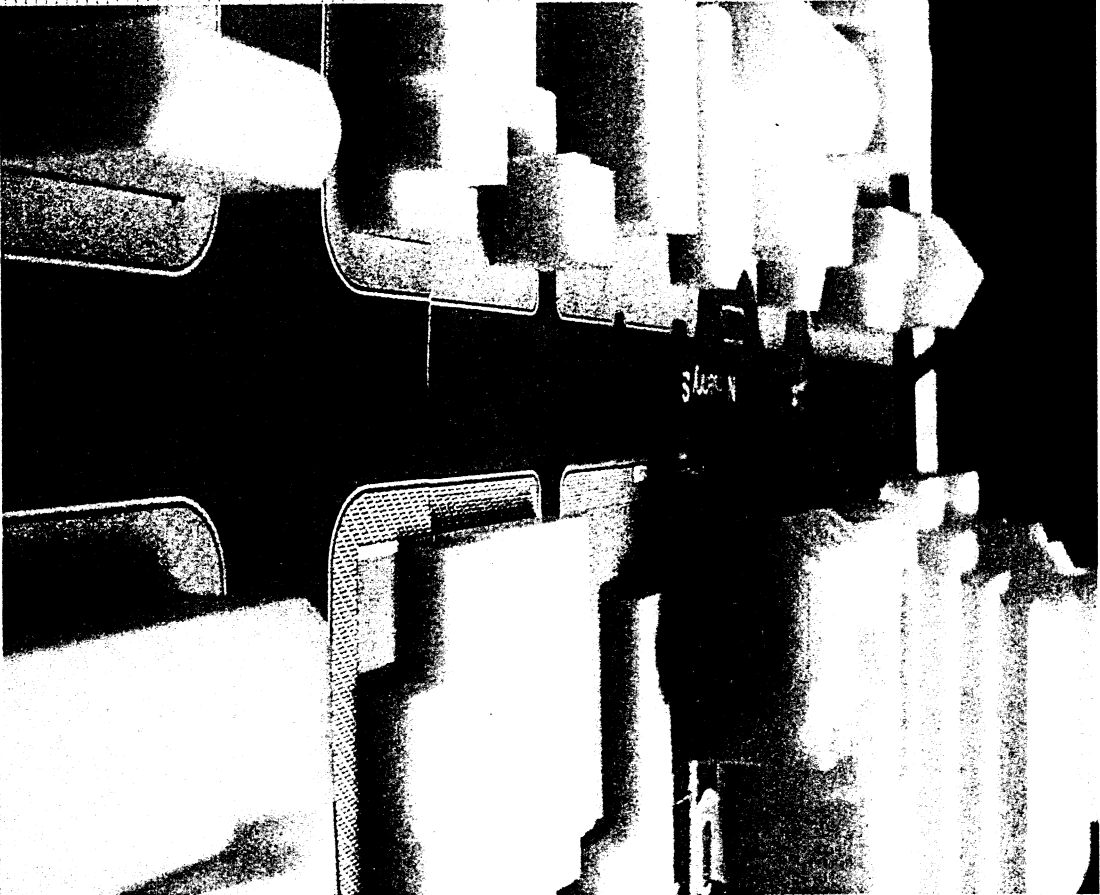


City of Alexandria - Department of Planning & Zoning  
City Council Hearing of September 21, 2004



# BUILDING MASSING

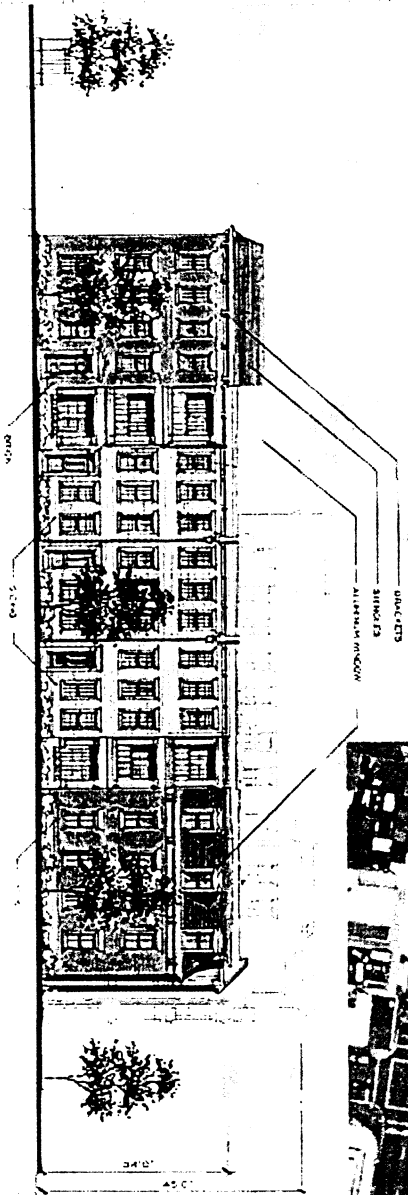
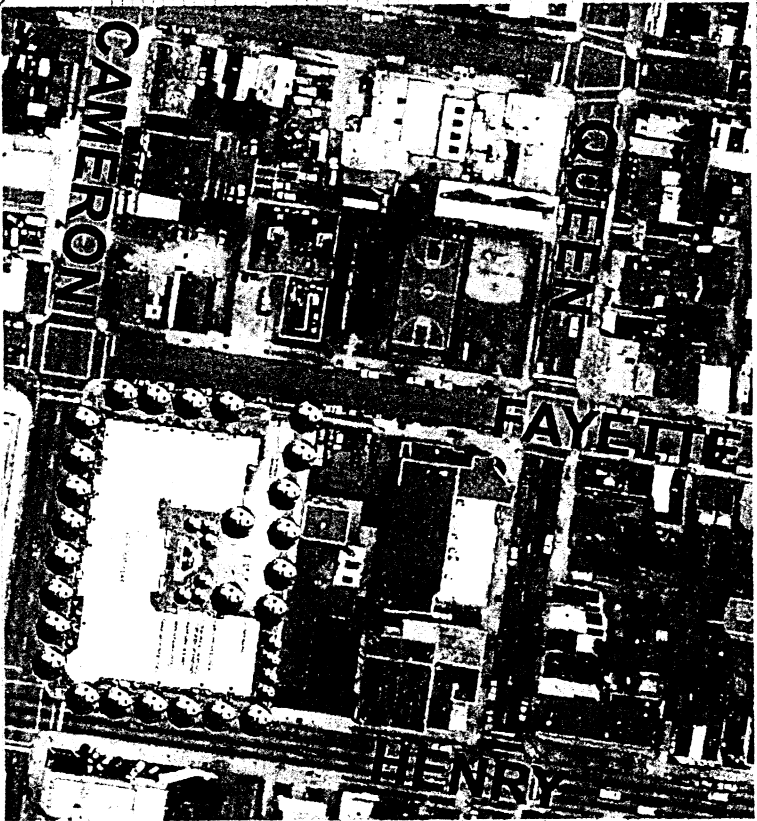
THE PRESCOTT  
DSUP #2004-0001



City of Alexandria – Department of Planning & Zoning  
City Council Hearing of September 21, 2004

# FAYETTE STREET

HOPKINS WAREHOUSE - THE PRESCOTT  
DSUP # 2004-0001



# ACCESS

THE PRESCOTT  
DSUP #2004-0001

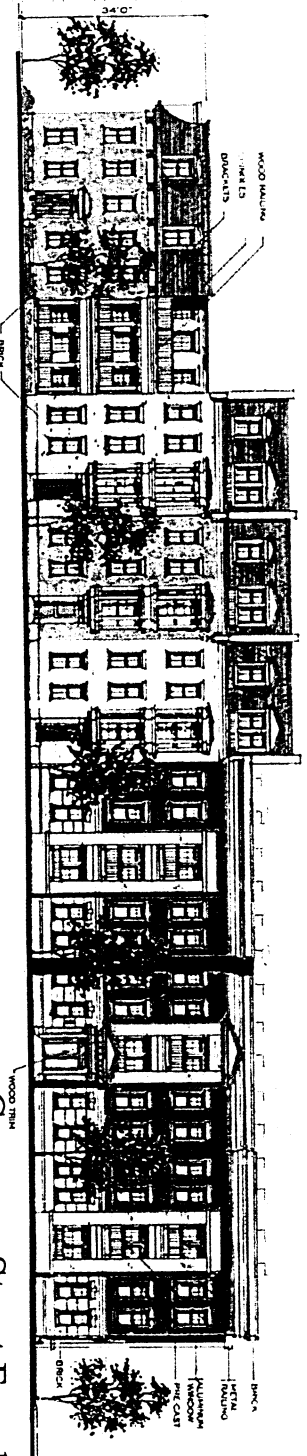


# PLANNING COMMISSION

HOPKINS WAREHOUSE - THE PRESCOTT  
DSUP # 2004-0001

Recommend approval 6-1, with staff recommendations, and following:

- \$10k for traffic calming on Fayette Street
- Reduce mass on Henry Street & corner of Henry & Cameron
- Place restrictions on hauling and construction traffic



Cameron Street Facade

15  
9-21-04



<w4thompson@aol.com>

09/21/2004 12:59 PM

Please respond to  
<w4thompson@aol.com>

<alexvamayor@aol.com>, <delpepper@aol.com>, <council@joycewoodson.net>, <councilmangaines@aol.com>, <rob@krupicka.com>, <macdonaldcouncil@msn.com>

cc

bcc

City of Alexandria Website Contact Us - EMail for Mayor, Vice-Mayor and Council Members (alexvamayor@aol.com, delpepper@aol.com, council@joycewoodson.net, councilmangaines@aol.com, rob@krupicka.com, macdonaldcouncil@msn.com, paulcsmedberg@aol.com, rose.boyd@ci.alexandria.va.us, jackie.henderson@ci.alexandria.va.us, tom.raycroft@ci.alexandria.va.us)

**City of Alexandria Website Contact Us - EMail for Mayor, Vice-Mayor and Council Members (alexvamayor@aol.com, delpepper@aol.com, council@joycewoodson.net, councilmangaines@aol.com, rob@krupicka.com, macdonaldcouncil@msn.com, paulcsmedberg@aol.com, rose.boyd@ci.alexandria.va.us, jackie.henderson@ci.alexandria.va.us, tom.raycroft@ci.alexandria.va.us)**

Time: [Tue Sep 21, 2004 12:59:55] IP Address: [152.163.100.68]

Response requested:

**First Name:** Wilson  
**Last Name:** Thompson  
**Street Address:** 1201 Queen Street  
**City:** Alexandria  
**State:** VA  
**Zip:** 22314  
**Phone:** 703-683-2204  
**Email Address:** w4thompson@aol.com

Prepared Remarks from Wilson Thompson of 1201 Queen Street at the September 21, 2004 Public Hearing on Proposed Project at 1115 Cameron Street and 200 North Henry Street

I commend and support the City of Alexandria for its efforts to enhance the viability of the Parker Grey Historic District through redevelopment. The report does an excellent job in addressing the visual impact of the proposed project on the Parker Grey Historic District. However, the report

does not adequately address the economic impacts of the proposed project on the Parker Grey Historic District. As a business owner in the Parker Grey District on Fayette Street, I have some concerns which I would like to discuss.

#### Minority Owned Businesses

Neither the city nor the contractor's study mentioned most of the businesses and nonprofits including the American Legion and a church in the Fayette and Queen Street corridor. There are several minority small businesses within two blocks of the project. Perhaps the highest concentration of small minority businesses in the city of Alexandria is located in this area. However, the study mentions that free African Americans might have inhabited the area in 1810 and the possibility of discovering artifacts during the construction. Yet, neither the city nor the developer seems to be interested in protecting current African Americans businesses that reflect the historical character of the Parker Grey Historic District.

On page 48, Figure 2, Land Use Design, does not reflect all of the minority businesses including my office on Queen Street at Fayette Street. It is evident from a visual survey of the entire 1200 block of Queen Street that my business is there. There is no discussion of the existence of these minority owned establishments that have historically always existed in the Parker Grey District. This fact is well documented and should not be ignored in any analysis of this area.

The city and the developer have interacted with the Intercity Civic Association and other civic associations in the area. The Intercity Civic Association does not represent all points of views and does not seem to represent the majority of residents nor businesses in the Queen Street and Fayette Street corridor. I am a member of the Intercity Civic Association. This point of view was gleaned from the public hearing before the planning board, written correspondence to the city and my own interactions with individuals in the corridor. I do not believe I heard an endorsement by a single individual in the Queen and Fayette Street corridor of the egress and entrance being on Fayette Street.

#### Traffic Count Analysis

The study provides numerical count of traffic but there are little or no analyses or narrative of the traffic count. The traffic count and the minimum analyses give the impression that Fayette Streets

and Cameron Street have equal capacity to accommodate traffic. The traffic count for the afternoon indicates that the total traffic count for Cameron Street was approximately 1,595 vehicles or 798 vehicles per lane and for the same period the traffic count for Fayette Street was approximately 592 vehicles. Without proper analyses and narrative, the traffic count for Fayette Street with two lanes one going north and the other lane going south and Cameron Street with two lanes going west gives the impression that the two street accommodate traffic approximately equally.

**Comments:**

A proper analyses and narrative should indicate that the ease of movement of traffic on the two different types of streets is not equal. I hypothesize that Cameron Street can accommodate the movement of traffic much easier than Fayette Street. One of the primary reasons for the efficiencies in traffic movement on Cameron Street versus Fayette Street traffic going south on Fayette Street would have to make a left turn across traffic which would cause traffic going in the same direction to move slower. The slower moving traffic would contribute to congestion. These factors should be analyzed and appropriately discussed.

The City of Alexandria seems to make critical assumptions that are not supported by the traffic study and other empirical observations. For example pages 16 and 61 of the traffic study states that the traffic flow to the Fayette Street entrance to the project would be south on Henry and make a right on Cameron then a right on Fayette Street and a right into the development. However, the traffic study of Fayette Street indicates that there are significant amounts of traffic in the afternoon proceeding south on Fayette Street passing Queen and Cameron Streets. A great deal of this traffic leaves Henry Street because of congestion and goes to Fayette and proceeds south. The residents of the proposed project would find this least path of resistant attractive and avoid the same congestion as current motorists. They would use Fayette Street and attempt to turn left across traffic in the afternoon on Fayette Street creating congestion. As a business owner on Fayette Street, I have observed a significant amount of traffic going south on Fayette Street during the afternoon.

Egress and Entrance on Fayette Street  
The egress and entrance are currently designed

to be from Fayette Street. One of the primary reasons for my objections is congestion. Congestion during the afternoon is apt to be significantly higher with the proposed egress and entrance. The increased congestion would adversely impact the immediate businesses and residents in the neighborhood. I believe the entrance and egress should be from Cameron Street, which is less likely to cause congestion. The transportation study does not analyze or consider the alternative of locating the egress and entrance on Cameron Street. The study only informs us that Fayette Street is the best alternative without evaluating the Cameron Street alternative. During the planning commission hearings, I believe the traffic flow was mischaracterized. It was stated that if the egress and entrance were on Cameron Street the traffic would turn right on Queens Street. Consequently, it did not matter where the egress and entrance were. The traffic might turn right on Queen but it would most likely be in the morning hours and the traffic study indicated that traffic is a smaller volume. The time of day is a critical factor and does make a difference.

#### Conclusion

As I have discussed above, the inadequacies of the transportation study adversely impact small and minority business. This project should not be allowed to proceed until an adequate transportation study is completed and input obtained from businesses and nonprofits in the area. In addition, an assessment of impact of congestion on minority and small businesses in the Fayette and Queen Streets corridor should be analyzed. The potential lack of parking and traffic congestion would adversely impact the viability of these businesses

Submitted on September 21, 2004 by:

Wilson Thompson  
1201 Queen Street  
Alexandria, VA. 22314  
(703) 683-2204



**DSP 2004-0001**  
**The Prescott / Hopkins Warehouse**  
**Proposed Modifications to Conditions**

80. The applicant shall present a disclosure statement to potential buyers disclosing the following to the satisfaction of the Director of P&Z and the City Attorney:  
That a major highway and commercial and civic uses, with its associated issues such as traffic and noise, are located adjacent to the project. The project is located in an urban environment and at times there are street closures for special events. (T&ES)
81. Due to the close proximity of the site to the Jefferson Davis Highway and commercial and civic uses the following conditions shall be included in the development requirements:
- a. Applicant shall prepare a noise study identifying the levels of interior noise residents of the project will be exposed to at the present time and 10 years into the future in a manner consistent with the Noise Guidance Book used by the Department of Housing and Urban Development (HUD).
  - b. Identify options to minimize noise exposure to future residents at the site, particularly in those units closest to railroad, including: triple-glazing for windows, additional wall/roofing insulation, installation of resilient channels between interior gypsum board and wall studs and any other special construction methods to reduce sound transmission. If needed, the applicant shall install some combination of the above to the satisfaction of the Directors of P&Z and T&ES.
  - c. The noise study shall be submitted and approved prior to final site plan approval. (T&ES)

September 21, 2004  
Public Hearing Item 15

Mayor Euille, Vice Mayor Pepper, and Members of Council:

I am Katy Cannady. I live at 20 East Oak Street.

Last spring this Council adopted an open space plan. Several members stated at that time that we needed to pay particular attention to providing open space in areas which lack it. I support the plan but I have been pessimistic and skeptical that it would ever be implemented. I'd like to have reasons for a happier outlook.

Now you have before you a proposal where 14 percent of the open space is on the roof and only a very small part of the other open space is even visible to passersby. It becomes even harder for me to be hopeful about open space. This building is just first in line ahead of several similar large condos planned in this area. More cause for gloom.

I understand completely how near neighbors who until now have had to live with a derelict warehouse and a defunct gas station are anxious for something different. In my mind this building isn't different enough. It has a footprint not very different from the warehouse and the gas station. Only it's going to be taller and it's going to tower over Cameron Street.

The Tyco building across Cameron is viewed as some kind of excuse for this one. The Tyco building is a mistake that's been there for about 20 years now. It should not be brought up as an excuse for another building that falls short of all the possibilities for a site so near to the Metro.

It seems to me that the six additional market rate units that could not otherwise be built compensate the developer in full for the four below market rate units included in this plan. Considering the income levels required for the "affordable units," I think it would be more accurate to call them "middle income ownership opportunities."

This developer, like everyone who comes to the city for a special use permit, should be giving the city something in exchange for the extra density that a special use permit confers. In Inner City with its notable lack of public open

space that something should ideally be land open to the public. Remember those pocket parks envisioned in the open space plan.

If that is not possible, then there should be a meaningful monetary contribution to the open space fund and that money should be set aside for use in the Inner City. Having the developer contribute \$10,000 to renovate an existing park is unacceptable. Park renovation is something taxpayers should be able expect for their money. The landowners and builders should assume the cost of greening Inner City. It's only fair that everyone there see real neighborhood improvements, not just bigger buildings and more traffic in the streets.

Thank you.

September 21, 2004

To: City Council Members:

My name is Frank McNelis and I reside at 1124 Princess Street, which is located at the southeast corner of North Fayette and Princess, approximately one and a half blocks from the Hopkins site. I am also a member of the ICCA.

I had planned to testify in person at this public hearing regarding Docket #15, DSUP 2004-0001, the Hopkins site development. However, yesterday I was asked by my company management to participate in a critical conference call with our Japanese affiliates this evening and therefore cannot attend this hearing. Due to the importance of the Hopkins site development, I have prepared this statement which I have asked my neighbor, Debra Sabourin, to read.

First of all, I want to convey my support for the Hopkins site development project, including Staff recommendations, with, however, an objection to the use of Fayette Street for ingress and egress. My reasons for this objection are as follows:

1. The volume of traffic on Fayette Street has been able to be managed, over time, through various, highly effective, measures. An ingress/egress on Fayette Street would create a significant, unwanted, additional traffic load. There are already two main north/south arteries in this area, Rt. 1 and West Street, and I do not see the need to add yet a third artery in this residential neighborhood.
2. The original Hopkins warehouse construction favors the use of Cameron Street for ingress/egress. To change this would seem to run counter to the historical nature of the construction in this area. There were likely good reasons for the use of Cameron over Fayette.
3. The existence of a playground across the street from the development would pose serious safety issues as a result of the additional traffic load that that a Fayette ingress/egress would create.

Finally, I wish to point out that I have documented this objection (and other comments) via email to the ICCA President, Amy Harris-White, on two previous occasions. The first was on June 10<sup>th</sup>, with a copy to Ms. Del Pepper. The second was to record my formal vote on this matter, as requested by the ICCA leadership, as input to the September 9<sup>th</sup> Planning Commission hearing. I have attached to this statement these two communications, which I would like to make part of the public record. I do so in order to correct the erroneous statement made by Ms. White at the September 9<sup>th</sup> Planning Commission hearing, to the effect that no resident concerns or objections to the proposed development were expressed.

I thank the Council for their time and indulgence.

Sincerely,

  
Frank P. McNelis



Print - Close Window

**Date:** Thu, 10 Jun 2004 10:12:17 -0700 (PDT)  
**From:** "Frank McNelis" <fpmcnel@yahoo.com>  
**Subject:** Input to the June 10th ICCA Meeting  
**To:** "Amy White" <white621n@att.net>  
**CC:** delpepper@aol.com

Amy: Hi! I am writing this because I will not be able to make tonight's meeting due to a prior company commitment, but wish to provide some input to any discussion and/or voting that may take place regarding the Hopkins development project.

Specifically, I am asking you to record my objection to the developer's plans to use Fayette Street for ingress/egress. This aspect of the plan was discussed at the initial information meeting and I believe that the developer was asked to re-evaluate this item. I don't know if this has been done, or if a different conclusion has been reached, but, if not, I am opposed to this aspect of the original plan.

My reason for my objection are;

1. The volume of traffic on Fayette Street has been able to be managed, over time, through such measures as the prohibition of southbound traffic during rush hour and the limitation on truck traffic. An ingress/egress on Fayette Street would create a significant, unwanted, additional traffic load.
2. The original warehouse construction favors the use of Cameron Street for ingress/egress. To change this would run counter to the historical nature of the construction in this area. There were likely good reasons for the use of Cameron over Fayette.
3. The existence of a playground across the street from the development would pose additional safety issues as a result of the additional traffic load. This item was also brought up at the initial meeting by Ms. Helen Miller.

Thank you for your consideration.

On an unrelated item, I have recently changed my email address and ask that you update your records. My new address is fpmcnel@yahoo.com. Thanks.

Sincerely,

Frank McNelis



Print - Close Window

**Date:** Wed, 8 Sep 2004 09:52:39 -0700 (PDT)  
**From:** "Frank McNelis" <fpmcnel@yahoo.com>  
**Subject:** Re: Input on Hopkins Project  
**To:** white621n@att.net

Amy: I support the Hennage development without additional comment.

**white621n@att.net** wrote:

Thanks for your feedback. What is your stance on the Hennage development?

----- Original message from Frank McNelis : -----

Amy: I will be out of town for this Thursday's meeting, but I want to provide my input for the record with respect to the Hopkins project.

I support the development of The Prescott (Hopkins project), subject to the Planning Commission's Staff recommendations, as detailed in the Staff report on Docket Item #13 for the September 9<sup>th</sup> Planning Commission Public Hearing.

In addition, I offer the following concerns and comments;

1. Although I fully support the Staff recommendation that Prescott residents should be ineligible to apply for or receive residential parking permits, I believe that enforcement is the key to this and that an increased community police presence is critical to this objective.
2. I object to the use of Fayette Street for ingress and egress. As I outlined in my note to you on June 10<sup>th</sup>, I do so for the following reasons:
  - a. The volume of traffic on Fayette Street has been able to be managed, over time, through such measures as the prohibition of southbound traffic during the afternoon rush hour and the limitation on truck traffic. An ingress/egress on Fayette Street would create a significant, unwanted, additional traffic load.
  - b. The original warehouse construction favors the use of Cameron Street for ingress/egress. To change this would run counter to the historical nature of the construction in this area. There were likely good reasons for the use of Cameron over Fayette.
  - c. The existence of a playground across the street from the development would pose additional safety issues as a result of the additional traffic load.
3. The existing restriction on southbound traffic on Fayette from the Orinoco Street intersection during the late afternoon time period needs to be strictly enforced. In addition, this restriction should be communicated clearly to all new Prescott residents by Prescott management.

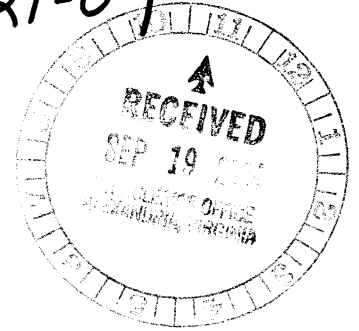
Thanks.

Sincerely,

Frank P. McNelis

15  
9-21-04

September 14<sup>th</sup>, 2004



William D. Euille  
Mayor  
City of Alexandria  
301 King Street  
Alexandria, VA 22314

Dear Mayor Euille:

As residents of North Fayette Street, Alexandria, we wish to express some concerns we have with the proposed condominium development on North Fayette and Cameron Streets.

While we are in general agreement with the plan to redevelop this section of the neighborhood, we were extremely troubled to hear that the Planning Commission has approved vehicular access to the project via Fayette Street. We believe this will cause a significant increase in traffic on North Fayette Street, which already suffers from a high volume of traffic, and could reduce quality of living and negatively impact our property value. On the contrary, we strongly suggest the project plans be amended so that vehicular access is via Cameron Street, already a busy traffic through-fare and without other residential buildings.

We very much hope that you, as a representative on the City Council, will reverse this decision and we will be closely following this matter as it comes to vote on September 21<sup>st</sup>.

Sincerely,

North Fayette Street residents


Attachment

### 300 NORTH FAYETTE STREET SIGNATORIES

We the undersigned strongly urge Alexandria City Council members to reverse a decision to permit vehicle access on North Fayette Street for the Fayette/Cameron Street condominium development.

Signature: \_\_\_\_\_

Name: \_\_\_\_\_

  
Sean Carroll

Address: \_\_\_\_\_

320 North Fayette St  
Alexandria VA 22314.



### 300 NORTH FAYETTE STREET SIGNATORIES

We the undersigned strongly urge Alexandria City Council members to reverse a decision to permit vehicle access on North Fayette Street for the Fayette/Cameron Street condominium development.

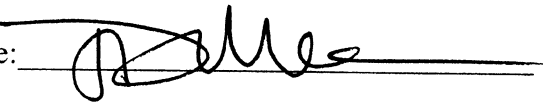
Signature: eileen marousek

Name: 313 n. fayette st

Address: eileen marousek

### 300 NORTH FAYETTE STREET SIGNATORIES

We the undersigned strongly urge Alexandria City Council members to reverse a decision to permit vehicle access on North Fayette Street for the Fayette/Cameron Street condominium development.

Signature: 

Name: Jaselle Williams

Address: 316 N. Fayette Street

1

**SPEAKER'S FORM**

DOCKET ITEM NO. 15

**PLEASE COMPLETE THIS FORM AND GIVE IT TO THE CITY CLERK  
BEFORE YOU SPEAK ON A DOCKET ITEM.**

PLEASE ANNOUNCE THE INFORMATION SPECIFIED BELOW PRIOR TO SPEAKING.

1. NAME: HOWARD MIDDLETON
2. ADDRESS: 3110 FAIRVIEW PARK DR.
- TELEPHONE NO. 703 641 4225 E-MAIL ADDRESS: pmiddleton@redmont.com
3. WHOM DO YOU REPRESENT, IF OTHER THAN YOURSELF? \_\_\_\_\_  
APPLICANT
4. WHAT IS YOUR POSITION ON THE ITEM?  
FOR:  \_\_\_\_\_ AGAINST: \_\_\_\_\_ OTHER: \_\_\_\_\_
5. NATURE OF YOUR INTEREST IN ITEM (PROPERTY OWNER, ATTORNEY, LOBBYIST, CIVIC INTEREST, ETC.):  
ATTORNEY
6. ARE YOU RECEIVING COMPENSATION FOR THIS APPEARANCE BEFORE COUNCIL?  
YES  NO \_\_\_\_\_

This form shall be kept as a part of the permanent record in those instances where financial interest or compensation is indicated by the speaker.

A maximum of three minutes will be allowed for your presentation. If you have a prepared statement, please leave a copy with the Clerk.

Additional time not to exceed 15 minutes may be obtained with the consent of the majority of the council present; provided notice requesting additional time with reasons stated is filed with the City Clerk in writing before 5:00 p.m. of the day preceding the meeting.

The public normally may speak on docket items only at public hearing meetings, and not at regular legislative meetings. Public hearing meetings are usually held on the Saturday following the second Tuesday in each month; regular legislative meetings on the second and fourth Tuesdays in each month. The rule with respect to when a person may speak to a docket item at a legislative meeting can be waived by a majority vote of council members present but such a waiver is not normal practice. When a speaker is recognized, the rules of procedures for speakers at public hearing meetings shall apply. If an item is docketed for public hearing at a regular legislative meeting, the public may speak to that item, and the rules of procedures for speakers at public hearing meetings shall apply.

In addition, the public may speak on matters which are not on the docket during the Public Discussion Period at public hearing meetings. The Mayor may grant permission to a person, who is unable to participate in public discussion at a public hearing meeting for medical, religious, family emergency or other similarly substantial reasons, to speak at a regular legislative meeting. When such permission is granted, the rules of procedures for public discussion at public hearing meetings shall apply.

**Guidelines for the Public Discussion Period**

- (a) All speaker request forms for the public discussion period must be submitted by the time the item is called by the City Clerk.
- (b) No speaker will be allowed more than three minutes.
- (c) If more speakers are signed up than would be allotted for in 30 minutes, the Mayor will organize speaker requests by subject or position, and allocate appropriate times, trying to ensure that speakers on unrelated subjects will also be allowed to speak during the 30 minute public discussion period.
- (d) If speakers seeking to address council on the same subject cannot agree on a particular order or method that they would like the speakers to be called on, the speakers shall be called in the chronological order of their request forms' submission.
- (e) Any speakers not called during the public discussion period will have the option to speak at the conclusion of the meeting, after all docketed items have been heard.