

Docket Item #12
BAR CASE #2005-0240

BAR Meeting
October 19, 2005

ISSUE: Demolition

APPLICANT: Van Metre Companies by Catharine Puskar, Atty.

LOCATION: 1300, 1306, 1312 & 1320 Duke Street

ZONE: OC Office/Commercial

STAFF RECOMMENDATION:

Staff recommends approval of the application with the following conditions:

1. To insure that significant information is not lost as a result of the current development project, the applicant must hire an archaeological consultant to complete a Documentary Study for this property. Contact Alexandria Archaeology to obtain a scope of work for this study.
2. If the Documentary Study indicates that the property has the potential to yield significant buried resources, an Archaeological Evaluation will be required. If significant resources are discovered, the consultant must complete a Resource Management Plan, as outlined in the *City of Alexandria Archaeological Standards*. Preservation measures presented in the Resource Management Plan, as approved by the City Archaeologist, will be implemented.
3. All archaeological preservation measures must be completed prior to ground-disturbing activities (such as coring, grading, filling, vegetation removal, undergrounding utilities, pile driving, landscaping and other excavations as defined in Section 2-151 of the Zoning Ordinance). To confirm, call Alexandria Archaeology at (703) 838-4399.
4. Call Alexandria Archaeology immediately (703-838-4399) if any buried structural remains (wall foundations, wells, privies, cisterns, etc.) or concentrations of artifacts are discovered during development. Work must cease in the area of the discovery until a City archaeologist comes to the site and records the finds.
5. The statements in C-3 and C-4 above must appear in the General Notes of all site plans and on each site plan sheet that involves any demolition or ground disturbing activity (include sheeting and shoring and grading). This will insure that on-site contractors are aware of the requirements. Additional statements to be included on the Final Site Plan will be determined in consultation with Alexandria Archaeology.
6. Certificates of Occupancy will not be issued for this property until the final archaeological report has been received and approved by the City Archaeologist.
7. If warranted by the City Archaeologist, the developer will erect a historic marker on the property according to specifications provided by Alexandria Archaeology. The marker will highlight the historical and archaeological significance of the property.
8. If warranted by the City Archaeologist, the developer will produce a booklet for the public on the history and archaeology of the property, according to specifications provided by Alexandria Archaeology.
9. All archaeological work will be carried out in accordance with the *City of Alexandria Archaeological Standards* and is subject to the approval of the City Archaeologist.
10. The applicant should not allow any other metal detection to be conducted on the property, unless authorized by Alexandria Archaeology.

NOTE: This docket item requires a roll call vote.

I. ISSUE:

The applicant is requesting approval of a Permit to Demolish for the demolition of three buildings in the 1300 block of Duke Street, bounded by Duke, Payne and West streets. The buildings fall wholly or partially within the boundaries of the Old and Historic Alexandria District. The entire site, consisting of three separate parcels, is to be cleared in preparation for remediation and redevelopment with 18 townhouse/ condominium units along Duke Street and a 40-unit condominium building at the rear. This project has not yet been scheduled for hearing by the Planning Commission or City Council. Only the proposed townhouses fall within the historic district boundaries. The conceptual plans for the townhouse portion of the development is docketed for the October 19, 2005 Board of Architectural Review hearing (docket item #13, BAR Case #2005-0202). The three buildings to be demolished are described below:

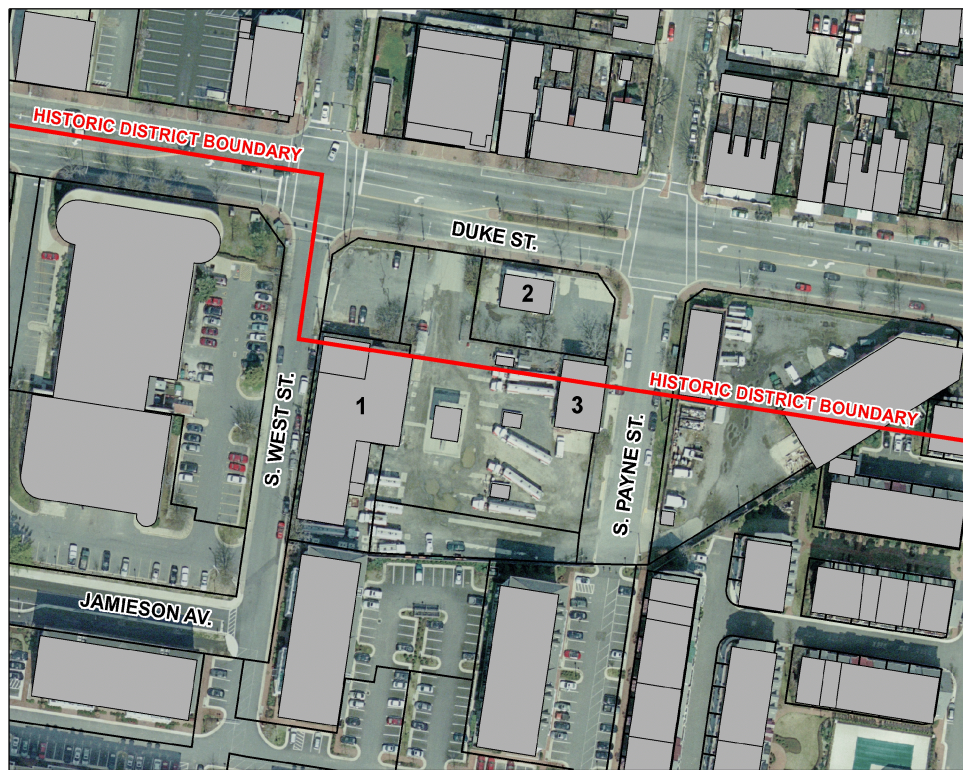


Figure 1- Buildings 1, 2, and 3 are to be demolished

Building #1 (318 & 320 West Street) is adjacent to West Street but is predominantly oriented to the interior of the block. It is accessed by drives from Duke Street and from West Street. The one story brick and cinder block building appears to have been constructed in at least three phases. The first small brick block at the north end was constructed for a scaffolding plant. Later additions were for a petroleum products warehouse. This large building is over 161' long and 40' to 80' wide at various points. The building is constructed of cinder block with brick cladding at the north end. The west facade facing West Street has two service bays and a

pedestrian door on the north end and a long blank wall to the south. There are four multi-paned metal windows at the east end of the north facade. The south facade appears to be blank. The east facade is the most complex, showing several periods of construction and having several panes. The north end has three metal windows, two pedestrian doors and a large service bay. The south end has a long frame addition clad in lapped siding, likely vinyl or aluminum.

Building #2 (1306 Duke Street) is the only building of the three which faces Duke Street. It was constructed as a service station but has most recently served as a thrifty car rental facility. This small, one story, flat roofed rectangular building measures 48' by 28'. Constructed of cinder block with stucco finish, only architectural embellishments on the simple building are the large curved plate glass window at the northeast corner and the blocky "tower" projecting several feet above the roofline on the Duke Street (north) facade. The tower was designed with stripes of glass block but these are no longer evident. In addition to the curved window, the front (north) facade has a pedestrian door and two service bays. One of the bays has been infilled with a frame wall and pedestrian door. The west facade has two multi-paned metal windows. The east facade has a similar window covered with a metal grate and a blocked pedestrian door. The south or rear facade has three metal windows covered with metal grates and a fourth window opening infilled with block.

Building #3 (1300 Duke Street or 320 South Payne Street) faces South Payne Street. The one story, flat roofed rectangular building was constructed as a truck maintenance facility. Measuring 60' by 40', it is constructed of cinder block. It has three large service bays with roll-up metal doors and a pedestrian door on the east facade facing Payne Street. The north and south facades each have three large metal windows. The west facade facing the interior of the lot has three service bays with roll-up doors.

In addition to the above mentioned buildings, there are numerous other features on this site, including concrete walls, chain link fences, concrete loading docks, concrete pads with fuel ports, gas pumps, a gable roofed frame shed measuring approximately 8' by 5', a trailer and asphalt and gravel parking areas.

II. HISTORY:

Located at the western edge of the early boundaries of the town, just beyond the depot, roundhouse and freight yards of the Baltimore and Potomac Railroad, the subject block has historically been lightly developed. Well into the 20th century it held only two 19th century dwellings (1300 and 1318 Duke Street) with their associated outbuildings. The earlier, frame house at 1300 Duke Street was demolished in 1953 while the later, brick house appears to have remained at least into the 1960s. The change to light industrial or commercial uses began in 1951. Building #1 was initially constructed in 1951 as a "scaffolding plant" for William H. Meeks according to plans by Gordon D. Rust (Building Permit #5020, February 14, 1951). Within a decade, this building had been expanded to the west and was in use for car repair services. In 1978 it was substantially enlarged to the south for Thomas J. Fannon to serve as a petroleum storage warehouse (Building Permit #10138, 8/18/1978). The Moderne styled Building #2 was constructed in 1953 for Morrison Clark as an Amaco gasoline service station according to what appear to be standard plans utilized by the American Oil Company (Building

Permit #5973, July 7, 1953). The property was subsequently acquired by the Fannon family. Beginning in 1987, it was converted to a Thrifty Car Rental office. Building #3 was constructed in 1978 as a truck maintenance shop for Thomas J. Fannon (Building Permit #10138, 8/18/1978).

III. ANALYSIS:

In considering a Permit to Demolish/Capsulate, the Board must consider the following criteria set forth in the Zoning Ordinance, §10-105(B):

- (1) Is the building or structure of such architectural or historical interest that its moving, removing, capsulating or razing would be to the detriment of the public interest?
- (2) Is the building or structure of such interest that it could be made into a historic house?
- (3) Is the building or structure of such old and unusual or uncommon design, texture and material that it could not be reproduced or be reproduced only with great difficulty?
- (4) Would retention of the building or structure help preserve the memorial character of the George Washington Memorial Parkway?
- (5) Would retention of the building or structure help preserve and protect an historic place or area of historic interest in the city?
- (6) Would retention of the building or structure promote the general welfare by maintaining and increasing real estate values, generating business, creating new positions, attracting tourists, students, writers, historians, artists and artisans, attracting new residents, encouraging study and interest in American history, stimulating interest and study in architecture and design, educating citizens in American culture and heritage, and making the city a more attractive and desirable place in which to live?

Staff believes that none of the above criteria are met. All three of the buildings date to the second half of the 20th century. Of these three utilitarian buildings, only Building #1 exhibits any sense of architectural design. However, it appears to be based on a standard plan that was undoubtedly replicated in large numbers across the region or country for Amaco service stations and has been altered over time with the infilling of openings and painting or cladding of the glass block trimmed tower feature. In conclusion, Staff believes the three buildings and other site features lack any obvious architectural or historical merit and has no objection to the proposed demolition. However, Staff does recommend that no demolition permit is issued until a special use permit has been approved by the City Council for the project.

Staff notes the comments of Alexandria Archeology and recommends that they be included as conditions of the approval.

IV. STAFF RECOMMENDATION:

Therefore, Staff recommends approval of the application with the following conditions:

1. To insure that significant information is not lost as a result of the current development project, the applicant must hire an archaeological consultant to complete a Documentary Study for this property. Contact Alexandria Archaeology to obtain a scope of work for this study.
2. If the Documentary Study indicates that the property has the potential to yield significant

buried resources, an Archaeological Evaluation will be required. If significant resources are discovered, the consultant must complete a Resource Management Plan, as outlined in the *City of Alexandria Archaeological Standards*. Preservation measures presented in the Resource Management Plan, as approved by the City Archaeologist, will be implemented.

3. All archaeological preservation measures must be completed prior to ground-disturbing activities (such as coring, grading, filling, vegetation removal, undergrounding utilities, pile driving, landscaping and other excavations as defined in Section 2-151 of the Zoning Ordinance). To confirm, call Alexandria Archaeology at (703) 838-4399.
4. Call Alexandria Archaeology immediately (703-838-4399) if any buried structural remains (wall foundations, wells, privies, cisterns, etc.) or concentrations of artifacts are discovered during development. Work must cease in the area of the discovery until a City archaeologist comes to the site and records the finds.
5. The statements in C-3 and C-4 above must appear in the General Notes of all site plans and on each site plan sheet that involves any demolition or ground disturbing activity (include sheeting and shoring and grading). This will insure that on-site contractors are aware of the requirements. Additional statements to be included on the Final Site Plan will be determined in consultation with Alexandria Archaeology.
6. Certificates of Occupancy will not be issued for this property until the final archaeological report has been received and approved by the City Archaeologist.
7. If warranted by the City Archaeologist, the developer will erect a historic marker on the property according to specifications provided by Alexandria Archaeology. The marker will highlight the historical and archaeological significance of the property.
8. If warranted by the City Archaeologist, the developer will produce a booklet for the public on the history and archaeology of the property, according to specifications provided by Alexandria Archaeology.
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CITY DEPARTMENT COMMENTS

Legend: C - code requirement R - recommendation S - suggestion F- finding

Code Enforcement:

Demolition Comments

C-1 Prior to the issuance of a demolition permit or land disturbance permit, a rodent abatement plan shall be submitted to Code Enforcement that will outline the steps that will taken to prevent the spread of rodents from the construction site to the surrounding community and sewers.

Concept Comments

F-1 FDCs are obstructed by planters or parking spaces. All FDCs shall be clearly visible and directly accessible from either West Street or Payne Street. Landscaping shall be kept clear of FDC locations.

F-2 The proposed project is located on a site used to store flammable and combustible liquids. An environmental assessment and proper remediation will be required prior to construction.

F-3 The height of these structures and the garage structure are required to be equipped with an automatic fire suppression system.

F-4 Currently, the tallest building is proposed as under 50 feet in height. Should any structure exceed the 50 foot limit, ladder truck access will be required to the two longest sides of the affected structure(s).

R-1 Provide an Emergency Vehicle Easement along the private roadway leading to the garage.

R-2 Based on a history of sound transmission complaints, it is recommended that all dwelling units have a STC rating of at least 60.

R-3 Handicap parking spaces for apartment and condominium developments shall remain in the same location(s) as on the approved site plan. Handicap parking spaces shall be properly signed and identified as to their purpose in accordance with the USBC and the Code of Virginia. Ownership and / or control of any handicap parking spaces shall remain under common ownership of the apartment management or condominium association and shall not be sold or leased to any single individual. Parking within any space identified as a handicap parking space shall be limited to only those vehicles which are properly registered to a handicap individual and the vehicle displays the appropriate license plates or window tag as defined by the Code of Virginia for handicap vehicles. The relocation, reduction or increase of any handicap parking space shall only be approved through an amendment to the approved site plan.

R-4 The applicant of any building or structure constructed in excess of 10,000 square feet; or any building or structure which constructs an addition in excess of 10,000 square feet

shall contact the City of Alexandria Radio Communications Manager prior to submission of final site plan. The proposed project shall be reviewed for compliance with radio requirements of the City of Alexandria to the satisfaction of the City of Alexandria Radio Communications Manager prior to site plan approval. Such buildings and structures shall meet the following conditions:

- a) The building or structure shall be designed to support a frequency range between 806 to 824 MHz and 850 to 869 MHz.
- b) The building or structure design shall support a minimal signal transmission strength of -95 dBm within 90 percent of each floor area.
- c) The building or structure design shall support a minimal signal reception strength of -95 dBm received from the radio system when transmitted from within 90 percent of each floor area.
- d) The building or structure shall be tested annually for compliance with City radio communication requirements to the satisfaction of the Radio Communications Manager. A report shall be filed annually with the Radio Communications Manager which reports the test findings.

If the building or structure fails to meet the above criteria, the applicant shall install to the satisfaction of the Radio Communications Manager such acceptable amplification systems incorporated into the building design which can aid in meeting the above requirements. Examples of such equipment are either a radiating cable system or an FCC approved type bi-directional amplifier. Final testing and acceptance of amplification systems shall be reviewed and approved by the Radio Communications Manager.

- R-5 Sheeting and shoring shall not extend beyond the property line; except when the developer has obtained a written release from adjacent property owners which has been recorded in the land records; or through an approved encroachment process.
- C-1 The public parking garage (Use Group S-2) is required to be equipped with a sprinkler system (USBC 903.2.11).
- C-2 The public parking garage floor must comply with USBC 406.2.6 and drain through oil separators or traps to avoid accumulation of explosive vapors in building drains or sewers as provided for in the plumbing code (USBC 2901). This parking garage is classified as an S-2, Group 2, public garage.
- C-3 Enclosed parking garages must be ventilated in accordance with USBC 406.4.2. Show vent locations on plans.
- C-4 Provide two Siamese connections located to the satisfaction of the Director of Code Enforcement.
- C-5 A separate tap is required for the building fire service connection.

- C-6 The developer shall provide a building code analysis with the following building code data on the plan: a) use group; b) number of stories; c) type of construction; d) floor area per floor ; e) fire protection plan.
- C-7 The developer shall provide a separate Fire Service Plan which illustrates: a) emergency ingress/egress routes to the site; b) two fire department connections (FDC) to the building, one on each side/end of the building; c) fire hydrants located within on hundred (100) feet of each FDC; d) on site fire hydrants spaced with a maximum distance of three hundred (300) feet between hydrants and the most remote point of vehicular access on site; e) emergency vehicle easements (EVE) around the building with a twenty-two (22) foot minimum width; f) all Fire Service Plan elements are subject to the approval of the Director of Code Enforcement.
- C-8 The final site plans shall show placement of fire easement signs.
- C-9 A soils report must be submitted with the building permit application.
- C-10 Prior to submission of the Final Site Plan #1, the developer shall provide a fire flow analysis by a certified licensed fire protection engineer to assure adequate water supply for the structure being considered.
- C-11 A Certificate of occupancy shall be obtained prior to any occupancy of the building or portion thereof, in accordance with USBC 119.0.
- C-12 Required exits, parking, and accessibility within the building for persons with disabilities must comply with USBC Chapter 11.

Historic Alexandria:

“Prefer wood windows and doors.”

Alexandria Archaeology:

- F-1 Tax records indicate that at least one house was present on this property by the early 19th century. John Emerson owned most of the block from at least 1850 into the 1870s, and lived on the property. In addition, there is evidence for the presence of a free African American household on this street face in 1810 and 1830, but the exact address is unknown. The development lot therefore has potential to yield archaeological resources that could provide insight into life in early Alexandria, perhaps relating to free blacks.
- C-1 To insure that significant information is not lost as a result of the current development project, the applicant must hire an archaeological consultant to complete a Documentary Study for this property. Contact Alexandria Archaeology to obtain a scope of work for this study.
- C-2 If the Documentary Study indicates that the property has the potential to yield significant buried resources, an Archaeological Evaluation will be required. If significant resources

are discovered, the consultant must complete a Resource Management Plan, as outlined in the *City of Alexandria Archaeological Standards*. Preservation measures presented in the Resource Management Plan, as approved by the City Archaeologist, will be implemented.

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- R-2 The applicant should not allow any other metal detection to be conducted on the property, unless authorized by Alexandria Archaeology.

Transportation and Environmental Services:

“No comment.”