

Docket Item # 17  
DEVELOPMENT SPECIAL USE PERMIT #2003-0018  
CAMERON STATION PHASE VI

Planning Commission Meeting  
November 6, 2003

**ISSUE:** Consideration of a request for a development special use permit, with site plan, for construction of townhouses.

**APPLICANT:** Cameron Development, L.L.C.  
by Duncan W. Blair, Esquire

**LOCATION:** 500 Cameron Station Boulevard/Ferdinand Day Drive

**ZONE:** CDD-9/Coordinated Development District

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***A. Overview***

Staff is recommending approval of the proposed 97 townhouses and the accompanying transportation management plan (SUP#2003-0092). The site is located south of Samuel Tucker Elementary School within Phase VI of Cameron Station. This application is one of the final two phases within the Cameron Station CDD-Coordinated Development District to be developed. The other remaining Phase VII condominium proposal is being processed concurrently.

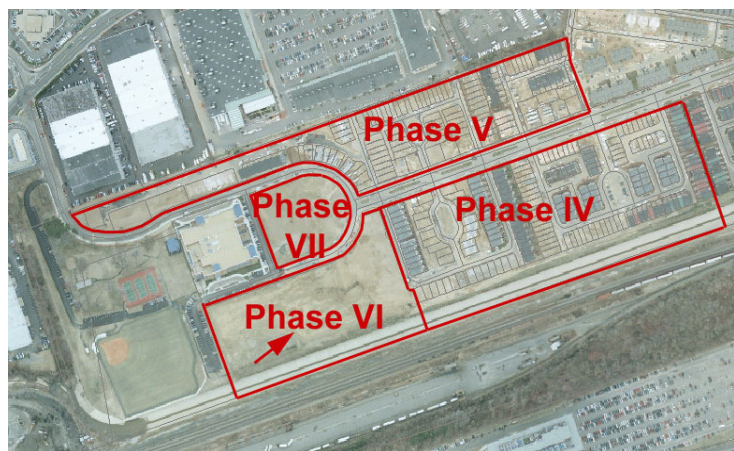
The initial areas of concern with staff and the community regarding the proposed development included:

- Extending the existing street grid pattern;
- Providing usable consolidated areas of open space;
- Providing visual and physical connections to the adjacent linear park;
- Improving the quality and amenities of the open space;
- Parking;
- Linear Park; and
- Providing additional variation in the buildings.

The applicant worked with the City and the community to address many of these issues. The proposed use and scale are appropriate to the context for this portion of Cameron Station, which is primarily townhomes, and will provide an appropriate mass and scale adjacent to the linear park on the southern portion of the site. There remain issues of phasing for the linear park, affordable housing, building design and open space. These issues can be addressed by the recommended conditions of approval.

***B. Background/History:***

On May 7, 2002, the Planning Commission recommended denial of the 309-unit multi-family rental apartment Archstone proposal. The applicant requested deferral of the application prior to the May 18, 2002 City Council meeting. The community, staff, and Planning Commission expressed concerns with the overall mass and scale of the building. The parking for the proposed facility would have been located in a six-level, above-grade parking structure which added considerable mass to the site.



***Phase VI and surrounding area***

**C. Project Description:**

Phase VI is a 5.15-acre site located in the southwest portion of Cameron Station, with frontage on Ferdinand Day Drive and Tancreti Lane. Phase VII and Samuel Tucker Elementary School are located to the north of the site; the City’s Armistead Boothe Park is located to the west; the linear park is to the south; and townhomes (phase IV), are to the east. The subject property is currently vacant.

The proposed three- to four-story townhouses are rear-loaded garage units accessed from rear alleys, with the exception of those adjacent to the linear park, which are front-loaded units. Each townhouse includes parking for two vehicles, mostly as two-car garages, with six 16-foot wide units that provide two tandem parking spaces. Twenty percent visitor parking is provided and distributed throughout the site. About a third of the units are mews units which do not front onto a street. These units are separated by pedestrian connections that link the linear park to the south to Ferdinand Day Drive to the north.

A total of 30% of the site (1.56 acres) is ground level open space, in the form of common areas adjacent to the pedestrian connections, pocket parks, and private front and rear yards. Part of the development for phase VI also includes continuing the construction of the linear park (along the length of the site), adding to the open space accessible to residents and the public in general. The proposed phase VI and phase VII are the remaining two phases within Cameron Station.

CAMERON STATION DEVELOPMENT SUMMARY								
Phase	Approved					Proposed	Proposed	TOTAL
	I	II	III	IV	V	VI	VII	
Land Area (Acres)	20.52	24.02	14.11	11.52	11.80	5.15	2.44	89.56
Total Number of Units	341	541	317	214	191	97	148	1849
Single Family	15	6	0	0	11	0	0	
Townhouse	169	153	207	178	120	97	0	
B/B Townhouse	4	54	0	36	0	0	0	
Stacked Townhouse	40	52	0	0	60	0	0	
Multifamily	113	276	110	0	0	0	148	
Multifamily/Elderly	0	0	0	0	0	0	0	
Density (Units/Acre)	16.62	22.52	22.47	18.58	16.19	18.84	60.66	20.6
Gross Floor Area (Square Feet)	819,914	910,513	777,817	648,311	451,700	266,304	204,059	4,078,618
Open Space (Acres & Percent)	6.0 (29.2%)	6.98 (29%)	3.94 (27.9%)	2.31 (20%)	3.42 (29.9%)	1.56 (30%)	1.1 (45%)	25.31 (28.3%)

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Type of Unit and Parking		Phase							Total
		I	II	III	IV	V	VI	VII	
A	32' Cottage 2 garage spaces	8				6			14
B	40' Single Family 2 garage spaces	7	6			5			18
C	20' Front Load Townhouse 1 garage/1 driveway space	31		29	78				138
D	22' Front Load Townhouse 1 garage/1 driveway space	28		23	24	46			121
E	24' Front Load Townhouse 2 garage spaces	24		52		27			103
F	20' Rear Load Townhouse 2 garage spaces	37	47	21	16				121
G	24' "Stacked Townhouse" 1 garage/1 surface space	40	52			60			152
H	Condominiums above Retail Underground/surface parking	113		110					223
I	16' Rear Load 2 stacked garage spaces	25	48	40	28		6		147
K	24' Back-To-Back Townhouses 2 garage spaces	4	54		36				94
L	18' Front Load Townhouse 1 garage/1 driveway space	3	58	20		31			112
M	4 story Multifamily "Stacked Townhouse" surface parking		132						132
N	4 story Multifamily 1 garage space/surface space		144						144
P	28' Front Load Townhouse 2 garage spaces	21		22	32		24		99
Z	Radial Townhouse 2 garage spaces					16	7		23
	4 story Condominiums above underground parking							148	148
	20' x 40' Rear Load Townhouses 2 garage spaces						43		43
	20'X50' Rear Load Townhouses 2 garage spaces						11		11
	20' x 64' Rear Load Townhouses 2 garage spaces						6		6
	<b>TOTAL</b>	<b>341</b>	<b>541</b>	<b>317</b>	<b>214</b>	<b>191</b>	<b>97</b>	<b>148</b>	<b>1849</b>

***D. Zoning:***

The site is zoned Coordinated Development District (CDD). A CDD Concept Plan was approved for Cameron Station by the City in 1996. The approved Cameron Station CDD Concept Plan establishes general standards for Cameron Station which, in combination with the CDD zoning and special use permit approvals for each phase form the requirements for development within Cameron Station. The CDD zoning establishes the maximum number of units that may be developed on Cameron Station at roughly 2,445 - 2,510 units (depending on the number of units removed in exchange for elimination of the school site from the CDD). With the phase VI and phase VII proposals, the total number of units within Cameron Station will be 1,849.

<b>PHASE VI - CAMERON STATION</b>		
Property Address:	450 Ferdinand Day Drive	
Total Site Area:	310,186 sq.ft. (7.12 acres)	
Zone:	CDD#9/Coordinated Development District	
Current Use:	Vacant	
Proposed Use:	Townhouse Dwellings	
	<u>Permitted/Required</u>	<u>Proposed</u>
Floor Area	N/A	266,304 sq ft
FAR	N/A	1.19
Height	120 Feet	45 Feet
Open Space	No specific ordinance requirement but 25-30% proposed and required in earlier phases	1.56 acres (30%)
Maximum No. of Dwellings	2,445- 2,510 for Cameron Station	97 townhouse dwelling units (1849 total including phases VI and VII)
Parking	223 Total 2 Spaces for each townhouse Plus 15% visitor parking required by concept plan approval	233 Total 91- 2 Car Garages 6 - 1 Car Garages (w/tandem space in driveway) 39- Visitor Spaces (20% visitor spaces)

***E. Staff Analysis:***

Staff is recommending approval of the proposed townhouse development due to the desirable urban design and site characteristics, such as access from a rear alley and orientation of the buildings to the street. With the exception of the six new deeper units that are being proposed at the recommendation of staff, the proposed townhouses will be similar to units that have been built throughout Cameron Station. The proposal provides slightly more open space than has been typical in previous five phases of Cameron Station and provides more visitor parking than is required. Staff supports the application subject to the recommendations included in this report. The conditions outlined within the staff report will ensure enhanced architecture, landscaping, and pedestrian connectivity. The proposed use, scale, and height are compatible with the existing context of Cameron Station.

***Affordable Housing (Phase VI and Phase VII):***

The applicant has agreed to an affordable housing plan that will provide seven on site affordable units within the 148 unit condominium building proposed within phase VII. The seven units will consist of 4 one-bedroom units and 3 two-bedroom units. The one-bedroom units range in size from approximately 900-1,000 sq.ft., the two-bedroom units range in size from approximately 1,000-1,400 sq.ft.. The units will be affordable upon resale for a minimum term of 15 years, with other terms and conditions such as sales agreements, fees to be charges and qualifications for future owners. The current affordable housing proposal by the applicant is significantly more than the standard \$1.00 per square foot contribution.

Based upon the gross floor area, the standard contribution for phases VI and VII would be \$470,363. Given staff's estimate of the minimum likely purchase price of the units (approximately \$325,000 for a one-bedroom and \$350,000 for the two-bedroom units) and the price necessary to be considered affordable (\$175,000 for the one-bedroom units and \$225,000 for the two-bedroom units), the subsidy for each unit is an estimated \$150,000 for one-bedroom unit and \$125,000 for the two-bedroom units. The maximum income limits for purchasers of these units are \$68,700 for a 1-2 person household and \$79,500 for a household of 3 or more persons.

The total subsidy ( $\$150,000 \times 4$ ) + ( $\$125,000 \times 3$ ), the total subsidy by the applicant for the provision of seven affordable units will be approximately \$975,000, which equates to an approximately \$2.07 per square foot contribution. These values are based upon the estimated market value of the units and while prices have not been established, these prices are comparable to other condominium units in Cameron Station.

The standard \$1.00 per square foot contribution would result in \$470,363 for phase VI and phase VII, which based upon the market rate of the units would result in 3-4 affordable units within the condominium building, with the remaining 3-4 units proposed by the applicant going beyond the standard contribution with regard to these two phases.

Staff supports the applicants' proposal because it enables "workforce" households to live in close proximity to the adjoining school, City parkland and the Van Dorn Metrorail Station. The additional units will enable much needed affordable housing units within the City and provide a considerable public benefit for the community and the City.

### ***Linear Park***

Phases III, IV and VI are adjacent to the Linear Park which is on the southern portion of Cameron Station adjacent to the Backlick Run. In conjunction with the Concept Plan approval for Cameron Station, the applicant agreed to dedicate this land to the City including improvements to the park such as with a meandering 10' wide trail, an exercise course, bermed landscaping and trash receptacles prior to dedication to the city.

A 1.9-acre portion of the 7.55 acre Linear Park is located on the southern portion of the site. Completion of this final portion of the Linear Park and trail will connect Ben Brenman Park on the east to Armistead Boothe Park on the west. Sections of the Linear Park have been developed concurrently with the adjacent phase of residential development.

Problems with development and phasing of the Linear Park have occurred in part because the park was not irrigated and because the park was often developed with landscaping installed while construction activity was still on-going. To prevent these problems in the phase VI portion of the Linear Park, staff is recommending and the applicant has agreed to install irrigation in the Linear Park adjacent to phase VI. A recommendation of approval is that construction of the Linear Park be phased to ensure that the park is completed as early as possible. This will likely involve the construction of the units adjacent to the Linear Park as part of the first phase of development.

### ***Open Space – Landscaping***

The proposed open space design provides a combination of open space and pocket parks including the following:

- 3,200 sq. ft. at corner of Ferdinand Day Drive and Tancreti Lane;
- 7,200 sq. ft. for the 30-foot-wide eastern mews connecting Ferdinand Day Drive to the Linear Park;
- 7,670 sq. ft. for the 18 to 45-foot-wide western mews connecting Ferdinand Day Drive to the Linear Park;
- 6,000 sq. ft. for four additional connecting paths between the future extension of Brawner Place and the Linear Park;
- 39,750 as the final addition to the Linear Park; with the remainder scattered throughout Phase VI as sidewalk and adjacent landscape areas and private yards.

A recommendation of approval is that the 3,200-sq.-ft. open space area at the northeast corner of the site (the Ferdinand Day Drive and Tancreti Lane intersection) be designed and landscaped in a manner that will complement the open space across the street within phase V and the proposed open space for phase VII. The proposed northeast open space on phase VI, along with the mews, connecting paths, the new tree-lined frontage on Tancreti Lane and along the Armistead Boothe parking area and the proposed extension of Harold Secord Street will provide high-quality green connecting pathways through the site to the Linear Park.

However, the landscape plan for phase VI requires a significant amount of additional refinement. While the plan proposes an adequate amount of tree canopy coverage, the design of the internal pocket parks spaces do not appear to follow a coherent scheme. Staff is recommending that the landscape design, particularly of the common spaces, be further developed in a coherent scheme to provide open space amenities that are comparable to the existing common space areas and pocket parks in the community. Staff is recommending that the lots adjacent to the pedestrian connections be reduced in size with property lines closer to the building footprint to ensure that the common open spaces and associated pathways are maintained as common areas for the community. Lastly, staff is recommending the applicant switch the townhouses proposed for lot 45 and lot 6 to provide an additional 2,000 square of open space in the eastern mews, and improve the space and the pedestrian connection through this mews.

One concern regarding street trees within Cameron Station has been providing adequate space for street trees adjacent to the front loaded units. A recommendation of staff is to provide an additional eight feet of front setback for the units adjacent to the linear park. This will enable the landscape area devoted to the street trees to be increased in sized from approximately 40 to 100 square feet and will also enable additional visitor parking in the front driveway.

### ***Building Design***

The design, orientation, and scale of the proposed townhouses are consistent with the intent of the CDD Concept Plan and are compatible with the existing development pattern of Cameron Station. A concern of staff is the lack of variation in the roof types and articulation add to the perceived mass of the buildings. A recommendation of approval is to provide more varied roof types roof materials and articulation to reduce the perceived mass of the buildings.

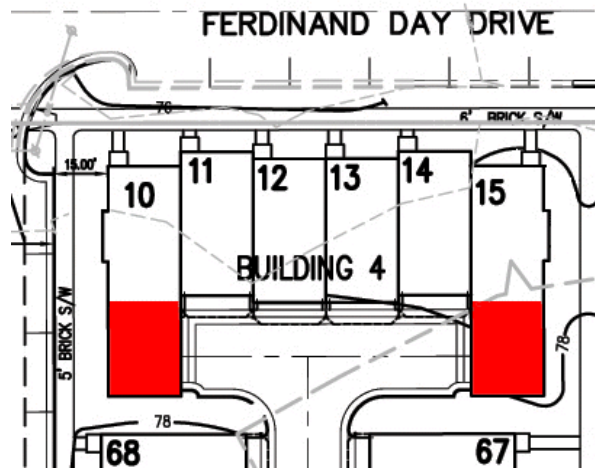
In addition, a recommendation of approval requires additional building articulation for 10 units (lots 1-3, 32-35 and 95-97). This is consistent with the concept for Cameron Station which seeks to provide a variety of unit types, as well as variety in building height and design. This articulation will create a street front that is varied both in elevation and plan that will contribute to a more lively and less monolithic appearance from the streets. These varied and random setbacks create building articulation that creates variety and richness for the street while still maintaining an urban “streetwall.” In addition, a recommendation of approval is that the units adjacent to the linear park be painted brick or significantly varied brick colors to provide additional variation for these units.



A concern that has been expressed about Cameron Station is that the alleys and the rears of the units are too visible from the streets. At the request of staff, the applicant has provided visual screening of the rear alleys and provided a more continuous streetscape adjacent to the streets by re-arranging the townhouses and extending the rears of six townhouses, similar to units that have recently been approved in Potomac Greens and Samuel Madden.



*Elevation as approved for Potomac Greens*



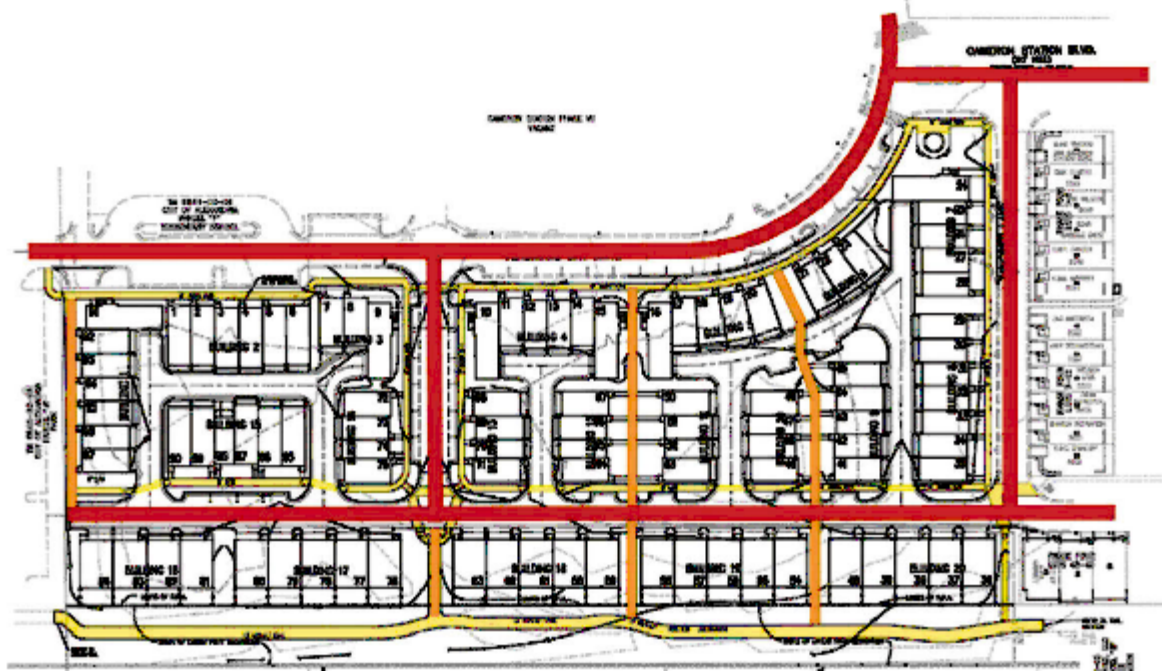
*Site Plan showing building extensions to screen alley*

A recommendation of approval is that the units be limited to brick, precast concrete, stone, or cementitious siding. Where the rears and sides of units are visible from the streets, such as lots 24 and 34, a recommendation of approval is that the front, side, and rear of the unit be brick

### ***Block Size – Street Grid***

A primary concern with the previous proposal for the site was the large, uninterrupted buildings and the fact that the proposed development did not continue the existing modified street grid of Cameron Station. Therefore, from the initial concept meeting, staff has emphasized the importance of continuing the street grid and maintaining internal pedestrian connections to the linear park.

Cameron Station's street system, a modified grid, is a fundamental element establishing the scale and character of Cameron Station. The grid has not been perfectly applied as development on the site has progressed, but as the illustration below shows, the street grid within Cameron Station has fairly successfully broken the project into human-scaled blocks. And where a street has not broken down a block, a pedestrian connection/open space between buildings has generally completed the grid of openings and connections.



*Site Plan Connections - Street Grid (Red), Sidewalks (Yellow) and Pedestrian Connections (Orange)*

Through the concept review process and at the request of staff, the applicant has revised the plan to provide a street grid, internal open space areas, and pedestrian connections for the adjacent linear park. In addition to providing physical connections to the linear park and throughout Phase VI, these connections also provide visual breaks for the proposal.

The streets and pedestrian connections provide physical connectivity throughout the community for vehicles and pedestrians. The street and pedestrian connections within each block also provide residents with visual links through the community, and provide for a coordinated streetscape, helping to tie different phases of the project together into a unified whole. The smaller blocks benefit the public realm by providing more “openness” and porous circulation for both vehicles and pedestrians, both of which are desirable urban design characteristics.

#### *Alley Access and Site Layout*

The site layout provides rear alleys for the units in the project, excluding the units adjacent to the linear park, which all have garages accessed from the front of the units. The internal alleys minimize the number of curb cuts and eliminate garage doors from the adjoining streets – consistent with the neo-traditional design principles and the intent of Cameron Station.

The goal of Cameron Station has been to include streets and streetscapes that emulate the best characteristics of streets in the traditional neighborhoods. The streets and streetscape are important elements of the plan and establish a character in keeping with traditional urban neighborhoods. The sidewalk widths, street trees, brick surfaces, on-street parking, distance between buildings, and minimal curb cuts work together to create an environment that promotes pedestrian activity within this phase. The site plan successfully utilizes the blocks to create a hierarchy of internal open spaces, streetscapes, and a variety of building sizes to create an attractive public realm.

### ***F. Community Concerns***

The community has raised concerns regarding the proposal's provision of adequate visitor parking, common open space; and the impact of additional users on the recreational facilities at the Cameron Club. In a letter from the Cameron Station Community Association to the developer, Greenvest LLC, dated October 16, 2003, the Community Association requested that the developer assist the community in performing an evaluation of the size, layout and equipment provided in the existing fitness center, office, meeting space and pool facilities, to be conducted by an independent consultant to assess the adequacy of the facilities to accommodate 245 additional homes. If the results of the evaluation indicate that the facilities have reached capacity, the community association is requesting that Greenvest assist in providing professional advice and financial contributions to address the shortfalls.

Another one of the issues of concern voiced by the Community Association is the disruption anticipated due to construction of phase VI and VII. In addition to the City regulations regarding construction, staff has included recommendations requiring construction phasing, traffic and parking plans for construction vehicles and workers, and temporary measures to accommodate vehicular and pedestrian traffic. The community association will be forming an ad hoc committee to monitor construction and report to their board and residents, Greenvest and City staff, and ask that Greenvest appoint a representative to sit on the ad hoc committee.

To address the parking issues, the applicant has worked to revise the site plan to provide as many visitor parking spaces as possible within the proposed project while providing open space. As with any development there is a fine balance between the merits of providing additional parking spaces and providing more open space. The applicant has worked with staff to provide the maximum number of surfaces parking spaces without diminishing the quantity and quality of landscaped open space. The proposal provides 20% visitor parking spaces distributed throughout this phase of development. This is more than has been provided in other phases and because it is relatively evenly distributed throughout the development, staff is recommending approval of the amount and location of visitor parking.

The letter from the Community Association closes by relating that for the first time the Board of Directors has taken an official stand on a Greenvest proposal and have voted unanimously to support the phase VI and phase VII proposals.

***H. Staff Recommendation:***

Staff recommends **approval** of the development special use permit with the conditions outlined in the staff report.

**STAFF:** Eileen Fogarty, Director, Department of Planning and Zoning;  
Jeffrey Farner, Chief, Development;  
Stephen Milone, Urban Planner;  
Lorrie Pearson, Urban Planner.

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**STAFF RECOMMENDATION**

Staff recommends **approval** subject to compliance with all applicable codes and ordinances and the following conditions:

1. The final architectural elevations shall be consistent with the level of quality and detail provided in the preliminary architectural elevations dated August 20, 2003. In addition, the applicant shall provide additional refinements to the satisfaction of the Director of P&Z that shall at a minimum include:
  - a. The materials for the front of each unit shall be limited to masonry, precast concrete and cementitious siding as generally depicted on the preliminary plans.
  - b. The front, side and rear facades of lots 24, 35, 41, 49, 53, 64, 71, 75, and 97 shall be brick.
  - c. The front and sides of lots 9, 10, 15, 16, 20, 21, and 23 shall be brick.
  - d. The units adjacent to Ferdinand Day Drive (excluding the radial units) shall provide varying materials and colors and varying roof materials of shingle and metal roofs.
  - e. Exclusive of the radial units and type P units adjacent to the linear park, units of similar style shall be randomly grouped, including groups of 2 or 3 units, in a manner consistent with traditional development patterns.
  - f. Access to the radial units shall be provided on the Ferdinand Day Drive frontage as depicted in the preliminary site plan.
  - g. Stoop heights for all units shall not exceed 48".
  - h. Lots 97, 75, 71, 64, 53, 49, 41, 35, 10, and 9 shall provide a window treatment on the first floor adjacent to sidewalks or internal pedestrian connections that provides the appearance of habitable space and screens the parked cars.
  - i. Paint alternating groupings of front loaded units (adjacent to the linear park) or provide significantly varying brick colors between the groupings of townhomes.
  - j. The fenestration and design for the sides of lots 97, 75, 71, 64, 53, 49, 41, and 35 adjacent to the streets shall be designed to appear more as front facades.
  - k. The wall between lots 23 and 24 on Ferdinand Day Drive shall be a 3-3.5 ft. tall solid brick screening wall and gate.
  - l. The HVAC units and mechanical appurtenances shall be located on the roof-tops or located within the interior alleys, excluding the front loaded units. The air conditioning units for the front loaded units may be permitted in the rear yards. Details on the screening methods shall be indicated on the final site plan. (P&Z)
2. The configuration of the units shall be revised to provide the following:
  - a. Relocate the unit for lot 45 to lot 6 and for lot 6 to lot 45.
  - b. Units 1-3, 32-35 and 95-97 shall provide a minimum of 2-4 ft. setback from the adjoining units to provide building variation in the rows of the townhomes.
  - c. Lots 36-40, 54-58, 59-63, 76-80, and 81-84 shall be setback an additional 8 feet to provide an additional planting area for street trees for the 28' unit types adjacent to the linear park. The rear fences for these units shall be open decorative metal fences. (P&Z)

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3. The internal pedestrian connections shall be part of the common area "Parcel A" and the lots adjacent to the internal pedestrian connection shall be revised to exclude these common areas. The common area ("Parcel A") pedestrian connections shall be a minimum of 20-25 ft. wide. The width between the units shall continue to be 30-40 ft. as depicted on the preliminary plan. (P&Z)
4. A plat of consolidation and final subdivision plan shall be consistent with the final site plan, and shall be approved and recorded prior to the release of the final site plan. The subdivision plan and all easements shall be submitted as part of the first final site plan submission.
5. A subdivision plat for the linear park shall be consistent with the final site plan, and shall be approved and recorded prior to the release of the final site plan. The linear park plan shall be submitted as a separate first final site plan. The linear park shall be a separate final site plan. This plan shall be submitted concurrently with the Phase VI final site plan. (P&Z)
6. The townhouse garages shall contain a minimum unobstructed dimension of 18 ft. x 18.5 ft. for two parking spaces, excluding the six units that shall have two tandem parking spaces. The tandem spaces for these six units shall be full size 9 ft. x 18.5 ft. spaces. Each of the townhouse garages shall also provide a sufficient area for a city standard trash can. (P&Z)
7. All sidewalks for the development shall be brick and shall be a minimum of 6 ft. wide, excluding the sidewalk on the western portion of the development to the satisfaction of the director of P&Z.
  - a. Stamped and colored asphalt crosswalks shall be provided for each of the proposed alley curb cuts and the curb cut on Ferdinand Day Drive to provide uninterrupted brick sidewalks.
  - b. Provide stamped and colored asphalt crosswalk at the intersection of Ferdinand Day Drive and Harold Secord Street connecting the pedestrian sidewalk adjacent to lot 10 to the walkway on the western portion of Phase VII.
  - c. Straighten the pedestrian walkway between lots 44/45 and 46 and provide a stamped asphalt crosswalk across the alley to provide a continuous brick sidewalk.
  - d. Continue the brick sidewalk that is proposed along the western edge of the property south of Brawner Place so that the sidewalk connects to the asphalt trail in the Linear Park.
  - e. The driveways for the front loaded units adjacent to the linear park shall be decorative pavers or brick. (P&Z)
8. A revised landscape plan shall be provided with the final site plan to the satisfaction of the Directors of P&Z and RC&PA. At a minimum the plan shall provide the level and quality of landscaping depicted on the preliminary landscape plan and the plan shall also provide:
  - a. Willow Oak street trees the entire length of Ferdinand Day Drive and London Plane street trees along Tancreti Lane, that are a minimum of 4" caliper at time of planting at a maximum spacing of 30'-35' on-center.
  - b. A continuous row of street trees shall be provide on each side of the internal

- streets(excluding alleys) approximately 30-35 ft. on-center. The trees shall consist of shade trees or columnar species as defined with the Landscape Guidelines and shall not include ornamental trees.
- c. Provide major street tree along the western edge of the development, rather than the smaller scale shade trees proposed, at a minimum of 4" caliper at time of planting and at a maximum spacing of 30' on-center as depicted on the preliminary site plan.
  - d. A significant amount and variety of additional landscaping, including shrubs and groundcover and street trees adjacent to Ferdinand Day Drive, Tancreti Lane, the linear park, and the western edge of the development. Provide a detail of the typical foundation planting for each unit.
  - e. Utility lines such as water, storm sewer and electric lines shall be located to minimize impacts on existing and proposed street trees and open space.
  - f. Street lights shall be located to minimize conflicts with the street trees.
  - g. As private trees mature they are to be limbed up by the HOA to a minimum 6 feet. Trees are not to be planted under or near light poles.
  - h. The maximum height for the shrubs is 36 inches.
  - i. All landscaping shall be maintained by the HOA in good condition and replaced as needed.
  - j. Place underground utilities and utility structures under proposed streets or away from proposed landscaped areas to the extent feasible, to minimize any impact on the root systems of the proposed landscaping, to the satisfaction of the Director of T&ES and the City Arborist.
  - k. All plant materials and specifications shall be in accordance with the current and most up to date edition of the *American Standard for Nursery Stock (ANSI Z60.1)* as produced by the American Association for Nurserymen, Washington, D.C.
  - l. The landscape plan shall be designed by a registered landscape architect.
  - m. Common areas shall be irrigated.
  - n. Common areas shall be designed in consultation with the Cameron Station Community Association. (P&Z) (RP&CA) (Police)
9. Within the portion of the linear park adjacent to Phase VI provide the following to the satisfaction of the Directors of P&Z and RC&PA:
- a. Use a variety of evergreen trees, with less emphasis on white pine.
  - b. Indicate areas to be sod, grass, groundcover, or planting beds.
  - c. Indicate areas within the planting beds to be groundcover, perennials, or grasses.
  - d. Include shrubs, groundcovers, grasses, perennials, benches, and trash receptacles within the linear park landscape design, consistent with the existing linear park and overall character of Cameron Station.
  - e. Provide lighting consistent with the existing lighting of the linear park. Provide details of the light fixture.
  - f. Provide a schedule of dedication for the linear park.
  - g. The linear park trail shall be located over the underground utilities to maximize planting areas for landscaping.

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- h. The applicant shall design and install irrigation in the linear park adjacent to the property to the satisfaction of the Director of R.P.&C.A. The irrigation system shall be maintained by RP.&CA.
  - i. The development shall be phased to ensure that all improvements to the linear park (adjacent to phase VI) shall be completed prior to the issuance of the first certificate of occupancy and after completion of all units adjacent to the linear park. (P&Z) (RP&CA)
  - j. A detailed open space plan shall be approved in conjunction with the final site plan, and any physical elements within the open spaces shall also be shown on the final site plan. The amount of open space and the open space calculations shall not include any portion of the linear park. The dimensions of the interior courtyards shall not decrease from the level generally depicted on the preliminary site plan. The open space, courtyards and linear park shall provide the amenities provided on the preliminary plan. (P&Z)
  - k. The interior pedestrian connections shall provide streetscape and site furnishings to the satisfaction of the Director of P&Z. Provide information delineating the character, location and type of such site furnishings on the final site plan (including but not limited to: benches, lights, trash receptacles, bike racks) and signs or sign systems. Streetscape and site furnishings shall be consistent with that approved and provided in other phases of Cameron Station. The interior courtyards shall be designed to functions as open space and pocket park areas for the use of the residents. The interior open space areas shall provide amenities such as benches, trellis, sitting areas, trash receptacles and decorative pavers and additional amenities to encourage their use for the interior courtyards. These spaces shall be designed by a landscape architect. (P&Z)
10. Freestanding signs other than traffic/directional signs shall be prohibited. (P&Z)
11. A 3-3.5 ft. tall decorative open metal fence shall be provided on the western property line (excluding the linear park) to the satisfaction of the Director of P&Z. Fences of then residential units facing the linear park shall be open, decorative metal, no higher than 6 feet and must have a gate or opening to the park to the satisfaction of the Director of P&Z (P&Z)
12. The final location and screening of all above grade utility structures, such as transformers, switch-boxes, cable and telephone pedestals must be approved by the Director of Planning & Zoning prior to the installation of such structures. To the extent feasible, all such structures shall be located out of view of public streets and shall be screened. (P&Z)
13. The applicant shall prepare and submit a plan that delineates a detailed phasing plan and construction management plan for the entire project for review and approval by the Directors of P&Z, T&ES and Code Enforcement prior to the release the first final site plan for the project. At a minimum, the plan shall include the following:



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- a. Phasing for each required public improvement (streets, traffic signals, sidewalks, etc.).
  - b. A Traffic Control Plan detailing proposed controls to traffic movement, lane closures, construction entrances, haul routes, and storage and staging.
  - c. A plan for temporary pedestrian and vehicular circulation during construction.
  - d. A parking plan for construction workers will be prepared that provides on-site parking for workers.
  - e. Provisions in the event construction is suspended for 6 months or more for:
    1. temporary streetscape improvements;
    2. removal of debris;
    3. screening and barrier protection of construction areas and interim open space improvements.
  - f. Phasing for the construction and completion of the linear park. Units along the linear park shall be completed first to allow for timely completion of the linear park.
  - g. All other necessary phasing parameters deemed necessary by the Directors of P&Z, T&ES and Code Enforcement. (P&Z)
14. The applicant shall provide off-street parking for all construction workers without charge. For the construction workers who use Metro, DASH, or another form of mass transit to the site, the applicant shall subsidize a minimum of 50% of the fees for mass transit. Compliance with this condition shall be based on a plan, which shall be submitted to the Department of P&Z and T&ES prior to the issuance of the Excavation/Sheeting, and Shoring Permit. This plan shall set forth the location of the parking to be provided at various stages of construction, how many spaces will be provided, how many construction workers will be assigned to the work site, and mechanisms which will be used to encourage the use of mass transit. The plan shall also provide for the location on the construction site at which information will be posted regarding Metro schedules and routes, bus schedules and routes. If the plan is found to be violated during the course of construction, a correction notice will be issued to the developer. If the violation is not corrected within ten (10) days, a "stop work order" will be issued, with construction halted until the violation has been corrected. (P&Z)
15. Before commencing any clearing or grading of the site, the applicant shall hold a meeting with all adjoining property owners to review the hauling routes, location of construction worker parking, plan for temporary pedestrian and vehicular circulation, and hours and overall schedule for construction. The Departments of P&Z and T&ES shall be notified of the date of the meeting before the permit is issued. Copies of plans showing the hauling route, construction worker parking and temporary pedestrian and vehicular circulation shall be posted in the construction trailer and given to each subcontractor before they commence work on the project. (P&Z)
16. The applicant shall identify a person who will serve as liaison to the community throughout the duration of construction. The name and telephone number of this individual shall be provided in writing to residents, property managers and business owners whose property abuts the site, and to the Directors of P&Z and T&ES. (P&Z)

17. Prior to the release of the first certificate of occupancy for the project, the City Attorney shall review and approve the language of the Homeowner's Agreement to ensure that it conveys to future market rate homeowners and public housing residents the requirements of this development special use permit, including the restrictions listed below. The HOA language shall establish and clearly explain that these conditions cannot be changed except by an amendment to this special use permit approved by City Council.
  - a. Individual townhouse garages may be utilized only for parking; storage which interferes with the use of the garages for vehicle parking is prohibited.
  - b. Vehicles shall not be permitted to park on sidewalks, in driveways which obstruct sidewalks, on any emergency vehicle easement, or on any portion of the interior alley. The Homeowner's Association shall maintain a contract with a private towing company to immediately remove any vehicles violating this condition.
  - c. No decks shall be permitted, except the decks shown on the approved final site plan.
  - d. Exterior changes or additions to units shall not be permitted without approval of City Council or the Director of Planning and Zoning, as determined by the Director.
  - e. No balconies, bay windows, or any other improvements shall be allowed to encroach into the space above an emergency vehicle easement.
  - f. All landscaping and screening shown on the final landscape plan shall be maintained in good condition and may not be reduced without approval of City Council or the Director of Planning and Zoning, as determined by the Director.
  - g. That heavy industrial uses, including an asphalt facility and a rock crushing facility, the City Waste-To-Energy Plant, the Police Firing Range and Metrorail track and other railway operations are located within the immediate vicinity of the project, are permitted to continue indefinitely, and will generate truck traffic, including empty garbage trucks emanating odors, on the public streets surrounding the project. (P&Z) (T&ES)
18. A temporary informational sign shall be installed on the site prior to the approval of the final site plan for the project and shall be displayed until construction is complete or replaced with a marketing sign incorporating the required information; the sign shall notify the public of the nature of the upcoming project and shall provide a phone number for public questions regarding the project. (P&Z)
19. The applicant shall be allowed to make minor adjustments to the building locations if the changes do not result in the loss of parking, open space, or an increase in the building height or building footprint. (P&Z)
20. Any inconsistencies between the various drawings submitted by the applicant shall be reconciled to the satisfaction of the Directors of Planning and Zoning and Transportation and Environmental Services. (P&Z)
21. Submit a building location survey to the Planning and Zoning staff prior to applying for a certificate of occupancy permit for each unit. (P&Z)

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22. Temporary construction trailer(s) shall be permitted and be subject to the approval of the Director of P&Z. A separate sales trailer will require approval of a special use permit approved by City Council. (P&Z)
23. Temporary structures for sales personnel, as well as sales/marketing signs, shall be permitted, with the size and site design for such temporary structures, including signs, subject to approval by the Director of Planning and Zoning. (P&Z)
24. All utility structures (except fire hydrants) shall be located out of view of public property and rights-of-ways and shall be screened to the satisfaction of the Director of Planning and Zoning. (P&Z)
25. The setbacks of the buildings along the Linear Park shall be increased to 18' from the emergency vehicle easement drive aisle to the building face. For all other buildings, the setback between the buildings and the drive aisles shall be a minimum of 2' to provide adequate turning movements and shall provide a maximum length of 4' or a minimum of 18'. Revise plans to reflect an appropriate setback. (T&ES)(P&Z)
26. The existing sanitary sewer located in Ferdinand Day Drive and the on-site main may be abandoned only after the proposed sanitary sewer main along Harold Secord Drive and Brawner Place (connecting to the existing 10" main) has been completed. (T&ES)
27. Applicant shall provide \$850/ea to the Director of T&ES for the purchase and installation of three (3) of City standard street cans along the public streets. (T&ES)
28. Provide \$10,000 to City for installation of traffic calming measures.(T&ES)
29. The applicant is advised that all stormwater designs that require analysis of pressure hydraulic systems and/or inclusion and design of flow control structures must be sealed by a professional engineer, registered in the Commonwealth of Virginia. If applicable, the Director of T&ES may require resubmission of all plans that do not meet this standard. (T&ES)
30. Plan must demonstrate to the satisfaction of the Director of T&ES that adequate stormwater outfall is available to the site or else developer is to design and build any on or off site improvements to discharge to an adequate outfall. (T&ES)
31. Solid waste services shall be provided by the City unless otherwise approved by the Director of T&ES. The developer must provide adequate space within each unit to accommodate a City Standard super can and recycling container. The containers must be placed inside the units or within an enclosure that completely screens them from view. The developer must purchase the standard containers from the City or provide containers that are compatible with City collection system and approved by the Director of Transportation and Environmental Services. (T&ES)

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32. All public refuse/recycling for buildings 4, 12 and 13 must be placed at the alley entrance for pick up. All public refuse/recycling for units 7, 8, 9, 16, 17, 18, 24, 25, 26, 27, 83, 84, 91, 92, and 93 must be placed at the beginning of the hammerhead for pick up. (T&ES)
33. Show existing and proposed street lights and site lights. Indicate the type of fixture, and show mounting height, and strength of fixture in Lumens or Watts. Provide manufacturer's specifications for the fixtures. Provide lighting calculations to verify that lighting meets City Standards. Note that the lighting fixtures must meet the approved Cameron Station lighting guidelines. (T&ES)
34. All site and building mounted light fixtures shall be shielded to direct light downward and eliminate glare into residential units.(T&ES)
35. Provide all pedestrian and traffic signage to the satisfaction of the Director of T&ES. (T&ES)
36. No overhangs (decks, bays, etc.) shall protrude into the vehicular travelways. (T&ES)
37. All driveway entrances and sidewalks in public ROW or abutting public ROW shall meet City standards. (T&ES)
38. Show all existing and proposed easements, both public and private. (T&ES)
39. Replace existing curb and gutter, sidewalks, and handicap ramps that are in disrepair or broken. (T&ES)
40. The developer shall notify prospective buyers, in its marketing materials, that Harold Secord Street and Brawner Place are private streets and that all on-site storm sewers are private and will be maintained by the Cameron Station Homeowners Association. (T&ES)
41. All private street signs that intersect a public street shall be marked with a fluorescent green strip to notify the plowing crews, (both City and contractor), that they are not to plow those streets. (T&ES)
42. All Traffic Control Device design plans, Work Zone Traffic Control plans, and Traffic Studies shall be sealed by a professional engineer, registered in the Commonwealth of Virginia. (T&ES)
43. If fireplaces are to be included in the development, the applicant is required to install gas fireplaces to reduce air pollution and odors. Animal screens must be installed on chimneys.(T&ES)
44. Due to the historic uses at the site and the potential for contamination, the following condition is included. The applicant shall design and install a vapor barrier and ventilation

system for the buildings and parking areas to prevent the migration or accumulation of methane or other gases under parking areas or into buildings, or conduct a study and provide a report signed by a professional engineer showing that such measures are not needed to the satisfaction of Directors of T&ES and Code Enforcement.(T&ES)

45. The final site plan shall not be released and no construction activity shall take place until the following has been submitted and approved by the Director of T&ES:
  - a. Submit a Site Characterization Report/Extent of Contamination Study detailing the location, the contaminants, and the estimated quantity of any contaminated soils and/or groundwater at or in the immediate vicinity of the proposed site.
  - b. Submit a Risk Assessment indicating any risks associated with the contamination.
  - c. Submit a Remediation Plan detailing how any contaminated soils and/or groundwater will be dealt with, including plans to remediate utility corridors. "Clean" backfill shall be used to fill the utility corridors.
  - d. Submit a Health and Safety Plan indicating measures to be taken during any remediation and/or construction to minimize the potential risks to workers, the neighborhood, and the environment. Submit 5 copies of each of the above. The remediation plan must be included in the Final Site Plan.(T&ES)
  
46. The applicant or its agent shall furnish each prospective buyer with a statement disclosing the prior history of the Cameron Station site, including previous environmental conditions and on-going remediation. These disclosures shall be made to the satisfaction of the Director of Transportation and Environmental Services.(T&ES)
  
47. Due to the close proximity of the site to heavy industrial uses, the City Waste-To-Energy Plant, the Police Firing Range and Metrorail track and other railway operations the following conditions are included in the SUP:
  - a. The applicant shall prepare a noise study identifying the levels of noise residents at the site will be exposed to at the present time and 10 years into the future in a manner consistent with the Noise Guidance Book used by the Department of Housing and Urban Development (HUD).
  - b. Identify options to minimize noise exposure to future residents at the site, including: Special construction methods to reduce noise transmission, including:
    - i. Triple-pane glazing for windows.
    - ii. Additional wall and roofing insulation.
    - iii. Installation of resilient channels between the interior gypsum board leaf and the wall studs.
    - iv. Others as identified by the applicant.If needed, install some combination of the above-mentioned noise mitigation measures or others to the satisfaction of the Directors of Planning & Zoning and T&ES. (T&ES)
  
48. Based on a history of sound transmission complaints, the applicant shall construct all dwelling units with a STC rating of at least 60. (Code Enforcement)

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49. The proposed homes shall be equipped with an automatic fire suppression system. (Code Enforcement)
50. Prior to submission of the Final Site Plan, the developer shall provide a fire flow analysis by a certified licensed fire protection engineer to assure adequate water supply for the structure being considered. The analysis that was previously submitted was not adequate. (Code Enforcement)
51. Applicant shall provide disclosure statement for prospective property buyers informing them of nearby noise related issues from trains, firing range, waste-to-energy plant, asphalt facility, and rock crushing facility to the satisfaction of the Director of Transportation and Environmental Services. (T&ES)
52. The stormwater collection system is part of the Cameron Run watershed. All on-site stormwater curb inlets within the limits of disturbance shall be duly marked to the satisfaction of the Director of T&ES. (T&ES)
53. Provide a drainage map for the area flowing to the chosen BMP, including topographic information and storm drains. (T&ES)
54. During the construction phase of this development, the site developer, its contractor, certified land disturber, or owner's other agents shall implement a waste and refuse control program. This program shall control wastes such as discarded building materials, concrete truck washout, chemicals, litter or trash, trash generated by construction workers or mobile food vendor businesses serving them and sanitary waste at the construction site and prevent its off site migration that may cause adverse impacts to the neighboring properties or the environment to the satisfaction of Directors of Transportation and Environmental Services and Code Enforcement. All wastes shall be disposed off site properly in accordance with all applicable federal, state and local laws. (T&ES)
55. Show the turning movements for vehicles backing out of driveways at units 8, 11, 14, 17, 25, and 92. If necessary, revise plan to better accommodate turning movements for these units. (T&ES)
56. The final site plans shall show placement of fire easement signs. (Code Enforcement)
57. The developer shall submit for reasonable approval by the City Manager an Affordable Housing Plan, which shall provide a total of seven affordable sales units consisting of four one-bedroom units priced at \$175,000 and three two-bedroom units priced at \$225,000. (Housing)

58. The seven affordable set-aside sales units shall be made available to households with at least one member who lives or works in the City of Alexandria, and with household incomes which do not exceed the City's income guidelines for discounted sales units, and subject to the following conditions:
- a. The sales units provided as affordable shall be identical to the market rate units of the same size and type specified in the approved Affordable Housing Plan, and shall have the same amenities as other similar units in the development.
  - b. The developer shall have the right to identify the specific units and the related specifications of the units that are to be provided as affordable, which shall be reasonably disbursed throughout the building, and shown on the Affordable Housing Plan.
  - c. From the time the developer begins to solicit potential buyers until 90 days after the date that the developer begins to write sales contracts at the development, the City, through its Office of Housing, shall have the exclusive right to refer qualified buyers for newly available set-aside sales units, unless the City waives that right.
  - d. Whatever incentives are offered to set-aside unit buyers will be specified in the Affordable Housing Plan.
  - e. The long-term affordability of the set-aside units shall be provided through deed restrictions in accordance with the set-aside resale procedures established in the City's Affordable Housing Policy.
  - f. In the event that the developer offers the market rate units for a price less than the estimated offering price set forth in the approved Affordable Housing Plan by 15% or greater, the developer shall make a proportionate reduction in the price of the equivalent set-aside units, as approved by the City Manager. (Housing)
59. The set-aside units may be sold only to buyers which meet the household income and eligibility conditions in Paragraph 2. In the event that the developer demonstrates that it is unable to secure qualified buyers for all of the set-aside units within a reasonable marketing period acceptable to the City Manager, but in no event later than the date of closing on the last market rate unit to close, the requirement for such sale may be waived with the approval of the City Manager, provided that, for each unit for which a waiver is granted, the developer shall contribute to the Housing Trust Fund the difference between the market rate price for such unit in effect on the effective date of the waiver, and the discounted, set-aside price established under Paragraph 1. (Housing)

**Special use permits and modifications requested by the applicant and recommended by staff:**

1. Development special use permit with site plan to construct a residential development under the Cameron Station Coordinated Development District (CDD#9).
2. Modification to provide 100% of 25% required crown coverage off site on the adjacent common open areas.

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**Staff Note:** In accordance with section 11-506(c) of the zoning ordinance, construction or operation shall be commenced and diligently and substantially pursued within 18 months of the date of granting of a special use permit by City Council or the special use permit shall become void.



CITY DEPARTMENT COMMENTS

Legend: C - code requirement R - recommendation S - suggestion F - finding

**Transportation & Environmental Services:**

- C- 1 Bond for the public improvements must be posted prior to release of the plan.
- C- 2 All downspouts must be connected to a storm sewer by continuous underground pipe.
- C- 3 The sewer tap fee must be paid prior to release of the plan.
- C- 4 All easements and/or dedications must be recorded prior to release of the plan.
- C- 5 Plans and profiles of utilities and roads in public easements and/or public right-of-way must be approved prior to release of the plan.
- C- 6 All drainage facilities must be designed to the satisfaction of T&ES. Drainage divide maps and computations must be provided for approval.
- C- 7 All utilities serving this site to be underground.
- C- 8 Provide site lighting plan to meet minimum city standards.
- C- 9 The applicant shall comply with the City of Alexandria s Noise Control Code, Title 11, Chapter 5, which sets the maximum permissible noise level as measured at the property line.
- C- 10 The applicant must comply with the Chesapeake Bay Preservation Act in accordance with Article XIII of the City s zoning ordinance for storm water quality control which includes requirements for pollutant load reductions and treatment of the Water Quality Volume Default (WQV).
- C- 11 The applicant must comply with the City of Alexandria, Erosion and Sediment Control Code, Section 5, Chapter 4. This includes naming a Responsible Land Disturber on the Erosion and Sediment Control sheets prior to engaging in land disturbing activities in accordance with Virginia Erosion and Sediment Control Law.
- C-12 All required permits from Virginia Department of Environmental Quality, Environmental Protection Agency, Army Corps of Engineers, Virginia Marine Resources must be in place for all project construction and mitigation work prior to release of the final site plan. This includes the new state requirement for a VPDES permit for all construction activities greater than 1 acre.

Code Enforcement:

The following are repeat comments from a review on 6/6/03 & 7/23/03. Updated comments from 8/28/03 review are noted in **BOLD**.

- C-1 The developer shall provide a separate Fire Service Plan which illustrates: a) emergency ingress/egress routes to the site; b) two fire department connections (FDC) to the building, one on each side/end of the building; c) fire hydrants located within one hundred (100) feet of each FDC; d) on site fire hydrants spaced with a maximum distance of three hundred (300) feet between hydrants and the most remote point of vehicular access on site; e) emergency vehicle easements (EVE) around the building with a twenty-two (22) foot minimum width; f) all Fire Service Plan elements are subject to the approval of the Director of Code Enforcement. Additional fire hydrants are required on the site. Roadways within the development shall be declared emergency vehicle easements. The hammerhead turnaround behind lots 10 to 15 has shallow pockets and cannot accommodate emergency vehicle turnarounds. Pocket depth shall be a minimum of 60 feet. As an alternative, the green space area between lot 15 and 16 shall be H20 rated and equipped with mountable curbs to the satisfaction of the Director of Code Enforcement and Transportation & Environmental Services. This area shall provide a 22 foot EVE connection between the two proposed streets. **Condition not met.** Fire Service Plan shall be submitted as an additional sheet within the set of plans and shall be submitted prior to the final site plan for review. Issues concerning EVE access and hydrant distribution must be addressed prior to final site plan. EVEs are not identified on the submitted plan. EVE access issues for Lots 64-67 has not been addressed. Hydrant spacing and distribution within the complex is inadequate. **Fire Service Plan not submitted. Vehicle turning movement submitted in lieu of Fire Service Plan. Please submit Fire Service plan as described above. EVE is still not shown on plans for lots 64-67, lots 78-84 and lots 92-97. Add 4 additional fire hydrants and relocate one hydrant from lot 85 to lot 75 as previously provided by Code Enforcement.**
- C-2 New construction must comply with the current edition of the Uniform Statewide Building Code (USBC). Condition met. Condition listed as General Note 2 on page C1.00.
- C-3 A soils report must be submitted with the building permit application Acknowledged by applicant.
- C-4 A Certificate of occupancy shall be obtained prior to any occupancy of the building or portion thereof, in accordance with USBC 118.0. Acknowledged by applicant.
- C-5 Prior to the issuance of a demolition permit or land disturbance permit, a rodent abatement plan shall be submitted to Code Enforcement that will outline the steps that will taken to prevent the spread of rodents from the construction site to the surrounding community and sewers. Acknowledged by applicant. Include rodent abatement note as a general note on plans. **Condition Met. Shown as Note 18 on Sheet C1.00.**

Historic Alexandria (Archaeology):

No comments