

Docket Item #13
DEVELOPMENT SPECIAL USE PERMIT #2004-0001
THE PRESCOTT (Hopkins Warehouse)

Planning Commission Meeting
September 9, 2004

ISSUE: Consideration of a request for a development special use permit, with site plan and modifications, for construction of a residential condominium building with underground parking.

APPLICANT: Cameron Street L.L.C., c/o Carr Homes, Inc. by
J. Howard Middleton, Jr. Attorney

LOCATION: 1115 Cameron Street and 200 North Henry Street

ZONE: CD/Commercial Downtown

SITE GRAPHIC
(AVAILABLE IN THE PLANNING AND ZONING OFFICE)

I. EXECUTIVE SUMMARY

A. Overview:

The applicant is requesting approval of a development special use permit to permit an increase in density from 35 d.u./acre to 54.45 d.u./acre, as well as an additional 20% increase in density and floor area ratio to provide on-site affordable units. The proposed three- to four-story building will be located on the southern portion of a block surrounded by the streets of Cameron, Henry and Fayette. The proposal consists of 64 condominiums and one level of below-grade parking accessed from Fayette Street. The site currently contains two buildings – a former one-story furniture warehouse (Hopkins Warehouse) and a one-story service station.

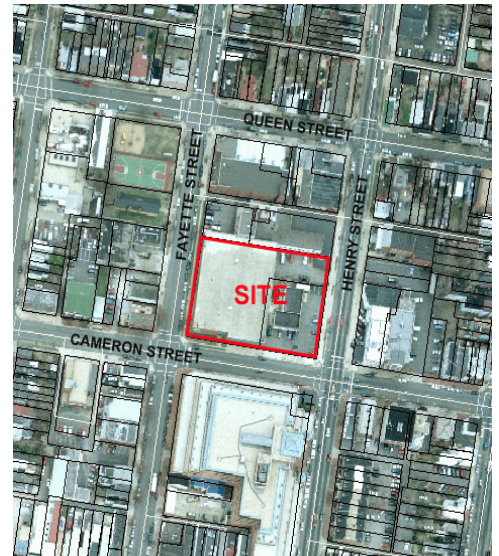


Figure 1 - Site Aerial

The site is located within the Parker Gray Historic District. On June 23, 2004, the Parker Gray Board approved the demolition of the existing warehouse and service station and the conceptual plans for the development, subject to the condition that the apartment segment at the corner of Cameron and Henry Streets be revised to appear as a three-story building, and that the mass of the northern building on Henry Street be reduced.

The site is in a transitional location between the typically lower-scale vernacular townhouses within Parker Gray to the north and west, King Street to the south, and the Old and Historic District to the east. Therefore, it is critical to ensure that the mass and scale of the proposal appear as a natural extension of the fabric of these three neighborhoods. The redevelopment of this site presents numerous challenges and opportunities that include:

Challenges

- Providing a mass and scale that are compatible with the adjoining Parker Gray District, Old and Historic District, and Henry Street.
- Ensuring that the building is well designed as one of the first significant redevelopment projects within Parker Gray.
- Maintaining a level of open space and “openness” for the development.
- Providing sufficient parking.
- Minimizing traffic impacts.



Figure 2 - Site Plan

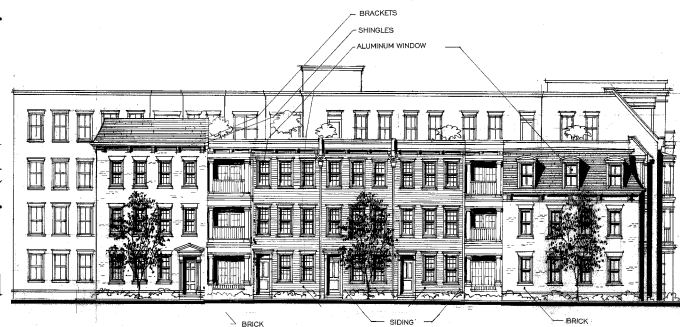


Figure 3 - Fayette Street Elevation

Opportunities

- Redevelopment of a vacant site that is considered a redevelopment site within the Braddock Road Metro Station Small Area Plan.
- Provision of on-site affordable housing.
- Provision of pedestrian and streetscape improvements.

While staff supports the redevelopment of the site and the proposed affordable housing, the additional density proposed to provide the affordable housing results in a mass and scale that are inconsistent with the scale of existing buildings on Henry Street and the neighborhood. Staff recommends approval of the proposal with significant changes to the mass, building design, open space and parking as discussed in detail below. These recommendations of approval are consistent with and reflect the findings of the Parker Gray Board, and are necessary to ensure that the development is compatible with the scale of the neighborhood, is consistent with the historic building pattern in the area, and provides an appropriate amount and quality of open space.

B. Issues with the Current Proposal:

Building Design/Mass and Scale

One of the characteristics that adds to the variety, richness and texture of the District is the variation of building styles and massing that occurs within each block. This is due to the fact that buildings within each block have been constructed over time, resulting in each block containing a collection of buildings, architectural styles and roof forms that are character-defining elements of Parker Gray and Old Town. Staff has encouraged the applicant to design the building to appear to be a collection of buildings that have been constructed over time, similar to the existing character of the district. The applicant has worked with staff to address this design issue.

The remaining area of concern for staff, as well as the Parker Gray Board, is the scale of the building at the intersection of Henry Street and Cameron Street and the northern portion of the building on Henry Street. Therefore, staff recommends that a portion of the building be set back at the corner of Cameron and Henry Street so that the fourth level will not be visible, as depicted in Figure 5, and that the northern portion of the facade on Henry Street be revised as depicted in Figure 16. These proposed changes will likely result in the elimination of some of the units or a change in the number of one and two bedroom units. These revisions are necessary to ensure that the proposed building is compatible with the existing two- and three-story buildings on Henry Street. In addition to reducing the overall height, the proposed modifications provide additional variation in the rooflines and greater building articulation, both of which help to reduce the perceived mass of the building. With these changes, the overall massing will reflect the comments of the BAR and can be supported by staff.



Figure 4 - Applicant Proposal - Cameron Street



Figure 5 - Staff and BAR Proposal - Cameron Street

Open Space and Streetscape

The proposal provides 26% ground-level open space and 14% open space on the rooftop terrace, for a total of 40% open space. The ground-level open space consists of the front yards on Fayette and Henry Streets and an internal courtyard. Conditions have been included to:

- Require the ground-level open space to be enhanced through additional landscaping, irrigation and fencing;
- Require improvements to the central courtyard, such as a focal element, special paving surfaces and additional amenities. Improved design of the rooftop terrace;
- Improve and increase the amount of ground-level open space by reconfiguring some surface parking; and
- Providing a \$10,000 contribution for pedestrian, landscape and signage improvements to the nearby Hunter Miller Park.

Visitor Parking

Typically, the City requires that a minimum of 15% visitor parking be provided in conjunction with new residential developments, except in those locations where adequate on-street parking is available to meet visitor demand. The applicant is requesting approval to provide 7 of the 17 (15%) visitor parking spaces on the adjoining streets and to provide all of the parking spaces as “hybrid” (smaller than a standard space but larger than a compact space) parking spaces. A parking study submitted by the applicant indicates that 50 to 85 (24% to 41%) of the on-street parking spaces within a block

of the site are available during peak visiting times. Additionally, the proposed development eliminates three curb cuts, which will allow the creation of an additional five to seven on-street parking spaces directly adjacent to the site. Staff is recommending approval to provide a limited amount (7 spaces) of the visitor spaces on the adjoining street because of the availability of parking indicated by the parking study. In addition, staff is recommending that the residents not be eligible for residential parking permits to ensure that the residents park within the underground garage. Staff believes this is an acceptable balance between open space and parking needs.

Affordable Housing

The applicant's proposal includes a request for a 20% increase in the allowable FAR and density in exchange for providing affordable dwelling units on-site. The applicant is proposing a total of 64 dwelling units, 10 of which are the "bonus" units that are allowed because of the affordable housing contribution. The applicant proposes to provide four affordable dwelling units in exchange for the bonus units. The estimated value of this contribution is approximately \$451,800. Staff believes that this is an appropriate location for affordable housing, as it is adjacent to services and transportation. However, as discussed above, staff believes that the additional mass proposed by the applicant in exchange for affordable units needs to be revised to provide a development that is compatible with the scale and character of the neighborhood. Reducing the mass and scale could result in a loss of up to four units, which in turn could result in a loss of affordable units under the current affordable housing proposal.

C. Community:

The applicant and staff conducted community meetings for this project with the Inner City Civic Association and adjoining residents and two hearings by the Parker Gray Board of Architectural review. The primary issues identified by the community include:

- Minimizing the traffic impact of construction on North Henry and Cameron Streets.
- Increasing variation in roof height.
- Reducing the mass and scale along Cameron and Henry Streets.
- Making Fayette Street pedestrian-friendly.
- Adding more ground-level open space, visible to the street.
- Setting an appropriate precedent for future redevelopment.

To address these concerns, staff has included conditions for construction phasing, reduction in the overall massing, streetscape improvements, additional open space and landscaping.

II. BACKGROUND

Site Description

The property is located at the southeastern boundary of the Parker Gray District. The remainder of the block is developed with one- and two-story buildings that consist of a mix of uses, including auto repair, a bookstore, hair salons, a restaurant, and a church.

The surrounding blocks to the east, north, and west are generally developed with two and three-story buildings. The block to the south, across Cameron Street, is developed with a seven-story, tiered office and retail building and a two-and-a-half story single-family house used as a commercial office.

The site consists of two lots and contains two vacant buildings – a former one-story furniture warehouse (Hopkins Warehouse) and a one-story service station (All Tune and Lube). The site is essentially flat and is almost entirely covered with impervious surfaces.

Proposal

The applicant has worked with staff to address many of the concerns raised by staff and the community and has made considerable changes to plans. The applicant's initial proposal was for a four-story condominium building. The fourth story was set slightly back from the first three floors, but otherwise the proposal was designed as a single massive four-story building with only moderate building articulation. In subsequent iterations of the plan, the applicant reduced portions of the building to three stories, designed the project to have the appearance of multiple buildings of varying styles, increased the setbacks on Fayette Street, and provided greater building articulation.

The proposed building will be located along Cameron Street between North Henry Street and North Fayette Street, with a footprint of approximately 175 by 250 feet. The 64-unit residential condominium building is proposed to consist of five one-bedroom units (approximately 750 square feet each) and 59 two-bedroom units (1,050 to 2,000 square feet each). The building will be three to four stories in height. Vehicular access will be via Fayette Street, and parking will be provided in a 110-space underground parking garage and a surface parking area consisting of 10 spaces for visitors, for a

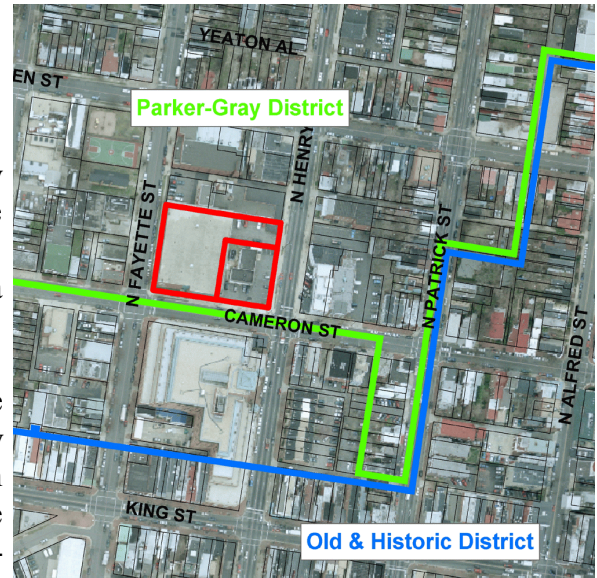


Figure 6 - Historic District Boundaries



Figure 7 - Building to the East



Fig 8 - Buildings to the South

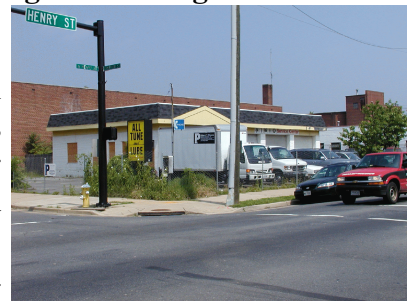


Figure 9 - Site

total of 120 off-street parking spaces. Controlled access will be provided for the underground garage. The primary pedestrian and visitor access into the building will be from Cameron Street and from the parking area at the rear of the building, midway between North Henry and North Fayette Streets.

Along Henry Street, the building will be three to four stories, with a maximum height of 48 feet. The fourth level will be set back so that along parts of Cameron and Fayette Streets the building will appear to be three stories. Terraces will be located on the top of the three-story section. Along Fayette Street, which is closer to the center of the Parker Gray District, the articulation of the building are smaller townhouses with the small front yards adjacent to Fayette Street, similar to other townhouses within Parker Gray district, such as those pictured on West Street.



Figure 10 - West Street Townhomes

The proposed building is U-shaped with an interior courtyard. The building will be constructed nearly to the street line on Cameron Street, but set back approximately 8 feet from North Henry Street and approximately 15 feet from North Fayette Street.

Parker Gray Board of Architectural Review:

The subject property is located in the Parker Gray District, and on June 23, 2004, the Board approved the demolition of the existing warehouse and service station, with the condition that demolition does not take place until City Council approves the special use permit. At that same meeting, the Board approved the conceptual plans for the development, with the condition that the apartment segment at the corner of Cameron and Henry Streets be revised to appear as a three-story building. Additionally, the BAR directed the applicant to address mass and scale concerns raised by staff.

Affordable Housing

The development proposal includes a request for a 20% increase in the allowable FAR and density in exchange for providing on-site affordable housing, and in accordance with Section 7-700 of the Zoning Ordinance. The applicant proposes to provide one-third of the “bonus” dwelling units as affordable, and pay a cash contribution of \$1 per gross square foot of “non-bonus” dwelling units. While this would equate to three dwelling units (3.3 units, rounded down) and approximately \$62,650 in cash, the applicant is instead proposing to provide 4 affordable dwelling units as discussed in more detail below.

III. ZONING:

The applicant is requesting approval for a development special use permit for condominiums in the CD Commercial Downtown District.

THE PRESCOTT		
Property Address:	1115 Cameron Street and 200 North Henry Street	
Total Site Area:	43,585 square feet (1.00 acre)	
Zone:	CD Commercial Downtown	
Current Use:	Warehouse and Vacant Service Station	
Proposed Use:	Condominiums	
	<u>Permitted/Required</u>	<u>Proposed</u>
FAR	1.25	1.50*
Density	35 d.u./acre, by right 54.45 d.u./acre, w/ SUP	63.96 d.u./acre*
Open Space	40%, 17,434 s.f.	40.2%, 17,527 s.f.
Height	50 feet	48 feet
Side Yard	25'; 1:2	14.6'; 1:3.3**
Parking, inc. visitor parking	127	120
* Special Use Permit Requested.		
**Modification Requested		

IV. STAFF ANALYSIS:

The site is identified in the Braddock Road Metro Station Small Area Plan as a potential redevelopment site, and represents one of the largest redevelopment sites in the Parker Gray District. Both staff and the community support redevelopment of the site. The site is located on the visually prominent intersection of Henry and Cameron Streets. Situated near the edge of the Old and Historic Alexandria District, the site serves as an important transition property between the two historic districts.

The applicant is requesting a special use permit for a 20% increase in density to provide affordable units. There is a balance between approving additional density for much needed on-site affordable units and approving a mass and scale compatible with the context of this relatively modest scale neighborhood. In fact, one of the reasons for the creation of Parker Gray District in 1984 was to ensure that the opening of the Braddock Metro Station in 1982 would not result in the loss of the architectural character and scale of the neighborhood. There are also several adjoining sites and small infill sites on Henry Street that may redevelop in the future.

Staff strongly supports affordable units at this location; however, the increased density allowed with the provision of affordable units, will result in a mass and scale that are significantly greater than the adjoining buildings. The proposed four-story elements on Henry Street will overwhelm the adjoining two to three-story buildings. Therefore, staff recommends elimination of portions of the fourth level on Henry Street to provide a better transition to the height on Henry Street and revisions to the overall design. These recommendations are also consistent with the concerns expressed by the Parker Gray Board and members of the community. With these revisions the overall massing will be more compatible with Henry Street and the adjoining Parker Gray District, provide additional variation on Henry Street, and address concerns expressed by the community. Staff believes that with the recommended changes to the building design, massing, parking, and open space, the proposed development can be a positive addition to the Parker Gray neighborhood.

A. Building Design/Mass and Scale

The site is located within a “transitional area” on the boundary of the Parker Gray District, Old and Historic District and a large office building on Cameron Street. Each of these street frontages has a different scale and character.

Fayette Street

The Fayette Street elevation is the shortest (132.5 feet) facade and is intended to be smaller in scale in response to the buildings on the west side of Fayette Street and to be compatible with the generally smaller scale of the Parker-Gray District. In addition, the entire facade is set back from the sidewalk by approximately 15 feet to mimic the front yards of many houses of the District, such as the townhouses in the 400 block of North West Street. The Fayette Street facade is generally well resolved in mass, scale and architectural character.

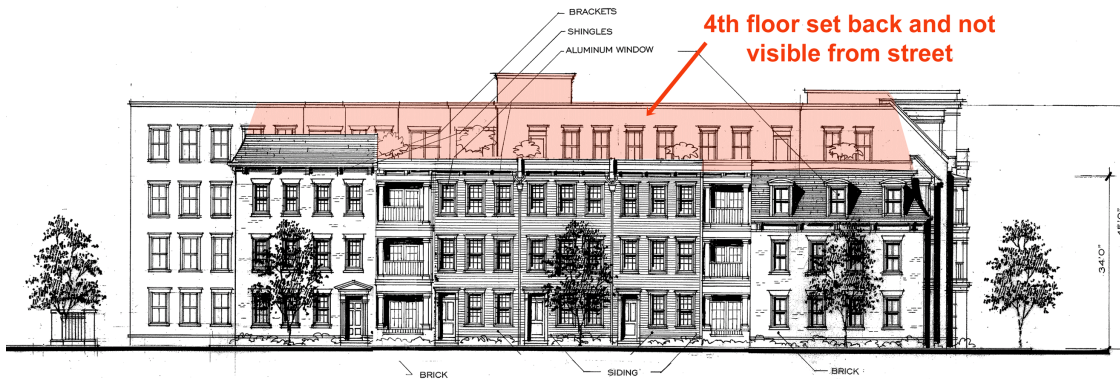


Figure 11 - Fayette Street Elevation

Staff recommends the following changes to the Fayette Street elevation:

- Each of the doors as depicted on the preliminary elevations shall be operable.
- The top floor of the balconies-hyphens shall not include a roof.
- The three central townhouses shall consist of cementitious siding.
- That the roof and parapets be designed in a way to screen the roof-top deck.
- The three central clapboard townhouses shall be recessed 2' from the adjoining facades.

Cameron Street

The Cameron Street elevation is the longest elevation (230 feet) and contains the principal entrance for the condominium building. The proposed building consists of three sections, the longest being the four-story apartment building segment, followed by a section consisting of three 3-story townhomes with full mansards, and then, at the corner of Fayette and Henry Streets, a two-story house with full mansard. The corner house and townhouses are linked by a smaller segment intended to appear as an architectural hyphen or wing.



Figure 12 - Applicant's Proposed Cameron Street Elevation

Overall, the proposed facades of an apartment building, three townhouses and a single-family building on the corner provides an appropriate mix of building types for this portion of the City. However, the primarily four-story element on the corner of Cameron and Henry Street is inappropriate to the scale and fabric of the neighborhood. Therefore, staff recommends that a portion of the fourth floor be eliminated, as generally depicted in Figure 13, so that the building will appear as a three-story building from the adjoining streets. This was also a recommendation of the Parker Gray Historic District Commission and a concern expressed by the community. The modification would result in the elimination of up to 4 units and 4,400 square feet of floor area. This elimination of mass at the corner is essential to enable a better transition to the adjoining two- and three-story buildings.



Figure 13 - Staff Proposal - Cameron Street

The following changes to the Cameron Street elevations are also recommended:

- The balcony-hyphen shall be revised to be subservient to the single-family building.
- The fourth level balconies for the three central townhomes shall be converted to dormers.
- The two-story balconies for the three central townhomes shall be converted to enclosed balconies consistent with the architectural style of the units.
- The eastern portion of the facade shall be revised per Condition #2.
- The setback for the hyphen shall be at least 8 feet, recessed behind the adjoining building faces, and the balconies shall either be flush with or recessed behind the adjoining building faces.

With these revisions the facade will appear to be two- to three-story, consistent with the character of the historic district. The proposed revisions to the mass also provide more variation in the roof heights, further reducing the perceived mass and scale of the proposed building.

Henry Street

The Henry Street elevation is 159 feet long and consists of three major segments. Starting from the south at the corner with Cameron Street is the continuation of the multi-family segment. This segment is intended to have the appearance of a secondary elevation of a mid-size multi-family block with a recessed wing extending to the north. The multi-family segment is followed by a four-bay, gable-roofed townhouse and a two-bay hyphen. The Henry Street elevation is terminated by a substantial, five-bay, gable-roofed townhouse segment on the north end.



Figure 14 - Applicant's Proposed Henry Street Elevation

As previously discussed, this facade is the one to which significant revisions need to be made before the mass and scale are compatible with the existing character of Henry Street. As with the Cameron Street facade, staff recommends that the building be designed to appear as a three-story building and that a portion of the building be redesigned with more townhouse bays as generally depicted in Figure 16.

The other concern raised by the Parker Gray Board and shared by staff is the perception of the northern portion of the building on Henry Street. The proposed three- to four-story element is significantly larger than adjoining buildings and will provide an inappropriate transition to the adjoining Bradham auto repair facility. While the Bradham property may redevelop in the future, the northern portion of the building needs to be designed at a scale that is appropriate for the existing condition rather than an assumed future condition. The scale is also significantly greater than the adjoining two- to three-story buildings on Henry Street. For all of these reasons, staff is recommending that the north end of the building be reduced to three stories to provide a better transition. The scale and height of the proposal, even with the staff recommendations, will be considerably more than the one-story gas station and two-story building that are on the site today.



Fig 15 - Massing Study (Proposal in Pink)



Figure 16 - Staff Proposal - Henry Street

B. Green Building

In order to minimize the environmental impact of the proposed building, staff recommends that the developer incorporate green building technology into the design. A green building will incorporate elements such as non-toxic or recycled-content building materials, maintain good indoor air quality, use water and energy efficiently, feature natural lighting, and use energy-efficient window systems. A recommendation of approval is that the applicant incorporate these or comparable elements as part of the building design.

C. Open Space and Pedestrian Streetscape

The CD zone requires 40% open space, a portion of which may be roof-top decks. The proposal provides 40% open space, 26% of which will be at grade level, consisting of front yards and an internal courtyard, with the remaining 14% as a third-floor rooftop terrace. The proposed front yards on Fayette are visually accessible from North Fayette Street, consistent with other townhouses within Parker Gray that have small front yards. Additionally, the applicant is proposing landscaped areas along Henry Street and the north property line, and a central courtyard to serve residents.

Staff has concerns regarding open space that are related to both quantity and quality. To address the issue of quality, additional landscaping, irrigation and amenities such as benches are recommended to make the ground-level open space more useable and attractive. In addition, staff recommends that the four parallel, at-grade parking spaces be eliminated, that all of the surface parking spaces be compact spaces, and that two additional 90° parking spaces be added. This reconfiguration will result in the net loss of two parking spaces, but will provide additional open space, increase the buffer from the adjoining auto repair facility, and increase the amount of visible open space from Fayette Street.



Figure 17 - Landscaping Plan

In order to enhance the pedestrian environment on this block, staff recommends generous 10 foot wide brick sidewalks with street trees. This will significantly enhance an environment which is now characterized by curb cuts and limited landscaping. Staff is further recommending additional improvements, including:

- Street trees and landscaping along North Fayette, Cameron and North Henry Streets;
- Sodded lawns;
- Brick sidewalks;
- Stamped crosswalks;
- Bicycle parking; and
- Locating all overhead utilities below grade.

While the applicant is providing 26% of the site as ground-level open space, a significant portion of that ground-level open space is within the internal courtyard and is not visually or physically accessible to the public. Therefore, staff recommends that the applicant contribute \$10,000 for additional landscaping and park improvements to the nearby Hunter Miller Park (on the west side of Fayette Street), benefitting both the residents of the proposed development and the adjoining neighborhood.

D. Parking

The applicant has provided the on-site parking required by the zoning ordinance for each condominium unit, plus 10 (9%) surface parking spaces for visitors. Typically, a minimum of 15% visitor parking is required in conjunction with new residential developments, except in locations the applicant can demonstrate through a parking study that adequate on-street parking is available to meet visitor demand. This approach is one that requires an evaluation based upon the size and location of each project and staff will continue to closely evaluate with each application. In other cases, on-street parking is unavailable or in short supply, percentages higher than 15% have been applied. For example, recent developments have been required to provide as much as 20% visitor parking (Old Town Crescent), or as little as no on-site visitor parking at all (Braddock Lofts).

At the request of staff, the applicant submitted a parking study prepared by Gorove/Slade Associates, Inc. The parking study shows parking availability in the area in the evenings and on weekends, the peak periods for visitors. The analysis showed that during all times over five sample periods there is significant available parking for the proposed development within a one-block radius.

Average Daily Parking Availability – 1-Block Radius

Study Period	Total Spaces*	% Available	# Available
Thursdays (2 evenings)	206 Spaces	41% Available	85 Spaces Available
Friday (2 evenings)	206 Spaces	29% Available	59 Spaces Available
Saturday (2 days)	206 Spaces	24% Available	50 Spaces Available
Saturday (2 evenings)	206 Spaces	40% Available	81 Spaces Available
Sunday (2 days)	210 Spaces	28% Available	58 Spaces Available
Averages	207 Spaces	32% Available	67 Spaces Available

*Note – the total number of spaces varies due to day-specific parking restrictions.

The streets adjacent to the site are Henry Street, Cameron Street, and Fayette Street. Henry Street and Cameron Street restrict parking for non-permit holders to two hours, while Fayette Street has no parking restrictions other than to prohibit parking two hours a week for street cleaning. The parking study indicates that within a 1-block radius, during peak visiting times, an average of 67 (32%) and a minimum of 50 (24%) on-street parking spaces are available. Therefore, the six visitor spaces which will not be provided on-site can easily be accommodated on the surrounding streets. Although the public streets can accommodate the expected number of periodic visitors, staff is concerned that the residents adjoining the public streets will find it more convenient to park on the streets rather than within the underground garage. Therefore, staff is recommending that a condition

of the parking reduction approval be that none of the residents of the development be eligible to apply for or receive any residential parking permits pursuant to City Code Sec. 5-8-71.

As discussed above, staff recommends the elimination of 2 surface parking spaces to increase open space and provide a landscaped buffer. This would reduce the number of visitor parking spaces to 8. However, adequate on-street parking is available within one block of the site to accommodate any visitors who cannot park on site, and the parking lot reconfiguration provides an appropriate balance between open space and parking needs.

E. Traffic and Site Access

Based on the applicant's traffic assessment report, this development will generate approximately 440 trips per day (220 entering and 220 exiting). During the AM peak hour, 37 trips will be generated (7 entering and 30 exiting), while during the PM peak hour, 42 trips will be generated (29 entering and 13 exiting). At build-out in 2006, site-generated traffic is expected to constitute 11 percent of the AM peak hour traffic on Fayette Street and 8 percent of the PM peak hour traffic. The total 2006 traffic volume on Fayette Street at the project site is projected to be approximately 320 vehicles during the AM peak hour and 510 vehicles during the PM peak hour. During both peak hours, these volumes are well below the operating capacity of Fayette Street.

At build-out, site-generated traffic will increase traffic demand at the adjacent intersections of Fayette Street with Cameron and Queen Streets by one to five percent during the peak hours. Both intersections currently operate at Level-of-Service A or B during the AM and PM peak hours, respectively. At build-out, these operating conditions are not projected to change. At build-out plus ten years, the applicant's traffic analysis concluded that both intersections will continue to operate at acceptable Levels-of-Service.

Overall, the traffic assessment concluded that the proposed development will not adversely impact Fayette Street or its intersections with Cameron and Queen Streets.

Access

Several of the adjoining residents have suggested that site access for this development be relocated from Fayette Street to Cameron Street in order to reduce potential impacts to Fayette Street. Staff appreciates this neighborhood concern; however, the findings of the traffic assessment do not suggest the need for such change in site access. The predominate access and egress patterns for this development are expected to be via Cameron and Queen Streets to the 200 block of Fayette Street where the site entrance is proposed. This access pattern combined with the existing through-traffic restriction on Fayette Street north of the site will effectively minimize any site-generated impacts.

F. Affordable Housing

The applicant is requesting a 20% increase in allowable FAR and density in exchange for providing affordable housing. The affordable housing bonus will yield 10 more dwelling units than would otherwise be allowed. Under the City's current program, the applicant is asked to provide one-third of the "bonus" dwelling units as affordable, plus pay a cash contribution of \$1 per gross square foot

of “non-bonus” dwelling units. This would equate to 3 dwelling units (3.3 units, rounded down), plus approximately \$62,650 in cash. As an alternative, the applicant proposes to provide 4 affordable dwelling units, three 2-bedroom units and one 1-bedroom unit, and no cash contribution. The proposal has been accepted by the Affordable Housing Advisory Committee.

The table below provides a comparison of the affordable housing contribution that is being provided for other recently approved projects. The table also lists the proposed contribution for the Prescott, as well as the approximate contribution that would be provided if reductions in building massing result in the loss of four dwelling units (the maximum number of units that staff anticipates may be lost).

Comparison of Affordable Housing

Project	Type	Units	Aff. D.U.s	% of Units	Value of Contribution	Contrib. per s.f.	% of Standard Contribution
Mill Race	Sales	326	13	4.0%	\$1,229,600	\$2.01	201%
Mill Race	Rental	369	15	4.1%	*	*	*
Preston	Sales	63	6	9.5%	\$100,435	\$1.04	104%
Cameron Station Condos	Sales	148	7	4.7%	\$975,000	\$2.07	207%
Northampton	Rental	572	25	4.4%	\$1,605,680	\$2.73	273%
Park Center	Sales	173	9	5.2%	\$720,000	\$3.48	348%
Prescott Proposed	Sales	64	4	6.3%	\$451,800	\$5.87	587%
Prescott Alternative	Sales	60	2	3.3%	\$312,650	\$4.06	406%

*Note – figures not available, but the \$2.01 per gross square foot contribution value applies to the entire Mill Race project, including the office building.

V. Conclusion

The subject property is in a transitional location between the Parker Gray District, King Street, and the Old and Historic District. Therefore, it is critical to ensure that the mass and scale of the proposal is appropriate, and that the development appears to be a natural extension of the fabric of these three neighborhoods. As such, the proposed condominium building has been designed to appear to be multiple buildings with a variety of styles and types, including an apartment building, single-family homes, and townhomes. The project is designed with four stories on Cameron Street and Henry Street, to provide a transition to the three stories along Fayette Street. However, additional changes are needed in order to ensure that the project will not be out of character with the surrounding neighborhood. Most critical is for the portion of the building that is at the corner of Cameron and Henry Streets to be designed to have the appearance of a three-story building, so that it is not out of character with other buildings on Henry Street. The reduction in the overall mass and scale and the provision of affordable housing provide a project that is more compatible with the character and scale of the neighborhood.

VI. STAFF RECOMMENDATION:

Staff recommends **approval** subject to compliance with all applicable codes and ordinances and the following conditions:

Building Design:

1. The building at the corner of Henry and Cameron Street shall be revised as depicted in *Attachment # 1* to the satisfaction of the Director of P&Z. The revisions to this portion of the building shall at a minimum include the following:
 - a. The fourth floor shall provide a setback of approximately 15 ft. and shall not be visible from the adjoining sidewalks.
 - b. A portion of the building shall be converted to appear as two townhouses on Henry Street.
 - c. The height of the northern portion of the building on Henry Street shall be reduced in height to appear as a two-story element with a roof and dormers. (P&Z)

2. The final design for the building shall be consistent in massing, articulation, general design and the architectural styles and character as generally depicted in the architectural elevations dated August 23, 2004, and also provide additional refinements to the satisfaction of the Director of P&Z that shall include:
 - a. With the exception of the multi-family building entrance, all entrances should replicate the scale, pattern, and architectural detail of a typical townhouse entrance.
 - b. With the exception of the screening for the mechanical equipment, the materials of the entire building shall be entirely masonry (brick, precast, stone), except the three townhouses on Fayette as required below.
 - c. The proportions of the windows, doors and design elements shall be consistent with the architectural style of each facade.
 - d. The HVAC units and mechanical appurtenances shall be located on the roof-tops, recessed and screened from view from the public streets. Details on the screening methods shall be provided on the final site plan.
 - e. Color elevations shall be submitted with the final site plan.
 - f. All refinements to the design and materials shall be revised prior to the release of the final site plan .
 - g. Architectural elevations (front, side and rear) shall be submitted with the final site plan. Each elevation shall indicate the average finished grade line.
 - h. The units shall continue to provide varying roof materials such as composite shingles and metal roofs.
 - i. The balconies shall be painted and the railing spacing shall be designed to provide screening for the balconies.
Fayette Street
 - j. Each of the doors as depicted on the preliminary elevations shall be operable.
 - k. The balconies serving the top floor of the hyphens shall not include a roof.
 - l. The three central townhouses shall consist of cementitious or wood siding.
 - m. The roof terrace on the top of the three central units shall be set back an additional

8 feet so that the railing is not visible from the adjoining streets, or the railing incorporated as part of the parapet.

- n. The three central clapboard townhouses shall be recessed a minimum of 2 feet from the adjoining facades.

Cameron Street

- o. The balconies serving the top floor of the hyphens shall not include a roof.
- p. The fourth level balconies for the three central townhomes shall be converted to dormers.
- q. The two story balconies for the three central townhomes shall be converted to enclosed balconies consistent with the architectural style of the units.
- r. The eastern portion of the facade shall be revised per Condition #1.
- s. The hyphen shall be recessed behind the adjoining building faces, and the front edges of the balconies shall either be recessed behind or coplanar with the adjoining building faces.

Henry Street

- t. The southern portion of the facade shall be revised per Condition #1.
 - u. The height of the north end of the northern townhouse shall be reduced to appear as three stories from the north per Condition # 1. (P&Z)
- 3. The building shall incorporate the use of green building technology and sustainable techniques for building systems design and efficiency to the satisfaction of the Director of P&Z. (P&Z)
 - 4. All stairs extend thru the roof so that door access to the roof for firefighting purposes is provided. (Code)

Landscaping

- 5. The applicant shall contribute an amount of \$10,000 for landscaping and park improvements to the nearby Hunter Miller Park. The amount shall be paid prior to the release of the final site plan. (P&Z, RP&CA)
- 6. The final landscape plan shall be provided with the final site plan to the satisfaction of the Director of P&Z and RP&CA. The plan shall include the level of landscaping depicted on the preliminary landscape plan and shall also provide:
 - a. Street trees along Fayette and Cameron Streets shall be planted in 5 foot by 12 foot tree wells. A manual irrigation system shall be provided for the tree wells.
 - b. The developer shall be responsible for the installation and maintenance of trees adjacent to the public streets through bond release. This maintenance shall include, but not be limited to, pruning, watering, pest control, and replacement of street trees as necessary.
 - c. The tree wells will be planted with an evergreen ground cover such as vinca minor or lirioppe.
 - d. The tree wells on Henry Street shall be replaced with a 5 foot wide continuous planting strip for the street trees. The landscape strip shall include groundcover. A manual irrigation system shall be provided for the planting strip.

- e. The caliper of the street trees shall be a minimum of 3½" - 4" at the time of planting.
 - f. The planting depth on top of the planting deck shall be a minimum of 2'-3' for the shrubs, groundcover, and ornamental trees, and a minimum of 4' of soil depth for shade trees, with adequate drainage to support the trees without the use of raised planters. The raised planters on the perimeter of the building shall be eliminated.
 - g. Ornamental trees shall be provided with the open space-front yards for each of the units on Fayette Street
 - h. Additional street trees on Henry Street and Cameron Street shall be provided.
 - i. The parking garage vents shall be located at grade and be of a size and type to minimize the impact on open space and visibility from adjoining streets. Landscape screening shall be provided, if necessary to screen the vents.
 - j. The landscape, hardscape and roof-top open space plans shall be prepared and sealed by a licensed landscape architect.
 - k. All lawn areas shall be sodded.
 - l. All utility structures, including transformers, shall be depicted on the final development plan. All utility structures (except fire hydrants) shall be clustered where possible and located so as not to be visible from a public right-of-way or adjoining property. When such a location is not feasible, such structures shall be located behind the front building line and screened.
 - m. Crown coverage for street trees shall be denoted under a separate tabulation.
 - n. All plant specifications shall be in accordance with the current and most up to date edition of the American Standard For Nursery Stock (ANSI Z60.1) as produced by the American Association for Nurserymen; Washington, D.C.
 - o. All work shall be performed in accordance with Landscape Specifications Guidelines, 4th Edition as produced by the Landscape Contractors Association (LCA) of Maryland, District of Columbia and Virginia; Gaithersburg, Maryland.
 - p. Utility lines such as water, storm sewer and electric lines shall be located to minimize impacts on proposed street trees and open space.
 - q. The location of all light poles shall be coordinated with the street trees.
 - r. As trees mature they are to be limbed up to a minimum 6 feet or greater as necessary to maintain traffic sign visibility. Trees are not to be planted under or near light poles.
 - s. The maximum height for the shrubs is 36 inches.
 - t. No shrubs higher than three feet shall be planted within six feet of walkways.
 - u. All landscaping shall be maintained in good condition and replaced as needed. (P&Z) (RP&CA)
7. The open space and landscaping for the each unit on Fayette Street shall be revised to have the appearance of individual front yards, through the use of landscaping and/or fencing and lead walks for each unit. The two "single-family" homes on each end of Fayette Street shall provide open decorative metal fencing adjacent to the right-of-way to delineate the yards. (P&Z)
8. The central courtyard shall provide the amount of landscaping and amenities as depicted on the preliminary plan and to the satisfaction of the Director of P&Z and shall also at a minimum provide:

- a. A focal element such as a sculpture or water feature within the courtyard to provide a focal element that is an appropriate scale for the space of the courtyard.
 - b. Additional amenities such as special paving surfaces, materials, benches, trash receptacles, landscaping, etc. shall be provided within the courtyard to encourage its use.
 - c. Planters within the courtyard for shade trees as well as other landscaping.
 - d. The plaza shall be redesigned to the building entrance in the corner of the courtyard.
 - e. A manual irrigation system for all landscaping.
 - f. Low scale pathway or bollard lighting.
 - g. Wheel stops or curbing shall be provided for the adjoining ninety-degree parking spaces to prevent cars from projecting onto the adjoining sidewalk. (P&Z)
9. The applicant shall provide pedestrian streetscape improvements that at a minimum shall provide the level of improvements depicted on the preliminary plan and shall also provide the following to the satisfaction of the Director of P&Z:
- a. All sidewalks for the development shall be brick and shall comply with City standards.
 - b. The brick sidewalk shall continue over the proposed curb cut for the parking garage on Fayette Street to provide a continuous uninterrupted brick sidewalk.
 - c. The sidewalk width on Cameron and Fayette Streets shall be reduced from 7 feet to 6 feet to increase the landscape buffer adjacent to the building.
 - d. All sidewalks shall align and connect with the existing sidewalks.
 - e. A public access easement shall be provided for the portion of the sidewalk on Henry Street not located within the public right-of-way.
 - f. All streetscape improvements shall be completed prior to the issuance of a certificate of occupancy permit.
 - g. Provide two (2) city standard street cans, to the satisfaction of the Director of T&ES.
 - h. The stamped asphalt crosswalks at the intersections of Henry and Cameron and Fayette and Cameron shall be eliminated.
 - i. All appropriate on-street parking signage and any other signage for control of pedestrians and vehicles adjoining the site shall be installed by the developer to the satisfaction of the Director of T&ES. (P&Z) (T&ES)
10. The rooftop open space shall be designed to function as high-quality usable open space for the residents. At a minimum, the revised plans for the plaza decks shall include the following to the satisfaction of the Directors of P&Z and RP&CA:
- a. Features and elements such as seating, trash receptacles, and pedestrian-scale lighting;
 - b. Varied and high quality paving materials;
 - c. A significant amount of additional landscaping, including manual irrigation systems;
 - d. The railing for the roof-top deck shall be set back a minimum of 8 feet or incorporated as part of the parapet so that the railing is not visible from the adjoining streets; and

- e. The lighting for the roof-top open space shall be pedestrian scale lighting and shall not be visible from the adjoining streets. Noise generated in conjunction with the roof top open space shall be limited to the type and hours normally associated with a residential use. (P&Z)
11. Coordinate landscape plan with fire service plan. (Code)
 12. In order to provide the required corner vision clearance, all street trees shall be located a minimum of 35 feet from the back-of-curb of the nearest intersecting street. (T&ES)

Parking

13. Residents of the building shall be ineligible to apply for or receive any residential parking permits pursuant to City Code Sec. 5-8, Article F. (P&Z) (T&ES)
14. The four parallel spaces on the northern portion of the site shall be removed and replaced with open space and landscaping. Two additional 90° spaces shall be located adjacent to the internal courtyard, to the satisfaction of the Director of Planning and Zoning. (P&Z)
15. In order to promote teleworking, each of the units will be wired for high-speed internet access. (P&Z)
16. A minimum of 110 parking spaces, as represented on the preliminary plan, shall be located in the underground garage. (P&Z)
17. A minimum of one space for each unit shall be provided within the garage as part of the purchase price for each unit. (P&Z)
18. The applicant shall provide controlled access into the underground garage. The controlled access to the garage shall be designed to allow convenient access to the underground parking for residents and visitors. (P&Z)
19. The applicant shall provide a minimum of seven (7) bicycle parking spaces for use by residents within the garage and two at-grade (2) visitor bicycle parking spaces, to the satisfaction of the Director of T&ES. (T&ES)
20. The applicant shall provide off-street parking for all construction workers without charge. For the construction workers who use Metro, DASH, or another form of mass transit to the site, the applicant shall subsidize a minimum of 50% of the fees for mass transit. Compliance with this condition shall be based on a plan, which shall be submitted to the Department of P&Z and T&ES prior to the issuance of the Excavation/Sheeting, and Shoring Permit. This plan shall set forth the location of the parking to be provided at various stages of construction, how many spaces will be provided, how many construction workers will be assigned to the work site, and mechanisms which will be used to encourage the use of mass transit. The plan shall also provide for the location on the construction site at which information will be posted regarding Metro schedules and routes, bus schedules and routes.

If the plan is found to be violated during the course of construction, a correction notice will be issued to the developer. If the violation is not corrected within ten (10) days, a "stop work order" will be issued, with construction halted until the violation has been corrected. (P&Z) (T&ES)

21. Provide 1 additional handicap parking space for a total of 4 spaces per USBC. (Code)
22. A second, rated egress stair is required in the garage. The garage ramp does not qualify as a means of egress. (Code)

Site Plan

23. All existing and proposed utility poles and overhead electrical/telephone lines adjoining the site shall be located underground and the cost of such undergrounding shall be the sole responsibility of the developer. All transformers shall be located adjacent to the internal street to the satisfaction of the Director of P&Z. (P&Z) (T&ES)
24. The portion of the drive aisle that is located on top of the parking garage shall be designed to AASHTO HS-20 loading requirements of delivery trucks and fire apparatus to the satisfaction of the Directors of P&Z, T&ES and Code Enforcement. (P&Z)
25. The two existing lots shall be consolidated. The plat of consolidation shall be submitted with the submission of the first final site plan. The plat of consolidation shall be approved and recorded prior to the release of the final site plan. (P&Z)
26. A freestanding subdivision or development sign shall be prohibited. (P&Z)
27. Show all utility structures, including transformers, on the final development plan. All utility structures (except fire hydrants) shall be clustered where possible and located so as not to be visible from a public right-of-way or property. When such a location is not feasible, such structures shall be located behind the front building line and screened to the satisfaction of the Director of P&Z. (P&Z)
28. The applicant shall be allowed to make minor adjustments to the building location if the changes do not result in off-street parking or open space below that required by the Zoning Ordinance and do not result in an increase in building height or floor area ratio. (P&Z)
29. Temporary structures for construction shall be permitted and the period of such structures shall be subject to the approval of the Director of P&Z. The trailer shall be removed prior to the issuance of the certificate of the last certificate of occupancy permit. (P&Z)
30. All condominium association covenants shall be approved by the Director of P&Z and the City Attorney prior to applying for the first certificate of occupancy permit for the project. The association covenants shall include the conditions listed below, which shall be clearly expressed in a separate section of the covenants.

- a. The principal use of the underground garage and parking spaces shall be for passenger vehicle parking only; storage which interferes with the use of a parking space for a motor vehicle is not permitted.
 - b. The designated visitor parking spaces shall be reserved for the use of the condominium guests.
 - c. No more than two parking spaces shall be assigned to a specific condominium unit; all unassigned spaces in the garage shall be made generally available to residents and/or visitors.
 - d. That a public access easement for the pedestrian connection is provided for the sidewalk on Henry Street.
 - e. All landscaping and open space areas within the development, shall be maintained by the homeowners and condominium owners.
 - f. Exterior building improvements or changes by future residents shall require the approval of the Parker Gray BAR and/or City Council, as determined by the Director of P&Z. (P&Z)
31. Any inconsistencies between the various drawing submitted by the applicant shall be reconciled to the satisfaction of the Director of P&Z and T&ES. (P&Z)
 32. The applicant shall identify a person who will serve as liaison to the community throughout the duration of construction. The name and telephone number of this individual shall be provided in writing to residents, property managers and business owners whose property abuts the site, and to the Directors of P&Z and T&ES. (P&Z)
 33. A temporary informational sign shall be installed on the site prior to approval of the first final site plan for the project and shall be displayed until construction is complete or replaced with a marketing sign incorporating the required information: the sign shall notify the public of the nature of the upcoming project and shall provide a phone number for public questions regarding the project. (P&Z)
 34. Temporary construction and sales trailer(s) shall be permitted and be subject to the approval of the Director of P&Z. The trailer(s) shall be removed prior to the issuance of a certificate of occupancy permit for the building. (P&Z)
 35. Provide a site lighting plan to the satisfaction of the Director of T&ES in consultation with the Chief of Police. The plan shall show the existing and proposed street lights and site lights. Indicate the type of fixture, and show mounting height, and strength of fixture in Lumens or Watts. Provide manufacturer's specifications for the fixtures. Provide lighting calculations to verify that lighting meets city standards and are located to prevent excessive spillover lighting and glare from adjacent properties. (T&ES) (P&Z)
 36. Before commencing any clearing or grading of the site, the applicant shall hold a meeting with all adjoining property owners to review the hauling routes, location of construction worker parking, plan for temporary pedestrian and vehicular circulation, and hours and overall schedule for construction. The Departments of P&Z and T&ES shall be notified of the date of the meeting before the permit is issued. Copies of plans showing the hauling

- route, construction worker parking and temporary pedestrian and vehicular circulation shall be posted in the construction trailer and given to each subcontractor before they commence work on the project. (P&Z)
37. The applicant shall submit a final location survey for the buildings and interior of the parking garage prior to issuance of a certificate of occupancy permit. (P&Z)
 38. Provide all pedestrian and traffic signage to the satisfaction of the Director of T&ES. (T&ES)
 39. All Traffic Control Device design plans, Work Zone Traffic Control plans, and Traffic Studies shall be sealed by a professional engineer, registered in the Commonwealth of Virginia. (T&ES)
 40. Prior to the release of the final site plan, provide a Traffic Control Plan for construction detailing proposed controls to traffic movement, lane closures, construction entrances, haul routes, and storage and staging. (T&ES)
 41. The proposed Cameron Street crosswalk at Henry Street shall be moved closer to the intersection. (T&ES)
 42. Provide a brick paver pedestrian crossing across the entrance at Fayette Street, designed to the satisfaction of the Director of T&ES. (T&ES)
 43. All driveway entrances and sidewalks in public ROW or abutting public ROW shall meet City standards. (T&ES)
 44. Show all existing and proposed easements, both public and private. (T&ES)
 45. Replace existing curb and gutter, sidewalks, and handicap ramps adjacent to the site that are in disrepair or broken. (T&ES)
 46. Where needed, replace inlets along the frontage of the site and provide new inlets, to the satisfaction of the Director of T&ES. (T&ES)
 47. Applicant to provide the capital cost to install countdown pedestrian signals and Prisma accessible pedestrian signals at the intersection of Henry and Cameron Streets. (T&ES)
 48. Applicant to provide conduit for future traffic signal at intersection of Fayette Street and Cameron Street, as shown on preliminary plan. (T&ES)
 49. Applicant shall provide two four-inch traffic signal conduits running along Henry Street and Cameron Street under the sidewalk, behind the tree planters. These conduits shall terminate in a junction box at the corner of Henry and Cameron Streets. (T&ES)
 50. Slopes of garage ramps shall not exceed 10%, to the satisfaction of the Director of T&ES. (T&ES)

51. All archaeological work shall be carried out in accordance with the *City of Alexandria Archaeological Standards* and is subject to the approval of the City Archaeologist. (Archaeology)

Stormwater

52. Developer to comply with the peak flow requirements of Article XIII of AZO, as determined by the Director of T&ES. (T&ES)
53. Note 7 on Sheet 3A states that the “applicant requests a waiver of water quality due to the elimination of discharge into the combined system.” The City will not waive the requirements of Article XIII of the AZO because a) the infrastructure required to make the connection to the separate sanitary sewer is not unreasonable, and b) a written request was not submitted in accordance with Memorandum to Industry No. 2002-0001, dated January 4, 2002. Remove Note #7 from Sheet 3A and references to this note throughout the plans. (T&ES)
54. Plan must demonstrate to the satisfaction of the Director of T&ES that adequate stormwater outfall is available to the site or else developer is to design and build any on- or off-site improvements to discharge to an adequate outfall. (T&ES)
55. The applicant is advised that all stormwater designs that require analysis of pressure hydraulic systems and/or inclusion and design of flow control structures must be sealed by a professional engineer, registered in the Commonwealth of Virginia. If applicable, the Director of T&ES may require resubmission of all plans that do not meet this standard. (T&ES)
56. Provide proposed elevations (contours and spot shots) in sufficient details on grading plan to clearly show the drainage patterns. (T&ES)
57. The project lies within the Combined Sewer District; thus storm water management and compliance with the City's Chesapeake Bay program must be coordinated with City's policy for management of the Combined Sewer District. Applicant shall connect the sanitary sewer for the project to the Potomac Yard Trunk Sewer located at the intersection of Queen and Fayette Streets. (T&ES)
58. Provide complete pre- and post-development drainage maps including topographic information, storm drains, BMP's and either Worksheet A or B. (T&ES)
59. Provide complete drainage divide map for the project area including areas that contribute surface runoff from beyond project boundaries. Include topographic information, storm drains and storm water Best Management Practices (BMPs). (T&ES)
60. The stormwater Best Management Practices (BMPs) required for this project shall be constructed and installed under the direct supervision of the design professional or his designated representative. Prior to release of the performance bond, the design professional shall submit a written certification to the Director of T&ES that the BMPs are:

- a. Constructed and installed as designed and in accordance with the approved Final Site Plan.
 - b. Clean and free of debris, soil, and litter by either having been installed or brought into service after the site was stabilized. (T&ES)
61. The applicant shall submit a storm water quality BMP Maintenance Agreement with the City to be reviewed as part of the Final #2 Plan. It must be executed and recorded with the Land Records Division of Alexandria Circuit Court prior to approval of the final site plan. (T&ES)
62. The applicant shall be responsible for maintaining storm water Best Management Practices (BMPs) until activation of the homeowner association (HOA), if applicable, or until sale to an owner. Prior to transferring responsibility for the BMPs to the HOA or owner, the applicant shall execute a maintenance service contract with a private contractor for a minimum of three years and transfer the contract to the HOA or owner. A copy of the contract shall also be placed in the BMP Operation and Maintenance Manual. Prior to release of the performance bond, a copy of the contract shall be submitted to the City (T&ES)
63. If the units will be sold as individual units and a homeowner's association established the following two conditions shall apply:
- a. The applicant shall furnish the Homeowner Association with an Owner's Operation and Maintenance Manual for all the Best Management Practices (BMPs) used on site. The manual shall include at a minimum: an explanation of the functions and operations of the BMP(s); drawings and diagrams of the BMP(s) and any supporting utilities; catalog cuts on maintenance requirements including mechanical or electrical equipment; manufacturer contact names and phone numbers; a copy of the executed maintenance service contract; and a copy of the maintenance agreement with the City.
 - b. The Developer shall furnish each home purchaser with a brochure describing the storm water BMP(s) installed on the site, outlining the responsibilities of the homeowners and the Homeowner's Association (HOA) with respect to maintenance requirements. Upon activation of the HOA, the Developer shall furnish five copies of the brochure per unit to the HOA for distribution to subsequent homeowners. (T&ES)
64. Prior to release of the performance bond, a copy of the Operation and Maintenance Manual shall be submitted to the City on a digital media. (T&ES)
65. Prior to release of the performance bond, the applicant is required to submit a certification by a qualified professional to the satisfaction of the Director of T&ES that the existing storm water management facility adjacent to the project and associated conveyance systems were not adversely affected by the construction and that they are functioning as designed and are in a condition similar to prior to construction began. If maintenance of the facility or systems were required in order to make this certification, provide a description of the maintenance performed. (T&ES)

Housing

66. The developer shall set aside one-third of any bonus density units, rounded to the nearest whole unit, at prices not to exceed \$225,000 each for two-bedroom units, for sale to households with incomes not exceeding the City's income guidelines for the Moderate Income Housing Program (MIHP), which are currently \$68,700 for a one or two person household and \$79,500 for a three or more person household. All bonus density affordable units provided shall be two-bedroom units. The developer shall make a \$54,500 cash contribution to the City's Housing Trust Fund for the base building, or, if the project is approved with 10 bonus density units as proposed, the developer shall include one-bedroom unit for sale at \$173,200 to households with incomes as described above, in lieu of the cash contribution for the base building. Prices include one garage parking space for each unit. (Housing)
67. The set-aside units shall be of the same size, type and with the same amenities as other similar units in the development. The units shall be scattered throughout the building. (Housing)
68. Whatever incentives are offered to any potential home buyers shall also be offered to purchasers of the set-aside units. (Housing)
69. Thirty year afford ability of the set-aside units shall be provided through deed restrictions recorded as covenants at the time of sale of each of the set-aside units, in accordance with the City's set-aside resale policy. Language for the covenants shall be provided by the City in advance of the final sale of any unit. (Housing)
70. The City shall have exclusive right to market the set-aside units for 90 days, after which the developer will also have the right to market the units, at the affordable price, to buyers qualified for the set-aside program. The developer is encouraged to refer potentially qualified buyers to the Office of Housing's Implementation Division for participation in the set-aside and MIHP programs. In the event a qualified buyer cannot be found for a set-aside unit by the time the unit is completed and ready for occupancy, the developer shall have the right to sell the unit at full market price, and shall contribute to the Housing Trust Fund the equivalent of \$5.87 per gross square foot of each unit so sold. (Housing)
71. If the developer sells the market rate units for less than expected, the applicant shall index the price on the affordable units proportionately. (Housing)
72. Real estate commissions shall be paid (or not paid) for set-aside units in the same manner and on the same basis as for market rate units. (Housing)

General

73. In the event that Section 5-1-2(12b) of the City Code is amended to designate multi-family dwellings in general, or multi-family dwellings when so provided by SUP, as required user property, then refuse collection shall be provided by the City. (T&ES)

74. The developer agrees to deliver all solid waste, as defined by the Code of the City of Alexandria, to a refuse disposal facility designated by the Director of T&ES. The developer further agrees to stipulate in HOA documents that all tenants and/or property owners shall also comply with this requirement. (T&ES)
75. The sidewalk along Route 1 shall remain open during construction. (T&ES)
76. Plan does not indicate whether or not there are any known soil and groundwater contamination as required with all preliminary submissions. Should any unanticipated contamination or underground storage tanks, drums and containers are encountered at the site the applicant must immediately notify the City of Alexandria Department of Transportation and Environmental Services, Division of Environmental Quality. (T&ES)
77. If fireplaces are to be included in the development, the applicant is required to install gas fireplaces to reduce air pollution and odors. Animal screens must be installed on chimneys. (T&ES)
78. A “Certified Land Disturber” must be named on the Erosion & Sedimentation Control sheets prior to commencement of activity in accordance with the Virginia Department of Conservation and Recreation guidelines. (T&ES)
79. During the construction phase of this development, the site developer, its contractor, certified land disturber, or owner’s other agents shall implement a waste and refuse control program. This program shall control wastes such as discarded building materials, concrete truck washout, chemicals, litter or trash, trash generated by construction workers or mobile food vendor businesses serving them and sanitary waste at the construction site and prevent its off site migration that may cause adverse impacts to the neighboring properties or the environment to the satisfaction of Directors of Transportation and Environmental Services and Code Enforcement. All wastes shall be disposed off site properly in accordance with all applicable federal, state and local laws. (T&ES)
80. The applicant shall present a disclosure statement to potential buyers disclosing the following to the satisfaction of the Director of P&Z and the City Attorney:
That a major highway, with its associated issues such as traffic and noise, is located adjacent to the project. (T&ES)
81. Due to the close proximity of the site to the Jefferson Davis Highway the following conditions shall be included in the development requirements:
 - a. Applicant shall prepare a noise study identifying the levels of interior noise residents of the project will be exposed to at the present time and 10 years into the future in a manner consistent with the Noise Guidance Book used by the Department of Housing and Urban Development (HUD).
 - b. Identify options to minimize noise exposure to future residents at the site, particularly in those units closest to railroad, including: triple-glazing for windows, additional wall/roofing insulation, installation of resilient channels between interior gypsum board and wall studs and any other special construction methods to reduce

- sound transmission. If needed, the applicant shall install some combination of the above to the satisfaction of the Directors of P&Z and T&ES.
- c. The noise study shall be submitted and approved prior to final site plan approval. (T&ES)

STAFF: Eileen Fogarty, Director, Department of Planning and Zoning;
Jeffrey Farner, Chief, Development;
David Sundland, Urban Planner;
Laura Durham, Urban Planner.

Staff Note: In accordance with Section 11-506(c) of the zoning ordinance, construction or operation shall be commenced and diligently and substantially pursued within 18 months of the date of granting of a special use permit by City Council or the special use permit shall become void.

ADDITIONAL CITY DEPARTMENT CODE COMMENTS

Legend: C - Code Requirement R - Recommendation S - Suggestion F - Finding

Code Enforcement

- C-1 The developer shall provide a building code analysis with the following building code data on the plan: a) use group; b) number of stories; c) type of construction; d) floor area per floor; e) fire protection plan.
- C-2 The developer shall provide a separate Fire Service Plan which illustrates: a) emergency ingress/egress routes to the site; b) two fire department connections (FDC) to the building, one on each side/end of the building; c) fire hydrants located within one hundred (100) feet of each FDC; d) on site fire hydrants spaced with a maximum distance of three hundred (300) feet between hydrants and the most remote point of vehicular access on site; e) emergency vehicle easements (EVE) around the building with a twenty-two (22) foot minimum width; f) all Fire Service Plan elements are subject to the approval of the Director of Code Enforcement. Fire Service Plan shall be incorporated as an additional sheet in plan set.
- C-3 Fire Department ladder truck access is required for the two long sides of all buildings over 50 feet in height. This requires a truck to be able to position itself between 15 and 30 feet from the face of the building. All elevated structures used for this purpose shall be designed to AASHTO HS-20 loadings.
- C-4 Provide two Siamese connections to the building located to the satisfaction of the Director of Code Enforcement. Provide hydrants located within 100 feet of each FDC and on the same side of the street as each FDC.
- C-5 A separate tap is required for each building fire service connection.
- C-6 The public parking garage (Use Group S-2) is required to be equipped with a sprinkler system and shall be mechanically ventilated.
- C-7 The public parking garage floor must comply with USBC and drain through oil separators or traps to avoid accumulation of explosive vapors in building drains or sewers as provided for in the plumbing code. This parking garage is classified as an S-2, Group 2, public garage. Floors of public garages must be graded to drain through oil separators or traps to avoid accumulation of explosive vapors in building drains or sewers.
- C-8 Prior to the issuance of a demolition permit or land disturbance permit, a rodent abatement plan shall be submitted to Code Enforcement that will outline the steps that will be taken to prevent the spread of rodents from the construction site to the surrounding community and sewers.

- C-9 Prior to submission of the Final Site Plan, the developer shall provide a fire flow analysis by a certified licensed fire protection engineer to assure adequate water supply for the structure being considered.
- C-10 A soils report must be submitted with the building permit application.
- C-11 A Certificate of occupancy shall be obtained prior to any occupancy of the building or portion thereof, in accordance with USBC 118.0.
- C-12 Required exits, parking, and accessibility within the building for persons with disabilities must comply with USBC Chapter 11. The applicant shall define if handicap persons will have access to the underground parking structure. In addition, the plans do not show how surface parking pedestrians will access the structure at the rear of the building. There is too much overlying detail to clearly assess whether there is sufficient handicap access to the front lobby from Cameron Street.
- C-13 Sheeting and shoring shall not extend beyond the property line; except when the developer has obtained a written release from adjacent property owners which has been recorded in the land records; or through an approved encroachment process.
- R-1 For firefighting reasons it is recommended that all stairs extend thru the roof so that door access to the roof is provided.

Transportation and Environmental Services (T&ES)

- C- 1 An appropriate performance bond for the public improvements must be posted prior to release of the plan.
- C- 2 All downspouts must be connected to a storm sewer by continuous underground pipe.
- C- 3 The sewer tap fee must be paid prior to release of the plan.
- C- 4 All easements and/or dedications must be recorded prior to release of the plan.
- C- 5 Plans and profiles of utilities and roads in public easements and/or public right-of-way must be approved prior to release of the plan.
- C- 6 All drainage facilities must be designed to the satisfaction of T&ES. Drainage divide maps and computations must be provided for approval.
- C- 7 All utilities serving this site to be underground.
- C- 8 Provide site lighting plan to meet minimum city standards.
- C- 9 The applicant shall comply with the City of Alexandria's Noise Control Code, Title 11, Chapter 5, which sets the maximum permissible noise level as measured at the property line.

- C-10 The applicant must comply with the Article XIII of the City's zoning ordinance, which includes requirements for stormwater pollutant load reductions, treatment of the water quality volume default, and stormwater quantity management.
- C-11 The applicant must comply with the City of Alexandria, Erosion and Sediment Control Code, Section 5, Chapter 4. This includes naming a Responsible Land Disturber on the Erosion and Sediment Control sheets prior to engaging in land disturbing activities in accordance with Virginia Erosion and Sediment Control Law.
- C-12 All required permits from Virginia Department of Environmental Quality, Environmental Protection Agency, Army Corps of Engineers, Virginia Marine Resources must be in place for all project construction and mitigation work prior to release of the final site plan. This includes the new state requirement for a VPDES permit for all construction activities greater than 1 acre.

Virginia American Water Company

- F-1 Hydraulic calculations will be completed to verify that the required fire flow can be achieved at the proposed fire hydrants upon final submittal of the site plan. Profiles will be required for hydraulic calculations.
- F-2 The existing water main in North Fayette Street, between Cameron and Queen, is six-inch, not eight-inch as currently labeled.
- C-1 A double detector check backflow prevention device is required on all fire services. If located inside the premises, it must have a remote reading meter in a separate accessible room.

Archaeology

- F-1 Tax records indicate that a house may have been present on this property by 1810. There were free African American households on this street face in 1830, but the exact addresses are unknown. By the time of the Civil War, the lot had become the site of the Alexandria and Washington Railroad station. A warehouse and service station currently occupy the property. While the more recent construction has probably destroyed most of the surface evidence of 19th-Century activities on this lot, it is possible that some more deeply buried archaeological resources may remain intact.
- C-1 Call Alexandria Archaeology immediately (703-838-4399) if any buried structural remains (wall foundations, wells, privies, cisterns, etc.) or concentrations of artifacts are discovered during development. Work must cease in the area of the discovery until a City archaeologist comes to the site and records the finds.
- C-2 The above statement (in C-1) must appear in the General Notes of the site plan so that on-site contractors are aware of the requirement.

**REPORT ATTACHMENTS
AVAILABLE IN THE PLANNING AND ZONING OFFICE**