

Docket Item #12 A & B
DEVELOPMENT SPECIAL USE PERMIT #2004-0041
TMP SPECIAL USE PERMIT #2005-0022
EISENHOWER CENTER III

Planning Commission Meeting
September 8, 2005

ISSUE: Consideration of a request for a development special use permit, with a site plan and transportation management plan to construct a 6-story, 98,618 square foot office and retail building and a 5-level parking structure.

APPLICANT: Eisenhower Mill Properties, L.L.C., by
Donald Simpson, Sr.

LOCATION: 2320 Mill Road & 551 Elizabeth Lane

ZONE: CDD#2 /Coordinated Development District - Eisenhower East

SITE GRAPHIC
(AVAILABLE IN THE PLANNING AND ZONING OFFICE)

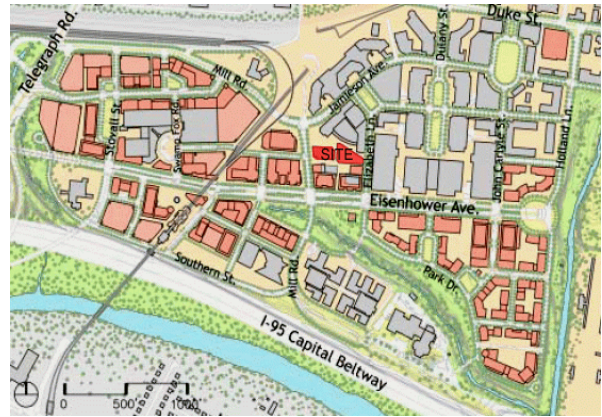
I. IMPACTS / BENEFITS:

IMPACT/BENEFIT	COMMENTS
Consistency with Strategic Plan	<ul style="list-style-type: none"> • The proposal represents redevelopment of a scale that is consistent with that which is outlined in the Eisenhower East Plan. • The development provides a mix of retail and office uses. • The building orientation and wide sidewalks provide a pedestrian-oriented streetscape.
Use	<ul style="list-style-type: none"> • Mixed-use building 89,484 sq. ft. of office and 9,134 sq. ft. of ground floor retail
Open Space	<ul style="list-style-type: none"> • No on-site open space required • Participation in the Eisenhower East Open Space Plan
Pedestrian / Streetscape	<ul style="list-style-type: none"> • Provision of 14 ft. public sidewalk and street trees. • Building fronts on the street and public sidewalk • Streetscape conforms to Eisenhower East Plan
Building Compatibility	<ul style="list-style-type: none"> • 85 ft. tall building, with 92 ft. tall tower element. • Building will be similar in scale to existing Eisenhower Center buildings, but smaller than other adjoining buildings. • Building will be constructed of high quality materials.
Affordable Housing	<ul style="list-style-type: none"> • The applicant will make a voluntary offer of \$1.50 per sq.ft for a total voluntary contribution of \$147,927.
Traffic/Transit	<ul style="list-style-type: none"> • Eisenhower East Plan sets parking maximums to encourage transit use. • TMP will encourage use of transit and other alternative means. • Participation in the Eisenhower East TMP.
Parking	<ul style="list-style-type: none"> • 1 space proposed per 500 sq. ft. of building. • Eisenhower East Plan permits one 5-level, above-grade parking structure will be architecturally treated.
Environment	<ul style="list-style-type: none"> • A recommended condition requires the incorporation of green and sustainable building technology.

II. EXECUTIVE SUMMARY:

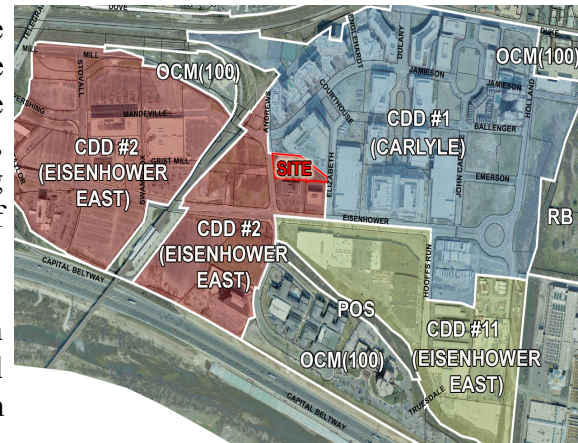
A. Overview:

The applicant is requesting development special use permit (DSUP) and transportation management plan (TMP) approval to construct an office building with ground floor retail and a freestanding parking structure within Eisenhower East. A complete application for the proposal was submitted in May 2005 and the proposed design was approved by the design review board in June 2005.



Eisenhower East Plan

The Eisenhower East CDD zoning for this site (CDD#2), permits a building area of approximately 400,000 sq.ft. or approximately 170,000 sq.ft. more than permitted by the previous zoning prior to the adoption of the Plan. In addition, while above grade parking is strongly discouraged in Eisenhower East, this site is one of the areas where the parking structure (117,000 sq.ft.) is not counted as part of the allowable floor area.



Eisenhower East Zoning

While the Plan and zoning permit an increase in building area with a special use permit, the approval of the increased building area is based on compliance with the following:

- Eisenhower East Plan;
- Guidelines to Achieve Quality Development;
- Approval by a Design Review Board;
- Participation in a Eisenhower East Open Space Fund;
- Participation in a Eisenhower East Transportation Management Plan (TMP); and
- Participation in infrastructure improvements.



Site Plan

The Plan allows for an increase in density and height as part of a special use permit in close proximity to the adjoining Eisenhower metro station to provide:

- an urban mix of uses;
- balance of jobs and housing;
- optimizing the use of transit;
- grid of interconnected streets;
- reduced parking;
- active street frontages;
- large consolidated areas of public open space; and
- Eisenhower Avenue as an urban boulevard.



Quality Pedestrian Environment

The Plan and accompanying CDD zoning established parameters to provide developers-applicants with certainty through the identification of use, heights and overall building volumes. Both Carlyle and Eisenhower East have attracted high quality development within these planned areas. The Plans provide the framework for ensuring that the area will be a high quality mixed-use area.

In addition, the Plan provides certainty for the City and community by providing parameters to ensure high quality development, open space, infrastructure and an urban mix of uses expected as part of the height and density approved as part of the Plan

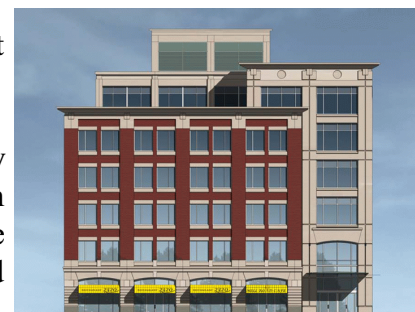
The proposal is one of the many upcoming proposals for East Eisenhower, which are in the conceptual, preliminary stages or are under construction. There are six sites in Eisenhower East which have been submitted for conceptual review under the Plan which consist of residential, office, retail, and hotel uses and total over 2.6 million sq ft. Collectively the sites that are under review or construction total over 3.5 million sq. ft., or 44% of all potential redevelopment within East Eisenhower.

Eisenhower East Developments



 IN CONCEPT REVIEW	 IN FINAL REVIEW
 CDD CONCEPT	 UNDER CONSTRUCTION

A fundamental principle of the Plan is that elements typically provided on an individual project by project basis, such as open space and TMPs, would be provided in a more comprehensive way that the City, community and developers agreed would provide greater benefit for everyone.



West Building Elevation (Mill Road)

B. Open Space:

One of the key principles of the Eisenhower East Plan is the creation of meaningful public parks, plazas, and other open space within the study area, with linkages between the spaces provided by trails, streetscapes, and linear parks extending through the area's Resource Protection Areas. The plan's approach represents a significant departure from past practice in the City where open space is created in small often unconsolidated increments on each individual site.

The future parks, open space areas, and squares are distributed across several property owners. Depending on the location of the property, owners will be required to dedicate land and/or contribute a monetary amount to the Eisenhower East Open Space Fund. The amount of contribution was calculated and available to the applicant. Based on an analysis by Parks and Recreation and Planning the amount of contribution ranges between \$1.75/sq.ft. to \$2.00/sq.ft. of development within Eisenhower East. Staff is recommending \$1.75/sq.ft. of development within Eisenhower East as the open space contribution. A more detailed discussion of the open space contribution is tentatively scheduled for the October Planning Commission hearing. To allow the applicant to proceed, staff has included recommendations that will require the applicant to be subject to the open space contribution as approved by the Planning Commission and City Council.

C. Transportation Management Plan:

Overall, the approach is to provide disincentives for single-occupancy vehicles, while also providing incentives for using mass transit. In addition, a strategy for Eisenhower East is the creation of a district-wide Transportation Management Program. The program will incorporate existing TMPs, such as the TMP that is the subject of this application, and will coordinate a shuttle transit program, and will monitor the transit incentives. In the short term a TMP will exist for this project, however, in the future with additional development, a transportation management program will be structured and implemented in a comprehensive district-wide manner for the Eisenhower East area, rather than on a site-by-site basis, and existing site-specific TMPs will be incorporated into the program. To allow the applicant to proceed, staff has included recommendations that will require the applicant to be subject to the TMP policy approved by the Planning Commission and City Council.

D. Conclusion:

Staff recommends **approval** with the conditions of approval as outlined in the staff report. The recommendation of approval is based on compliance with the Plan, Design Guidelines and conditions that will ensure equitable participation for open space and TMP's. In addition, a recommendation has been added requiring participation in a possible East Eisenhower special taxing district as identified by the Plan. In general, the proposal is consistent with the intent of the Plan, as it provides a mix of uses, provides an appropriate density near the metro and orients the building to the street and pedestrians.

III. BACKGROUND

A. Site Description:

The site is located to the north of the intersection of Mill Road and Eisenhower Avenue. The 59,984 sq. ft. site has frontage on Mill Road and Elizabeth Lane. The existing site contains a one-story, 16,000 sq.ft. office building and surface parking lot which are proposed to be demolished. A 6-story, 57,000 sq. ft. office building, a 5-story, 36,000 sq. ft. office and retail building, and accompanying surface parking lots are located to the south of the site. That area is part of the same complex and is identified in the Plan as Phase 2 of the Simpson/Eisenhower Center development.



**Carlyle Towers & Courthouse
(North of Site)**

To the north of the site is the United States District Courthouse, a 225 ft. tall, 400,000 sq. ft. building, and farther north are the Carlyle Towers condominiums, with building heights of 180 ft. To the northwest is a site which is designated for a 150 ft. tall, 100,000 sq. ft. hotel, for which concept plans have recently been submitted, and to the west is the approved Mill Race development, with heights of 247 ft., 184 ft., and 179 ft. and a total of 835,000 sq. ft. of office, residential, and retail. To the east is the PTO west parking garage, which is fronted by 65 ft. tall “office townhouses.”



**PTO Office Townhouses
(East of Site)**

B. Proposal:

The office building will have frontage on the eastern portion of Mill Road, while the parking structure will have frontage on Elizabeth Lane. The first floor of the building is proposed to provide 9,134 sq. ft of ground floor retail, while the remaining 5 floors will provide office space. The main entrance for the office portion of the building will be on the southwestern portion of the building on Mill Road. Entrances for the retail space on Mill Road will be established depending on the future tenants.



South Elevation

After the approval of the Plan in March 2004, the applicant prepared plans and filed a complete preliminary site plan application for the site in May 2005. The proposed office building is located on a irregular shaped parcel that is located adjacent to the visually prominent intersection of Mill Road and Eisenhower Avenue. The general dimension of the building footprint is



Site Plan

approximately 100 ft. x 160 ft. The building faces are articulated into a series of vertical bays, with the sixth level setback 10 ft. to provide visual interest. The proposal will provide a 15 ft. wide sidewalk on Mill Road and will retain the sidewalk on Elizabeth Lane.

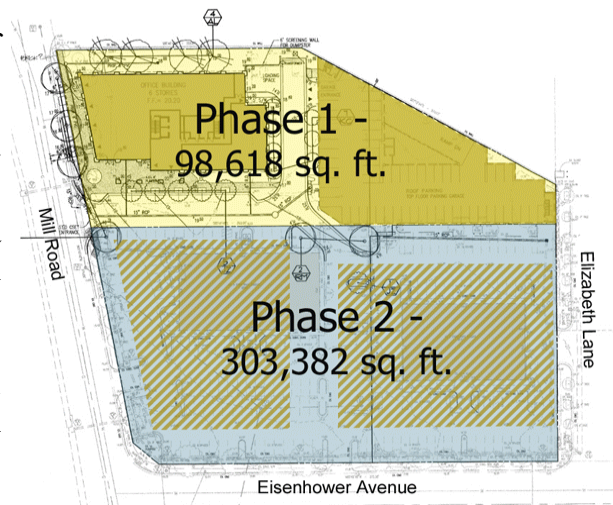
The applicant is also proposing to construct a 323-car, 5-level parking structure on the portion of the property fronting Elizabeth Lane. The new parking structure will serve the new office building and also the tenants in the existing and future office buildings to the south. The top floor of the parking structure will be reserved for use by jurors for the Federal Courthouse and the ramp to the top level will be designed with special security measures accordingly. The parking structure and loading area for the office building will be accessed via existing drive aisles and curb cuts on Mill Road and Eisenhower Avenue.

While the Plan strongly encourages parking below grade, due to the limited visibility of the northeast portion of the site, the grade difference (approximately 10 ft.) between Elizabeth Lane and the site, and the proximity of the above-grade parking structure of the Courthouse, the Plan allows an above-grade parking structure. The Plan states, “the structure must be architecturally designed with special attention to the Elizabeth Lane facade and constructed of quality materials. The structure should be no more than 5 levels above grade or exceed the height of 45 ft. to the upper parapet as measured at sidewalk level in the northeast corner of the property adjoining Elizabeth Lane.”

IV. ZONING:

The applicant is requesting development special use permit and transportation management plan approval to allow the construction of an office/retail building and parking structure in the CDD#2 Coordinated Development District. The primary purpose of the CDD zones is to promote development consistent with the Plan in order to encourage a mix of uses, land assemblage, and/or cooperation and joint planning where there are multiple land owners. The Plan seeks to create a vibrant, urban, mixed-use community that is transit-focused and oriented to the pedestrian. The CDD zoning and Design Guidelines help to achieve these goals, and offers an incentive of increased building area and height with special use permit approval based on compliance with the Plan's standards and guidelines.

The Plan lists the maximum building area that is allowed on each block. Since a building of approximately 98,000 sq. ft. was anticipated on the Eisenhower Center Phase I site, the Plan indicates a specific floor area for each phase - 98,000 sq. ft. for Phase I and 304,000 sq. ft. for Phase II. However, since the two phases share the same block, floor area can be exchanged between the two phases, provided that each resultant building on the block meets the standards and guidelines of the Plan. Therefore the proposed 98,618 sq. ft. building is allowable with an SUP, and the allowable floor area for Phase II will be reduced to 303,382 sq. ft.



Eisenhower Center Phasing

EISENHOWER CENTER III		
Property Address:	2320 Mill Road	
Site Area:	1.377 acres (59,984 sq.ft.)	
Zone:	CDD#2 Coordinated Development District (Eisenhower East)	
Current Use:	Office	
Proposed Use:	Office/Retail/Parking Structure	
	<u>Permitted/Required</u>	<u>Proposed</u>
Building Area w/o SUP	74,980 sq. ft.	
Building Area With SUP	98,000 sq. ft.	98,618 sq. ft. *
Open Space	N/A	N/A.
Height	200 feet	92 feet (tower element) 85 feet (remainder of building)
Parking	1.66 long-term sp. / 1,000 s.f. office 0.34 short-term sp. / 1,000 s.f. office 2.0 spaces / 1,000 s.f. of retail Total Maximum = 197	197 spaces 127 ** spaces Total 324 spaces
<p>* Applicant proposing to use 618 sq.ft. of building area from Phase II. Both phases are within the same block (Block 23).</p> <p>** 127 parking spaces are provided in the parking structure and will also provide parking for the Phase II office buildings on the lot as envisioned by the Plan. The parking permitted by the Plan are <i>maximums</i> rather than the more typical minimum parking requirements.</p> <p>*** Also see Attached Design Guidelines Analysis</p>		

V. STAFF ANALYSIS:

The Plan seeks to create a vibrant, urban, mixed-use community that is transit-focused and oriented to the pedestrian. The proposed development will replace the existing one-story building and surface parking lot. The proposal represents an opportunity to develop a more urban development that will be consistent with the Plan. The approval is based on the applicant complying with and participating in the Eisenhower East open space and TMP programs. The proposal has been reviewed and approved by the Design Review Board based on compliance with the Eisenhower East Design Guidelines. With the proposed conditions, staff is recommending approval.

A. Compliance with Eisenhower East Plan:

The Plan was approved as a “form based code” rather than the more traditional zoning, which uses floor area, setbacks, etc. In exchange for the flexibility as part of the form based code, applications are required to comply with the form specified in the Plan and Design Guidelines, similar to the process in Carlyle.

The overall, volume, size and height that are proposed for this building are consistent with the Plan. In addition to overall building volume and height, the Plan established Design Guidelines to ensure that buildings will consist of high quality design and materials. Similar to Carlyle and Potomac Yard, the Plan also establishes a design review board to review compliance with the Design Guidelines. The principles of the Plan include:

- *Create an urban, not suburban, development - an extension of Old Town/Carlyle;*
- *Maximize the use of Metro and other transit;*
- *Ensure a network of urban streetscapes;*
- *Provide a jobs/housing balance, development tied to performance criteria, and an appropriate level of affordable housing;*
- *Provide a coordinated recreation system of public streets, resource protection areas and open space squares; and*
- *Ensure parking programs and parking standards consistent with urban - not suburban - models*

Principle: *To create an urban, not suburban, development: an extension of Old Town/Carlyle*

The proposal provides a mix of ground floor retail and office uses that will help to complement the existing and future mix of residential, retail, and office uses in the immediate vicinity of the Eisenhower Metro station on a primary pedestrian pathway to the metro station.

Principle: *Maximize the use of Metro and other transit*

The site is within 1,500 feet of the Metro station which provides connection to the regional rail system. Additionally, the site has immediate access to bus service. The following chart identifies the bus service provided by these lines:

Bus Routes

Bus Line		Frequency
Alexandria Transit Company (DASH)	AT7 - Lee Center to Landmark Center via Eisenhower Ave	Approx. every half hour from 6:00 am to 8:00 pm
	AT2 - Eisenhower Metro - Van Dorn Metro - Braddock Metro	Approx. every half hour during AM and PM rush hours
	AT6 - Eisenhower Metro to NVCC via King Street	Approx every half hour between 6:00 and 10:00 am and 3:00 and 8:00 pm
Fairfax Connector	Route 110 to Springfield Mall and King Street Metro.	Weekdays: 30mins during peak hours, 1 hr off peak

Staff is recommending that the applicant implement an aggressive package of transit subsidies for the building, charge market rate prices for the parking, and coordinate with other TMPs in Eisenhower East. These measures will strongly encourage the use of transit and strongly discourage single-occupancy vehicles. Given the size of the office building, it is anticipated that a range of approximately 400 to 650 employees will occupy the building. The number of employees varies considerably based on tenants. In addition to transit subsidies, the use of market rate pricing will eliminate employers’ subsidies of parking, allowing the parking cost to provide a further incentive for using transit. In addition, the applicant will be required to provide ride-sharing, bicycle storage, showers and locker rooms for bicycle commuters and parking for flex/zip cars. The applicant is also providing a bus shelter on Mill Road to enhance the environment for those who use the bus. Specifically, conditions have been included that will require the following:

- employer-provided transit benefits and subsidies;
- “flex car”;
- market rate pricing of parking;
- restricted parking requirements;
- promotion of staggered work hours;
- guaranteed ride-home programs;
- ride-share matching and free parking for rideshare users;
- secured bicycle storage, showers and locker rooms for bicycle commuters;
- safe and enhanced pedestrian circulation and access to the Metro station through the development of an active pedestrian environment; and
- participation in Eisenhower East transportation management program, when established.

These transportation management measures will allow the Plan’s proposed 43% non-SOV mode share projections to be achieved.

Principle: Ensure a network of urban streetscapes.

The applicant is providing a 15 ft.-wide continuous brick sidewalk on Mill Road and is retaining the existing sidewalk on Elizabeth Lane which, combined with the continuous row of street trees and landscaping, will enhance the urban streetscape for each street frontage. In an emerging area of retail and neighborhood-serving uses in close proximity to a Metro station, it is important that pedestrian access and facilities be improved. Staff is recommending landscape and hardscape improvements along the frontages including additional landscaping, benches, pedestrian scale lighting, trash receptacles and a bus shelter on Mill Road.

Principle: Provide a jobs/housing balance, development tied to performance criteria and an appropriate level of affordable housing.

The Plan seeks to achieve a 50/50 balance between housing and office uses. This goal is a basis for the allocation of floor area and land uses throughout the planning area. Developing within the parameters of the Plan helps achieve the objective of a jobs/housing balance. To date, the other conceptual proposals that have been submitted to staff since the adoption of the Plan are beginning to implement the desired balance, as they include 3 residential developments and 3 office developments.

The proposal will be achieving the form and goals of the Plan, with active uses on the ground floor, a building that is oriented to the street, wide sidewalks, transit incentives, parking that is limited in quantity, and a building form and mass that meets the parameters that are established in the Design Guidelines. The proposal does not include any residential units, but the applicant has voluntarily offered to provide an affordable housing contribution of \$147,927.

Principle: Provide a coordinated recreation system of public streets, resource protection areas and open space squares.

While the proposal contains none of the resource protection areas or neighborhood parks identified in the Plan, it does provide a wide sidewalk and bicycle parking to allow people to more easily access the future parks and open space. The proposal will also be required to participate in the Eisenhower East open space plan.

Principle: Ensure parking programs and standards consistent with urban - not suburban - models.

The proposed building and parking structure is Phase I of the larger 3.5-acre site that includes the potential for two new office buildings totaling up to 303,382 sq. ft. Due to the topography change on the site and the need to replace parking displaced by new building and parking structure, the Plan permits a parking structure on the eastern portion of this site, though the garage must be architecturally treated. The Plan permits the garage to be up to 5 levels and a total height of 45 ft.

The proposed parking structure will contain a total of 323 parking spaces. These spaces will be comprised of 196 parking spaces for the new building (148 long-term office parking spaces, 30

short-term office spaces, and 18 retail spaces), 77 spaces to replace the Eisenhower Center I and II spaces that were lost to the redevelopment, and 50 spaces for jurors for the Federal courthouse. For the new building (located within 1,500 feet of the Eisenhower Metro Station) the Plan permits 1.66 employee parking spaces and 0.34 visitor/drop-off spaces per 1,000 sq. ft. of office, and 2 spaces per 1,000 sq. ft. of retail. For the proposed building, this calculates as 196 parking spaces. The parking totals comply with the amount of parking envisioned by the Plan.

B. Compliance with the Eisenhower East Design Guidelines:

As previously discussed, the Plan requires approval by a design review board to ensure compliance with the Design Guidelines. The Design Guidelines are intended to provide a clear set of design expectations for the property owners, developers, and their architects to ensure that all new projects contribute to a high quality environment within Eisenhower East. The applicant submitted a proposal that asserts its presence without competing with the “signature” sites or gateway features. A challenge with the current proposal is that the buildings to the north, south and west of the building will be considerably taller (87 to 168 ft.) taller. The building volume is setback at the top to add visual interest for the top of the building, and the design provides a tower element, pedestrian entrances on the street. The design is focused on the pedestrian level - on the base of the building and the treatment of the retail bays, and a high level of detail and quality materials are provided.

The proposed building as outlined in the table below complies with the quantitative and qualitative requirements of the Design Guidelines. The Guidelines are intended to provide parameters and ensure high quality materials but not to dictate style or application of the Guidelines, while allowing flexibility for high quality design.

Summary of Eisenhower East Design Guidelines		
Criteria	Standard	Provided
Building height	10-15 stories; 200 ft. max	6 stories; 85 ft. w/ 92 ft. tower <i>Allowable floor area makes a taller building impractical.</i>
Parking	2 spaces / 1,000 sq. ft. max	2 spaces / 1,000 sq. ft. plus replacement spaces and courthouse juror parking
Massing	Building shall front street 75% or more of facade must meet build-to line	Building fronts street 76% of facade meets build-to line
Building Stepbacks	10 - 20 ft. setback @ 40 - 60 ft. building height	10 ft. setback at 60 ft.
Building Base	Building base to be 5 stories	Building base 1 story <i>Larger base not appropriate given total building height</i>

Facade	Facade must articulate a residential scale w/ varied surface, articulation of color, scale, & material	Requirement met
Articulation	Building to have vertical windows and punched openings Building to have base, middle, & top	Requirement met
Materials	Building to be masonry "Heavier" material used for base	Building is brick, precast concrete and stone, w/ stone at base
Solid/Void Ratio	Retail areas - 75-95% glazing Non-retail - 35-55% glazing	Retail - requirement met 47% glazing (37% vision glass, plus 10% spandrel)

C. Approval of the Proposed Building by a Design Review Board:

Building:

The Design Review Board (DRB) reviewed the proposal on June 30, 2005, and found that the building complies with the Design Guidelines. The proposal was approved with conditions, and the proposal has since been modified to incorporate those conditions. The DRB found that the corner element expressed as a tower (six stories against the five story background block) has an appropriate differentiation in color and scale from the balance of the building. Both the tower and the main portion of the building incorporate a clearly defined base, middle, and top, and the relative proportions of these parts seem appropriately balanced to the scale of the building. The setback of the remainder of the top floor, and its contrasting color, create a successful transition between building and sky, and the treatment of the penthouse seems appropriate. The Board found that the overall approach and basic fabric of the office façade is generally well composed, and the proposal complies with the intent of the Design Guidelines.



South Elevation

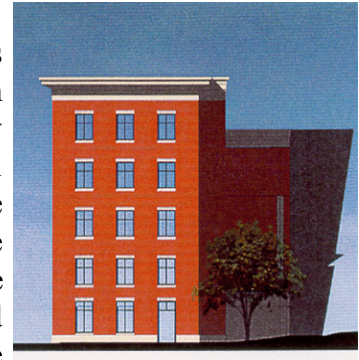
Parking Structure:

The only vehicular entrance to the parking structure will be on the west side of the structure, facing the proposed building and out of view of public streets. The garage will be clad in brick and precast, and is designed to have the appearance of a manufacturing/warehouse building, complete with regularly spaced punched openings. The south and west



Parking Structure - South Elevation

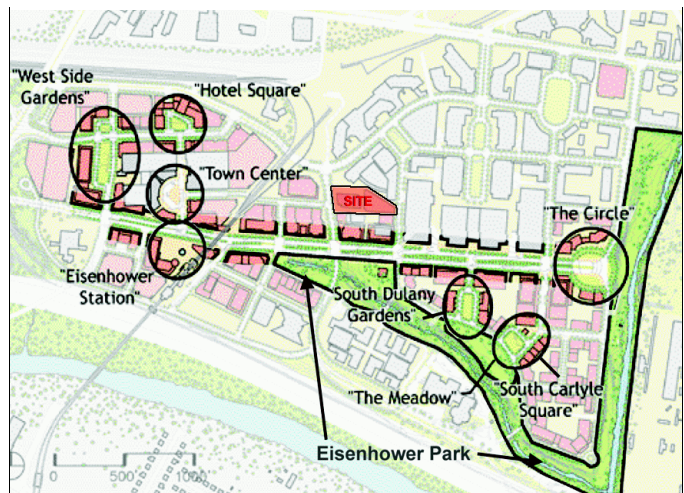
openings will not have glass and will be open with decorative railing, while the east openings (those facing the public street) will have glass windows. The parking structure has a unique wedge shape which provides two benefits. First, the parking structure has a very narrow street frontage, only 34 feet, on the Elizabeth Lane facade, which will have the appearance of a small office building. Second, all of the ramps will be on the north face of the garage, a side which must be solid due to security concerns of the courthouse. Therefore, all of the parking slabs will be level and the structure will not have the slanted horizontal structure that typically characterizes parking garages. The parking structure was reviewed and approved by the DRB without any modifications.



**Parking Structure -
East Elevation**

D. Participation in a Eisenhower East Open Space Fund:

The Plan provides for a variety of parks and open space to include “a comprehensive system of urban spaces, parks and conservation areas that are adequately sized and properly located to serve the neighborhood and the City.” In order to implement the Plan, the City has identified specific urban spaces/neighborhood parks including areas adjacent to Blocks 2 and 3, Blocks 24 and 25, and Blocks 25 and 27. In addition, significant conservation areas are reflected in Block 31 up to and adjoining the existing African-American Heritage Park, and a “green” median for Eisenhower Avenue east of the Eisenhower Metro station. To implement the acquisition, remediation and construction of the parks and open space, the City has prepared an Eisenhower East Open Space Plan. As explicitly set forth in the Plan, the implementation will “include a determination of the appropriate fair-share contribution of each of the property owners or developers”.



Eisenhower East Parks & Open Space

There have been discussions with the applicant and other potential property owners within Eisenhower East regarding this policy and the likely amount of contribution for open space. The amount initially discussed with the applicant was approximately \$1.75 to \$2.00/sq.ft. In the past couple months with more detailed analysis of the future park construction and more detailed discussions with Hoffman as part of their concept plan, this amount has become more refined and is approximately \$1.75/sq.ft. This amount and the methodology will be discussed in considerably more detail at the October Planning Commission hearing.

E. Participation in a Eisenhower East Transportation Management Plan (TMP):

The adoption of the Plan was predicated in major part upon the results of the comprehensive traffic study conducted for Eisenhower East, and the City continues to evaluate each project against the background of that traffic study. In addition, each development application must include a Transportation Management Plan (TMP). Several TMPs currently exist within Eisenhower East and include monetary and non-monetary contributions for transportation management. In addition, in accordance with the Plan, as the critical mass of development increases it is anticipated that “a district wide transportation management program” will be established. Concept plan approval should include similar TMP contributions and should include a condition for participation in the transportation management plan. Then-existing individual TMPs would be annexed into the district program.

F. Infrastructure Improvements:

The Plan notes that “significant funding [for improvements] will clearly be required, through an equitable or fair-share funding of the improvements”. One method discussed in the Plan is the creation of a special tax district. The City is considering what funding mechanism(s) would be appropriate to obtain the necessary “equitable or fair-share funding.”

G. Green Building:

Staff is recommending that the applicant incorporate elements of environmentally sensitive methods in the construction and operation of the proposed building. There is not any one single technique for designing and building a green building to these specifications, but certified buildings often preserve natural vegetation, contain non-toxic or recycled-content building materials, maintain good indoor air quality, use water and energy efficiently, conserve natural resources, feature natural lighting, and recycled construction materials. The City will work with the applicant as part of the building permit process and through a checklist to implement green and sustainable systems within the building. To address conditions for green building design, the applicant provided specific elements and features which are to be incorporated into the building’s design and operations. The City has begun to track various methods and materials for achieving this objective for development projects where this requirement is imposed by the Planning Commission or City Council.

VI. COMMUNITY:

A meeting was held at Carlyle Towers on April 26, 2005. The residents generally expressed support of the proposal, especially with the hope that the development would introduce new retail into the area and that the new parking structure might reduce demand for on-street parking. Concerns were also expressed about the following:

- Traffic impacts;
- Construction impacts (noise, traffic, walkway obstructions, and aesthetic);
- Appearance of parking structure, including solid wall facing courthouse; and
- Adequacy and screening of refuse containers.

VII. Conclusion:

Staff recommends **approval** of the proposed development with conditions as outlined in this staff report.

STAFF: Eileen Fogarty, Director, Department of Planning and Zoning;
Jeffrey Farner, Development Division Chief
David Sundland, Urban Planner

VIII. STAFF RECOMMENDATION:

Staff recommends **approval** subject to compliance with all applicable codes and ordinances and the following conditions:

A. MASS TRANSIT INCENTIVES:

1. The applicant shall create a program and implement a reporting system to encourage the use of mass transit, carpooling, teleworking, and ridesharing and discouraging the use of single occupancy vehicles to the satisfaction of the Directors of P&Z and T&ES by providing the following:
 - a. A TMP coordinator with experience in this occupation shall be designated for the project upon application for the certificate of occupancy permit for the building. The TMP coordinator shall have an on-site office, and the name, location and telephone number of the coordinator will be provided to the City at that time, and the City will be notified at the time of any changes. This person will be responsible for implementing and managing all aspects of the TMP and the parking management program for the project, and for providing biannual reports to the Office of Transit and Programs in the Department of T&ES. The biannual reports will include an assessment of the effects of the previous six month's TMP activities on carpooling, vanpooling, and transit ridership; an accounting of receipts and disbursements for any TMP accounts, and a work program for the subsequent six months.. The TMP coordinator shall be provided for the individual project until the overall TMP for Eisenhower East is implemented.
 - b. The applicant shall participate with other developments in the Eisenhower East TMP in the mutually agreed upon cooperative planning and implementation of transportation activities.
 - c. Transit, ridesharing, staggered work hours/compressed work weeks, parking restrictions, the elements of the parking management plan and the other program elements shall be promoted to prospective tenants and to employers and their employ
 - d. Information about all transit, ridesharing, and other TMP elements shall be distributed and displayed to employers, and employees—including transit schedules, rideshare applications and information, incentive information, parking information, etc. This information shall be kept current. Displays of these brochures and applications shall be provided in a prominent location within each building and a web site with this information and appropriate links to transit providers will be provided and maintained.
 - e. A ridesharing program shall be established that includes not only participation in the regional Metropolitan Washington Council of Governments Commuter Connections Program, but also site-specific matching efforts.
 - f. A Guaranteed Ride Home Program shall be established and promoted as part of the ridesharing and transit marketing efforts.
 - g. A share car program shall be established and marketed as part of the ridesharing and transit marketing efforts for all buildings. At a minimum at least two parking spaces should be reserved for the location of carshare vehicles. These spaces should be in a

convenient location for tenants and residents and the TMP Coordinator will arrange with any of the carshare companies for placement of vehicles in this project. (Currently, Zipcar and Flexcar both have vehicles in the Alexandria area.). For those individuals that take transit, carpool/vanpool, walk, or bike to work the TMP program will pay the registration and annual membership fees (not the usage fees) to use the carshare vehicles.

- h. Discounted bus and rail fare media shall be sold on-site to employees of the project including during hours that are convenient for residents who work. The fare media to be sold will include, at a minimum, fare media for Metrorail, Metrobus, DASH and any other public transportation system fare media requested by employees and/or the Office of Transit Services and Programs. The availability of this fare media will be prominently advertised.
- i. The project shall have a goal of a minimum of 43% of the employees using transportation other than single-occupancy vehicles during the peak time periods.
- j. At full occupancy of the building, a survey of employees shall be conducted to determine the number of employees, their place of residence/employment, modes of transportation, arrival and departure times, willingness and ability to use carpooling and public transit, and such additional information as the City may require. This survey will be conducted annually.
- k. The applicant and/or building tenants shall encourage use of a staggered work hour program for office workers including the promotion of the program among existing and prospective employees, the registration of staggered work hour participants, issuing stickers and/or electronic cards to verify vehicles participating in the program and monitoring the program.
- l. The applicant shall provide two (2) showers per gender to the satisfaction of the Director of T&ES and P&Z. A minimum of eight (8) clothes storage lockers per gender shall be installed. The lockers shall be accessible to all tenants of the building to the satisfaction of the Director of T&ES.
- m. The applicant shall participate in Ozone Action Days and other regionally sponsored clean air, transit, and traffic mitigation promotions by advertising such promotions in a manner and at such locations within the building acceptable to the applicant.
- n. The applicant shall fund at an annual rate per occupied square foot of commercial space a transportation management account to be used exclusively for the transportation activities listed above. The amount of the funding shall be consistent with the approved Eisenhower East Transportation Management Plan. As determined by the Director of T&ES and P&Z, any unencumbered funds remaining in the TMP account at the end of each reporting year may be either reprogrammed for TMP activities during the ensuing year or paid to the City for use. (P&Z)

B. PARKING:

- 2. The applicant shall provide a parking management plan which outlines mechanisms to maximize the use of the parking structure to the satisfaction of the Directors of P&Z and T&ES. At a minimum the plan shall include:

- a. Parking rates for the parking within the parking structure shall be consistent with market rates of comparable buildings located in adjoining developments within the City of Alexandria, except that free parking may be provided for retail patrons. All office employees shall be required to pay market rates for parking; no parking may be provided free or at reduced rates or with costs reimbursed by the employer unless that employer provides an equivalent benefit to all employees who utilize transit options to commute; i.e., if an employer provides a \$100 parking space to an employee free of cost, that employer must also offer a pretax benefit for transit of \$100 to all transit users.
- b. Parking spaces for carpool vehicles shall be conveniently located adjacent to garage entrances and exits, and/or elevator locations.
- c. The retail businesses stipulate that their employees who drive to work to use off-street parking.
- d. The applicant shall provide controlled access into the parking structure.
- e. The controlled access to the parking structure for short-term spaces shall be designed to allow convenient access for employees, visitors, and retail patrons.
- f. Spaces defined as “short-term” parking shall be solely utilized for use by the retail use and shall include all appropriate signage.
- g. The spaces within the parking structure shall be part of the maximum number of parking spaces permitted for Phase I and Phase II and the overall development shall be consistent with the parking requirements of the Eisenhower East plan.
- h. The walls and ceilings in the garages are to be painted white unless the photometric lighting plan demonstrates that sufficient lighting is being provided, to the satisfaction of the Police Chief and the Director of T&ES.
- i. The applicant agrees to provide parking for all construction workers without charge to the workers or shall provide subsidy for the construction workers in order that they may use Metro, DASH, provide a van for van pooling, or another method of providing for construction workers to arrive at the site. Compliance with this condition shall be based on a plan, which shall be submitted to the Department of Planning & Zoning and Transportation and Environmental Services prior to the issuance of the Excavation/Sheeting, and Shoring Permit. This plan shall set forth the location of the parking to be provided at various stages of construction, how many spaces will be provided, how many construction workers will be assigned to the work site, and mechanisms which will be used to encourage the use of Metro, carpooling, vanpooling, and other similar efforts. The plan shall also provide for the location on the construction site at which information will be posted regarding Metro schedules and routes, bus schedules and routes, and carpooling and vanpooling information. If the plan is found to be violated during the course of construction, a correction notice will be forwarded to the applicant. If the violation is not corrected within ten (10) days, a "stop work order" will be issued, with construction halted until the violation has been corrected. (P&Z)

C. PEDESTRIAN - STREETScape:

3. The applicant shall provide pedestrian streetscape improvements that at a minimum shall provide the level of improvements depicted on the preliminary site plan and shall provide the following, consistent with the Eisenhower East Design Guidelines to the satisfaction of the Director of P&Z and T&ES:
 - a. All sidewalks shall be City standard red brick and shall be a herringbone pattern.
 - b. The brick sidewalks shall continue over the proposed curb cut on Mill Road to provide a continuous uninterrupted brick sidewalk.
 - c. A TimberForm Restoration Series, Model #2118 public bench shall be required for the Mill Road frontage.
 - d. An Iron Site Bethesda Series, Model S-42 decorative black metal trash can shall be provided for the Mill Road frontage.
 - e. A minimum unobstructed sidewalk width of 15 feet on Mill Road, 6 feet on Elizabeth Lane, and 7 feet on southern sidewalk.
 - f. The design of the bus shelter shall match that which was approved for Carlyle, as is depicted in *Attachment 1*.
 - g. A minimum of 1 bicycle parking space per 20 vehicular parking spaces, to the satisfaction of the Director of T&ES. Bicycle parking spaces for employees shall be secure. Outdoor bicycle rack(s) for use by retail patrons and visitors shall be decorative.
 - h. Street lighting for all streets shall be a single acorn luminaires as specified in the Design Guidelines (W.J. Whatley Washington series fiberglass pole, General Electric Edison III luminaire, and Hadco Victorian III series refractive globe).
 - i. All pedestrian and traffic signage in accordance with the Manual of Uniform Traffic Control Devices (MUTCD), latest edition to the satisfaction of the Director of T&ES.
 - j. All driveway entrances, sidewalks, curbing, etc. in public ROW or abutting public ROW shall meet City design standards.
 - k. All existing curb and gutter, sidewalks, and handicap ramps that are in disrepair or broken shall be replaced.
 - l. All streetscape improvements shall be completed prior to the issuance of a certificate of occupancy permit. (P&Z) (T&ES)

4. The applicant shall provide lighting within the project and the adjoining rights-of-way pursuant to a plan approved by the Director of T&ES and Director of P&Z in consultation with the Chief of Police; the lighting plan shall incorporate the following requirements and elements:
 - a. Show existing and proposed street lights and site lights. Indicate the type of fixture, and show mounting height, and strength of fixture in lumens or watts.
 - b. Provide manufacturer's specifications for and installation schedule indicating the number of each fixture to be installed.
 - c. Provide lighting calculations and photometric plan to verify that lighting meets City Standards. Lighting plan to cover site, adjacent right-of-way and properties. (T&ES)

D. OPEN SPACE - LANDSCAPING:

5. The applicant shall participate and contribute the monetary open space contribution as required by the Eisenhower East Open Space Fund. The contribution shall be paid prior to the release of the final site plan or issuance of a building permit(s) for the project. (P&Z)(RPCA)
6. A perpetual public access easement shall be granted for the sidewalk on Mill Road located outside the public right-of-way. All easements and reservations shall be approved by the City and recorded prior to the release of the final site plan. (P&Z)
7. A final landscape plan shall be provided with the final site plan to the satisfaction of the Director of P&Z and RP&CA. The plan shall use industry standard nomenclature, shall include the level of landscaping depicted on the preliminary landscape plan, and shall at a minimum also provide:
 - a. A combination of deciduous and evergreen plantings on the southern portion of the parking structure. The spacing for the evergreen plants should be approximately 10 ft. on-center, while the deciduous trees should be approximately 25 ft. on-center.
 - b. Foundation plantings and shrubs along the southern portion of the building.
 - c. Additional trees and landscaping on the northwestern portion of the site between the building and the property line. Low growing shade tolerant shrubs/plants shall be planted beneath the proposed trees on northern portion of the site between the proposed and existing building.
 - d. Provide landscaping on the northeastern portion of the parking structure.
 - e. Provide an additional Japanese Zelkova tree on Elizabeth Lane.
 - f. Board and batten fencing and other measures if identified by the City Arborist shall be provided to protect the existing street trees on Elizabeth Lane during construction. Should any pruning of the existing trees be necessary during construction such pruning must be undertaken by a certified arborist. All tree protection shall be installed prior to any demolition, grading or construction.
 - g. All street trees shall be planted in a continuous planting trough with aeration, drainage and irrigation systems. The trough shall be large enough to provide sufficient arable soil volume to support adequate moisture for the tree. Troughs shall be a minimum of thirty inches deep and six feet wide from the face of curb.
 - h. Groundcover shall be provided within each tree well.
 - i. The street trees shall be a minimum of 3.5" to 4" caliper at the time of planting.
 - j. The location of all light poles shall be coordinated with the street trees.
 - k. The final landscape plan shall be prepared by a licensed landscape architect.
 - l. All utility lines shall be located away from the proposed landscaped areas to minimize the impact upon the proposed landscaping. Any switch boxes or transformers shall be located on the final site plan to the satisfaction of the Directors of P&Z and T&ES.
 - m. All plant specifications shall be in accordance with the current and most up to date edition of the American Standard For Nursery Stock (ANSI Z60.1) as produced by the American Association for Nurserymen; Washington, D.C.

- n. All proposed plant sizes are to be indicated (“B&B” is not a caliper designation, and “2” is not a size designation).
- o. Designation of “Acer rubrum cultivar” is not sufficient. Provide both genus and species names. Provide correct spelling of “Prunus” Provide species designation for Ilex x meserveae.
- p. All work shall be performed in accordance with the latest and most current edition of the Landscape Specifications Guidelines, as produced by the Landscape Contractors Association (LCA) of Maryland, District of Columbia and Virginia; Gaithersburg, Maryland.
- q. Depict all utility structures, including transformers, on the final development plan. All utility structures (except fire hydrants) shall be clustered where possible and located so as not to be visible from a public right-of-way or property. When such a location is not feasible, such structures shall be located behind the front building line and screened.
- r. All landscaping shall be maintained in good condition and replaced as needed. (P&Z) (RP&CA)

E. BUILDING:

- 8. The final architectural elevations shall be consistent with the level of quality and detail provided in the preliminary architectural elevations dated August 10, 2005, and shall comply with the approved elevations of the Design Review Board. In addition, the applicant shall provide additional refinements to the satisfaction of the Director of P&Z that shall at a minimum include:
 - a. The facade materials for the front, side and rear of the building other than the screening for the mechanical equipment shall be entirely masonry (brick, precast, stone).
 - b. For the parking structure, all windows which face Elizabeth Lane, as well as the first set of windows (all five levels) at the east end of the south and north facades, shall be vision glass.
 - c. The height of the upper parapet of the parking structure shall be limited to 45 ft., as measured from the level of the Elizabeth Lane public sidewalk at the northeast corner of the property.
 - d. In order to reduce the visibility from Elizabeth Lane of light poles on top of the parking structure, the use of freestanding light poles on the top level of the parking structure shall be minimized and the height shall be the minimum necessary. The use of bollard lighting or similar light sources other than freestanding poles is encouraged.
 - e. The screening for the mechanical equipment shall be reduced in size to the extent feasible and shall incorporate design elements of the building such as panelization.
 - f. The applicant shall provide wall-mounted light fixtures as depicted on the site plan. The style and locations shall be designed as an integrated part of the facade to enhance nighttime visual interest of the building.
 - g. The applicant shall provide larger scale drawings to evaluate the retail base, cornice brackets, entrance canopies and sign bands and that the final detailing, finish and color

of these elements. These detail elements shall be submitted prior to review and approved prior to the release of the final site plan.

- h. The entrance canopies shall be a high quality metal and/or comparable material.
 - i. Color architectural elevations (front, side and rear) shall be submitted with the final site plan.
 - j. Provision shall be made for constructing exhaust vent shafts and grease traps within the building to accommodate future ground floor restaurant uses.
 - k. The screening for the proposed dumpster enclosure shall be brick with an opaque metal gate.
 - l. Low-level lighting at the retail base as an integral part of the facade design to add nighttime visual interest to the buildings. Accent lighting is encouraged. (P&Z)
9. The applicant shall demonstrate the use of green building and sustainable techniques for building systems design for the project. The applicant shall provide examples as identified in the following list to the satisfaction of the Directors of P&Z and T&ES:

Sustainable Sites

- a. Utilize an Energy Star rated membrane roofing that exhibits a high reflectivity and emissivity. This roof can reduce cooling load, and thus HVAC size, by as much as 10%.
- b. Minimize exterior lighting fixtures. Provide shielding to exterior lights to ensure that there is no direct beam light trespass onto adjacent property lines.
- c. Provide bicycle storage facilities and showers as well as other methods to encourage alternative transportation to the site.

Water Efficiency

- d. Utilize native or adaptive plant species for the exterior planting beds and boxes.
- e. Provide water efficient fixtures.

Energy and Atmosphere

- f. Perform fundamental building commissioning prior to occupancy to ensure optimal performance of the building's systems.
- g. Minimize the need for artificial lighting for the interior spaces by maximizing day-lighting opportunities.

Materials and Resources

- h. Work to reuse salvageable materials from the existing building on site and for leftover building materials upon completion of construction.
- i. Provide centralized recycling collection point(s) within the building.
- j. Provide a waste management plan to target a reduction of waste being transported to local landfills.
- k. Where practical utilize materials that have high recycled content, such as steel and concrete with flyash. Purchase locally harvested and manufactured materials where practical.
- l. Provide tenant fit-out guidelines to encourage that materials chosen are environmentally sensitive.

Indoor Environmental Quality

- m. Encourage open office spaces with low partitions along the perimeter of the building to maximize day-lighting into the space. Encourage the placement of enclosed spaces toward the core of the building and glass partitions or vision panels to take advantage of day-lighting.
 - n. Provide interior finishes such as paint and carpet with low VOC off-gassing. (P&Z)
10. The ground floor retail tenant spaces, as depicted on the Preliminary Plan dated April 27, 2005, shall be solely utilized by retail uses including: a store engaged in the sale of goods for personal use that shall include bakeries, barber shop/beauty salon, banks, bookstores, clothing, clothing accessories, copier/reproductions, department stores, drugstores, dry cleaners(not dry cleaning plant), florists, cigar shops, travel agencies, health and sport clubs, groceries, jewelry, restaurants and any similar uses deemed by the Director of Planning and Zoning to meet the intent of providing active pedestrian-oriented neighborhood-serving retail uses. The floor-to-floor height of the retail space shall be a minimum of 18 ft. (P&Z)
11. The colors and materials of the retail tenant signs shall be designed of high quality materials and shall be designed as an integral part of the building that shall relate in materials, color and scale to the remainder of the building and to the retail bay on which it is displayed to the satisfaction of the Director of P&Z and shall comply with applicable codes and ordinances as well as the following:
- a. All signs must comply with the zoning ordinance requirements.
 - b. Illuminated or non-illuminated parapet signs or wall signs above the first level for retail uses are prohibited.
 - c. Signs applied to storefront windows shall cover no more than 20% of the glass.
 - d. Box signs shall be prohibited.
 - e. Any exterior decorative exterior banners/flags shall be deducted from the overall permitted sign area. Permanent or temporary advertising banners shall be prohibited.
 - f. Display cases, storage, carts or other obstructions shall not be designed to be temporarily or permanently located adjacent to the retail windows. Tables and other active uses adjacent to the window are encouraged.
 - g. Pedestrian scaled projecting signs over the sidewalk are encouraged for the retail uses.
 - h. Awnings are encouraged for retail and residential uses, but shall not be plastic or internally illuminated;
 - i. No freestanding signs other than traffic/directional signs shall be permitted. (P&Z)
12. The applicant shall provide a written letter of agreement between the applicant and the U.S. Marshals Service which provides a fire access easement on the northern portion of the parking structure to the satisfaction of the Director of Code Enforcement. The letter shall be submitted and approved prior to the submission of the final site plan. (Code)
13. The south face of the garage structure does not have ladder truck access. The applicant shall provide an emergency vehicle easement with the adjacent property owner prior to submission of final site plan. The easement shall be dedicated and recorded in the land records as an emergency vehicle easement. The easement shall be signed in accordance with City standards

for emergency vehicle easements. Fire apparatus turning movements which show apparatus can safely negotiate 4 foot radii by Eisenhower Office I shall be provided. Specifications for tiller and rear mounted ladder trucks shall be used. (Code)

14. All exterior walls within 5 feet from an interior property line shall have a fire resistance rating of 1 hour, from both sides, with no openings permitted within the wall. As alternative, a 2 hour fire wall may be provided. (USBC 704.5). The proposed openings are in conflict with the USBC. Applicant shows bricked recesses in lieu of openings and 1 hour rated solid wall.
15. The office building shall provide an automatic sprinkler system. Provide data as part of fire protection plan in Building Code Analysis. The garage may require an automatic sprinkler system depending upon final design requirements and how they conform to the openness requirements of the USBC. (Code)
16. Provide two Siamese connections for each structure located to the satisfaction of the Director of Code Enforcement. Relocate FDC at Mill and Grist Mill to Mill Road side (West face of building). A separate tap is required for the building fire service connection. Provide tap and fire line for garage. Hydrants and taps are off dead end main. Loop main back to Mill Road. (Code)
17. The developer shall provide a building code analysis with the following building code data on the plan:
 - a. use group;
 - b. number of stories;
 - c. type of construction; d) floor area per floor;
 - d. fire protection plan.Building Code Analysis must be updated to provide floor area per floor of garage and Fire Protection Plan. (Code)
18. The developer shall provide a separate Fire Service Plan which illustrates: a) emergency ingress/egress routes to the site; b) two fire department connections (FDC) to the building, one on each side/end of the building; c) fire hydrants located within on hundred (100) feet of each FDC; d) on site fire hydrants spaced with a maximum distance of three hundred (300) feet between hydrants and the most remote point of vehicular access on site; e) emergency vehicle easements (EVE) around the building with a twenty-two (22) foot minimum width; f) all Fire Service Plan elements are subject to the approval of the Director of Code Enforcement.(Code)

F. SITE PLAN:

19. The applicant has acknowledged that the approved Eisenhower East Plan contemplates the creation of a Special Services taxing district in the Eisenhower East area, and that the applicant's property would likely be included in that Eisenhower East district and pay an additional tax levy if and when in the future the City establishes such a district. (P&Z)
20. The applicant shall be allowed to make minor adjustments to the building location if the changes do not result in the loss of off-street parking, loss of plaza open space, does not result in an increase in building height or increase in floor area. (P&Z)
21. Temporary construction trailer(s) shall be permitted and be subject to the approval of the Director of P&Z. The trailer(s) shall be removed prior to the issuance of a certificate of occupancy permit for the building. (P&Z)
22. The applicant shall identify a person who will serve as liaison to the community throughout the duration of construction. The name and telephone number of this individual shall be provided in writing to residents, property managers and business owners whose property abuts the site, and to the Directors of P&Z and T&ES. (P&Z)
23. A temporary informational sign shall be installed on the site prior to approval of the first final site plan for the project and shall be displayed until construction is complete or replaced with a marketing sign incorporating the required information: the sign shall notify the public of the nature of the upcoming project and shall provide a phone number for public questions regarding the project. (P&Z)
24. Before commencing any clearing or grading of the site, the applicant shall hold a meeting with all adjoining property owners to review the hauling routes, location of construction worker parking, plan for temporary pedestrian and vehicular circulation, and hours and overall schedule for construction. The Departments of P&Z and T&ES shall be notified of the date of the meeting before the permit is issued. Copies of plans showing the hauling route, construction worker parking and temporary pedestrian and vehicular circulation shall be posted in the construction trailer and given to each subcontractor before they commence work on the project. (P&Z)
25. Prior to the release of the final site plan, provide a Traffic Control Plan for construction detailing proposed controls to traffic movement, lane closures, construction entrances, haul routes, and storage and staging. (T&ES)
26. The applicant shall submit a final as-built site plan (with landscape plan) and interior layout of the parking garage prior to issuance of a certificate of occupancy permit. (P&Z)

27. The applicant shall submit final plats of subdivision, and dedication that shall be approved prior by the Department of P&Z, T&ES and the City Attorney prior to release of the final site plan. (P&Z)
28. As required by Section 11-700 under Article XI of the City of Alexandria Zoning Ordinance, the special use permit and conditions attached thereto as granted by City Council, unless revoked or amended, shall run with the land and shall be mandatory and binding upon the applicant, all owners of the land and all occupants and upon all heirs, successors and assignees with whom sale or lease agreements are executed subsequent to the date of this approval. (P&Z)(T&ES)
29. If requested by the City, the applicant shall cooperate in the establishment of and participate in a special service tax district, or another district having a comparable purpose, in the Eisenhower East area. (P&Z)
30. Provide detail for the 6' screening wall in relation to the dumpster and the dumpster pad. (T&ES)
31. Provide enough dumpster space for solid waste and recycling material. The applicant shall provide for private trash collection for the retail spaces. Provide a note on the plans indicating means and methods of collecting trash and recycling material and transporting it to the Alexandria Waste Energy Plant. (T&ES)
32. Provide labels for all sidewalks, handicap ramps and city standard manholes. Provide signage, striping and appropriate markings for the EVE, handicap parking spaces, and crosswalks. (T&ES)
33. Show proposed slopes for all ramps within the garage, and limit ramp slopes to no more than 10%. In the event that a 10% slope is unattainable and to alleviate the effect of having such a steep slope, the applicant shall provide adequate drainage at the driveway entrance, heated ramps with transverse grooving or other means acceptable to the Director of T&ES. (T&ES)
34. All Traffic Control Device design plans, Work Zone Traffic Control plans, and Traffic Studies shall be sealed by a professional engineer, registered in the Commonwealth of Virginia. (T&ES)
35. Clearly label all trees to be demolished in the existing site plan exhibit. (T&ES)
36. The 8'x16' bus shelter easement shall be labeled as an easement. (T&ES)
37. Provide the size of the storm sewer pipe indicated on the plans to be demolished. (T&ES)
38. Provide a 10 ft. water line easement for mains and hydrants out of the public right-of-way. (VAWC)

39. A security survey is to be completed for any sales or construction trailers that are placed on the site. This is to be completed as soon as the trailers are placed on site by calling the Community Relations Unit at 703- 838-4520. (Police)
40. All retail tenants shall contact the Community Relations Unit of the Alexandria Police Department at 703-838-4520 regarding a security survey for the business and employees. (Police)
41. Additional Emergency Vehicle Easement signs shall be provided on fire lane entering from Mill Road and through new EVE along South face of garage. (Code)

G. STORMWATER:

42. Clarify that the existing 15" RCP storm pipe at eastern end of the property is publicly maintained; if so, provide evidence that there is an easement. The proposed BMP should be installed on the proposed pipe. The applicant is responsible for the maintenance of the bmp structure. (T&ES)
43. The applicant shall comply with adequate outfall requirement of Article XIII of the Alexandria Zoning Ordinance (AZO) and demonstrate this on the plans by means of computations. (T&ES)
44. The applicant is encouraged to involve the stormwater management designer at an early stage of the site plan process in order to ensure future submissions incorporate stormwater design aspects into the site design in accordance with Article XIII of the Zoning Ordinance. (T&ES)
45. The pre-development divides is showing the existing Eisenhower Center 1 building included, however the area has not been tabulated. The summation of the areas under the post development conditions exceeds the area under the pre development condition. Revise as needed for conformity. (T&ES)
46. Provide divisional arrows on all drainage divides. (T&ES)
47. Provide calculation to show that the 6" sanitary lateral is sufficient for the proposed office building. (T&ES)
48. Applicant should provide and receive approval of an erosion and sediment control plan for demolition before demolition permit is approved. (T&ES)
49. Plan must demonstrate to the satisfaction of the Director of T&ES that adequate stormwater outfall is available to the site or else developer is to design and build any on or off site improvements to discharge to an adequate outfall. (T&ES)

50. On Sheet 9, the line for the limits of clearing and grading is too similar to the drainage limits line. Make the distinction between these two more clear. Drainage divides are not clearly marked. It is impossible to determine, given the information provided, which draining areas drain to what storm sewer inlets. Provide complete drainage divide information for all existing and proposed storm sewer inlet location. Provide more topographic information to the north of the site to verify drainage divide along the property line and existing wall shown. (T&ES)
51. On Sheet 9 the fraction of CBPA drainage area served is incorrectly calculated. The figure should be .8578, with the load removed being 1.278 lbs/year. (T&ES)
52. Stormwater Narrative: Under Water Quality, it is still unclear how the “98% of the total impervious drainage area being treated by the aquaswirl” is being computed. (T&ES)
53. Project lies within the Cameron Run Watershed thus stormwater control must be such that post development stormwater runoff can not exceed 90% of the preexisting condition for the 2-year and 10-year events. (T&ES)
54. This project lies within 1000 feet of an old landfill. Due to the historic uses at the site and the potential for contamination, the applicant shall design and install a vapor barrier and ventilation system for the buildings and parking areas to prevent the migration or accumulation of methane or other gases under parking areas or into buildings, or conduct a study and provide a report signed by a professional engineer showing that such measures are not needed to the satisfaction of Directors of T&ES and Code Enforcement. (T&ES)
55. The final site plan shall not be released and no construction activity shall take place until the following has been submitted and approved by the Director of T&ES:
 - a. Submit a Site Characterization Report/Extent of Contamination Study detailing the location, the contaminants, and the estimated quantity of any contaminated soils and/or groundwater at or in the immediate vicinity of the proposed site.
 - b. Submit a Risk Assessment indicating any risks associated with the contamination.
 - c. Submit a Remediation Plan detailing how any contaminated soils and/or groundwater will be dealt with, including plans to remediate utility corridors. “Clean” backfill shall be used to fill the utility corridors.
 - d. Submit a Health and Safety Plan indicating measures to be taken during any remediation and/or construction to minimize the potential risks to workers, the neighborhood, and the environment.Please submit 5 copies of each of the above. The remediation plan must be included in the Final Site Plan. (T&ES)
56. The applicant is advised that all stormwater designs that require analysis of pressure hydraulic systems and/or inclusion and design of flow control structures must be sealed by a professional engineer, registered in the Commonwealth of Virginia. If applicable, the Director of T&ES may require resubmission of all plans that do not meet this standard. (T&ES)

57. The stormwater Best Management Practices (BMPs) required for this project shall be constructed and installed under the direct supervision of the design engineer or his designated representative. The design engineer shall make a written certification to the City that the BMP(s) are constructed and installed as designed and in accordance with the approved Final Site Plan. (T&ES)
58. The Developer shall furnish the owners with an Operation and Maintenance Manual for all Best Management Practices (BMPs) on the project. The manual shall include an explanation of the functions and operations of each BMP and any supporting utilities, catalog cuts on any mechanical or electrical equipment, a schedule of routine maintenance for the BMP(s) and supporting equipment, and a copy of the maintenance agreement with the City. (T&ES)
59. The applicant is encouraged to participate in the City's "Adopt-a-Street" program. (T&ES)
60. All waste products including but not limited to organic compounds (solvents), motor oil, compressor lubricant and antifreeze shall be disposed of in accordance with all local, state and federal ordinances or regulations and not be discharged to the sanitary or storm sewers or be discharged onto the ground. (T&ES)
61. During the construction phase of this development, the site developer, its contractor, certified land disturber, or owner's other agents shall implement waste and refuse control program. This program shall control wastes such as discarded building materials, concrete truck washout, chemicals, litter or trash, trash generated by construction workers or mobile food vendor businesses serving them and sanitary waste at the construction site and prevent its off site migration that may cause adverse impacts to the neighboring properties or the environment to the satisfaction of Directors of Transportation and Environmental Services, and Code Enforcement. All wastes shall be disposed off site properly in accordance with all applicable federal, state and local laws. (T&ES)

Staff Note: In accordance with section 11-506(c) of the zoning ordinance, construction or operation shall be commenced and diligently and substantially pursued within 18 months of the date of granting of a special use permit by City Council or the special use permit shall become void.

CITY DEPARTMENT COMMENTS

Legend: C - code requirement R - recommendation S - suggestion F - finding

Code Enforcement

Revised comments in **BOLD**

- F-1 Average finished grade has not been provided on plans.
- F-2 Garage layouts have not been provided showing locations of handicap parking.
- F-3 Fire Service plan shows one vehicle pathway and 2 vehicle specs (pumper and rear mounted ladder truck. It is unclear which spec is depicted in turning movements. Submit turning movement plans separate from fire service plans which show turning movements for rear mounted ladder truck and tiller ladder truck.**
- F-4 Provide additional Emergency Vehicle Easement signs on fire lane entering from Mill Road and through new EVE along South face of garage.**
- F-5 Revise turning radii at Mill Road from R-20 to R25.**
- F-6 The North face of the garage and office are not ladder truck accessible. The adjacent emergency vehicle easement is located on Federal property and is not regulated by the City. The EVE on Federal property is subject to restricted access due to security concerns. As such, a written letter of agreement between the applicant and the U.S. Marshals Service is required which permits fire access to this easement in the event of an emergency on the applicant's property. The letter of agreement shall be signed and submitted as an attachment to Code Enforcement prior to submission of the Final Site plan review. Applicant indicates this agreement is in process. The agreement shall be in place and submitted to Code Enforcement prior to Final #1 plan submission.
- F-7 The South face of the garage does not have ladder truck access. The applicant shall negotiate a emergency vehicle easement with the adjacent property owner prior to submission of final site plan. The easement shall be dedicated and recorded in the land records as an emergency vehicle easement. The easement shall be signed in accordance with City standards for emergency vehicle easements. **Connection has been made to existing EVE. Provide fire apparatus turning movements which show apparatus can safely negotiate 4 foot radii by Eisenhower Office I. Use specifications for tiller and rear mounted ladder trucks.**

- F-8 The garage is shown as located along 2 interior lot lines. All exterior walls within 5 feet from an interior property line shall have a fire resistance rating of 1 hour, from both sides, with no openings permitted within the wall. As alternative, a 2 hour fire wall may be provided. (USBC 704.5). The proposed openings are in conflict with the USBC. Applicant shows bricked recesses in lieu of openings and 1 hour rated solid wall. Issue has been resolved.
- C-1 The office building will require an automatic sprinkler system. **Provide data as part of fire protection plan in Building Code Analysis.**
- C-2 The garage may require an automatic sprinkler system depending upon final design requirements and how they conform to the openness requirements of the USBC. Parking garage will be fully sprinklered per applicant.
- C-3 Provide two Siamese connections for each structure located to the satisfaction of the Director of Code Enforcement. Relocate FDC at Mill and Grist Mill to Mill Road side (West face of building). **Condition met.**
- C-4 A separate tap is required for the building fire service connection. **Provide tap and fire line for garage. Tap provided. Hydrants and taps are off dead end main. Loop main back to Mill Road.**
- C-5 The developer shall provide a building code analysis with the following building code data on the plan: a) use group; b) number of stories; c) type of construction; d) floor area per floor ; e) fire protection plan. **Building Code Analysis is incomplete. Floor area per floor of garage is omitted, Fire Protection Plan omitted.**
- C-6 The developer shall provide a separate Fire Service Plan which illustrates: a) emergency ingress/egress routes to the site; b) two fire department connections (FDC) to the building, one on each side/end of the building; c) fire hydrants located within on hundred (100) feet of each FDC; d) on site fire hydrants spaced with a maximum distance of three hundred (300) feet between hydrants and the most remote point of vehicular access on site; e) emergency vehicle easements (EVE) around the building with a twenty-two (22) foot minimum width; f) all Fire Service Plan elements are subject to the approval of the Director of Code Enforcement. Provided.
- C-7 The final site plans shall show placement of fire easement signs. Acknowledged.
- C-8 A soils report must be submitted with the building permit application. **Acknowledged.**
- C-9 Prior to submission of the Final Site Plan, the developer shall provide a fire flow analysis by a certified licensed fire protection engineer to assure adequate water supply for the structure being considered. **Fire flow provided on 5/13/05, under review.**

- C-10 A Certificate of occupancy shall be obtained prior to any occupancy of the building or portion thereof, in accordance with USBC 119.0. **Condition met.**
- C-11 New construction must comply with the current edition of the Uniform Statewide Building Code (USBC). Shown as Note on Sheet 2.

Transportation and Environmental Services

- C- 1 A performance Bond to guarantee installation of the required public improvements must be posted prior to release of a development plan.
- C- 2 All downspouts must be connected to a storm sewer by continuous underground pipe.
- C- 3 The sanitary sewer tap fee must be paid prior to release of the plan.
- C- 4 All easements and/or dedications must be recorded prior to release of the plan.
- C- 5 Plans and profiles of utilities and roads in public easements and/or public right-of-way must be approved prior to release of the plan.
- C- 6 All drainage facilities must be designed to the satisfaction of T&ES. Drainage divide maps and computations must be provided for approval.
- C- 7 All utilities serving this site to be placed underground.
- C- 8 Provide site lighting plan to meet minimum city standards.
- C- 9 Plan shall comply with the Chesapeake Bay Preservation Act in accordance with Article XIII of the City's zoning ordinance for storm water quality control.
- C-10 Provide a phased erosion and sediment control plan consistent with grading and construction per City of Alexandria, Erosion and Sediment Control Code, Section 5, Chapter 4.
- C-11 The applicant shall comply with the City of Alexandria's Noise Control Code, Title 11, Chapter 5, which sets the maximum permissible noise level as measured at the property line.
- C-12 The applicant shall comply with the City of Alexandria's Noise Control Code, Title 11, Chapter 5, which sets the maximum permissible noise level as measured at the property line.
- C-13 The applicant must comply with the Chesapeake Bay Preservation Act in accordance with Article XIII of the City's zoning ordinance for storm water quality control which includes requirements for pollutant load reductions and treatment of the Water Quality Volume Default (WQV).

- C-14 The applicant must comply with the City of Alexandria, Erosion and Sediment Control Code, Section 5, Chapter 4. This includes naming a “Certified Land Disturber” on the Erosion and Sediment Control sheets prior to release of the final Site Plan in accordance with Virginia Erosion and Sediment Control Law VAC §: 10.1-563.B.
- C-15 All required permits from Virginia Department of Environmental Quality, Environmental Protection Agency, Army Corps of Engineers, Virginia Marine Resources must be in place for all project construction and mitigation work prior to release of the final site plan. This includes the new state requirement for a VPDES permit for all construction activities greater than 1 acre.

Housing

- S-1 The developer is encouraged to make a voluntary contribution for affordable housing. In consideration of the additional floor area being granted under the SUP approval, the suggested rate is \$1.50 per square foot of gross floor area.

Virginia American Water Company

- F-1 The existing water main that runs through this site is not shown correctly. Where the existing water main is currently shown as six-inch, it is actually eight-inch. A copy of the as-built was mailed to Bowman Consulting.
- F-2 VAWC will require a copy of the Code Enforcement-approved fire flow calculations for this project.
- C-1 As part of the final site plan submission, hydraulic calculations (computer modeling) will be required to verify main sizes. Profiles will be required for hydraulic calculations.
- C-2 A double detector check backflow prevention device is required on all fire services. If located inside the premise, it must have a remote reading meter in a separate accessible room.

Police

(The following recommendations related to lighting have not been included as conditions; rather, staff has recommended that the applicant prepare a lighting plan to the satisfaction of the Director of T&ES in consultation with the police, which will likely result in lower lighting levels than those desired by the Police.)

- R-1 The lighting for the parking garage is to be a minimum of 5.0 foot candle minimum maintained.
- R-2 The lighting for the surface lot and all common areas is to be a minimum of 2.0 foot candles minimum maintained.

Health

- C-1 An Alexandria Health Department Permit is required for all regulated facilities.
 - a. Permits are non-transferable.
 - b. Permits must be obtained prior to operation.
 - c. 5 sets of plans are to be submitted to and approved by the Health Department prior to construction of any facility regulated by the Health Department.
- C-2 Plans for food facilities must comply with the Alexandria City Code, title 11, Chapter 2, Food and Food Establishments. There is a \$135 fee for review of plans for food facilities.
- C-3 Personal grooming facilities must comply with Title 11, Chapter 7, Personal Grooming Establishments.
- C-4 Tanning Salons must meet State Code Title 59.1, Chapter 24.1, Tanning Facilities.
- C-5 Massage facility plans must comply with Title 11, Chapter 4.2, Massage Regulations. All massage therapists must possess a current massage therapist certification, issued by the Commonwealth of Virginia in accordance with the Code of Virginia Chapter 599, §54.1-3029 and must possess an Alexandria Massage permit in accordance with Alexandria City code title 11, Chapter 4.2 prior to engaging in any massage activity.
- C-6 Coin-operated dry cleaning facility plans must comply with Title 9, Chapter 4, Coin operated Dry Cleaning Establishments.
- C-7 Coin-operated laundry plans must comply with Title 9, Chapter 5, Coin Operated Laundries.
- C-8 Food must be protected to the point of service at any outdoor dining facility.

DSUP #2004-0041
TMP SUP #2005-0022
Eisenhower Center III

**REPORT ATTACHMENTS
AVAILABLE IN THE PLANNING AND ZONING OFFICE**